



# Administrative Approval Action

AA #3885 / SR-35-18, Clark Avenue Condos  
Transaction# 551196

City of Raleigh  
Development Services Department  
One Exchange Plaza  
Raleigh, NC 27602  
(919) 996-2495  
DS.help@raleighnc.gov  
www.raleighnc.gov

**LOCATION:** This site is located on the west side of Enterprise Street, between Clark Avenue and Garden Place. The site is comprised of several lots being 2204 Garden Place, 101 Enterprise Street, and 2303 Clark Avenue.

**REQUEST:** Development of several parcels being recombined into one 0.719 acre parcel zoned OX-5-UL CU and within the Special Residential Parking Overlay District. The development is to be a 101,686 square foot, 51 unit condominium building with on-site parking.

## DESIGN

### ADJUSTMENT(S)/

**ALTERNATES, ETC:** One Design Adjustment has been approved for this project, noted below.

1. Provision of 4'X25' planters in lieu of the standard 4' x 6' planters in sections of the right of way as specified by the Raleigh Street Design Manual.

**FINDINGS:** City Administration finds that this request, with the below conditions of approval being met, conforms to the Unified Development Ordinance. This approval is based on a preliminary plan submitted by WithersRavenel.

## **CONDITIONS OF APPROVAL and NEXT STEPS:**

**Note:** *This document must be applied to the second sheet of all future submittals with the exception of final plats.*

**PRIOR TO THE ISSUANCE OF ANY SITE PERMITS OR APPROVAL OF CONCURRENT REVIEW PROCESS, WHICHEVER IS APPLICABLE:**

## ENGINEERING

1. Next Step: A right-of-way obstruction permit must be obtained from Right-of-way Services prior to the commencement of any construction activities within the right-of-way.
2. That a thorough investigation of the intersection of Enterprise Street and Clark Avenue crosswalk occur. This will involve realignment of the current crosswalk in accordance with all applicable codes.

## STORMWATER

3. A stormwater control plan with a stormwater operations and maintenance manual and budget shall be approved by the Stormwater Engineer in compliance with Part 10A Chapter 9 of the Unified Development Ordinance.



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4. In accordance with Part 10A Section 9.4.4 of the Unified Development Ordinance, a surety equal to of the cost of clearing, grubbing and reseeding a site, shall be paid to the City.

## URBAN FORESTRY

5. Obtain required tree impact permits from the City of Raleigh.

## PRIOR TO ISSUANCE OF BUILDING PERMITS:

### GENERAL

1. Comply with all conditions of Z-24-17.
2. A Final Plat shall be recorded with the Wake County Register of Deeds office for all Recombinations, Right of Way and/or Easement Dedications.
3. A demolition permit shall be obtained.
4. Provide fire flow analysis.

### ENGINEERING

5. The  $\frac{1}{2}$  of 90' of required right of way on Clark Avenue,  $\frac{1}{2}$  of 59' of required right of way on Enterprise Street and  $\frac{1}{2}$  of 55' of required right of way on Garden Place for the existing streets shall be dedicated to the City of Raleigh and be shown on a plat approved for recordation.
6. A 5' general utility easement and associated deed of easement shall be approved by the City and the location of the easement shall be shown on the map approved for recordation. The deed of easement shall be recorded at Wake County Register of Deeds within one (1) day of recordation of the recorded plat. A recorded copy of these documents must be provided to the Development Services Department.
7. A sidewalk deed of easement for any public sidewalk on Private Property shall be approved by the City and the location of the easement shall be shown on the map approved for recordation. The deed of easement shall be recorded at Wake County Register of Deeds within one (1) day of recordation of the recorded plat. A recorded copy of these documents must be provided to the Development Services Department.
8. Next Step: In accordance with Part 10A Section 8.1.3, a surety equal to 125% of the cost of development related improvements, including streetscape trees on Clark Avenue, Enterprise Street, and Garden Place is paid to the Development Services Department.
9. Next Step: Concurrent review drawings for site permits and/or infrastructure must be approved prior to submitting applications for building permits.
10. That a Surety for all remaining public improvements is posted.



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11. Next Step: A right-of-way obstruction permit shall be obtained from Right-of-way Services for any construction activity within the right-of-way.

## PUBLIC UTILITIES

12. A Final Plat must be recorded at the Wake County Register of Deeds office for all necessary Recombinations, and Right-of-Way, Utility / Stormwater Easement Dedications.

## STORMWATER

19. The developer shall pay to the City a stormwater facility replacement fund payment equal to twenty-four percent (24%) of the estimated cost of constructing all stormwater control facilities shown on the development plans.
20. All stormwater control measures and means of transporting stormwater runoff to and from any nitrogen and stormwater runoff control measures shall be shown on all plats for recording as private drainage easements.
21. In accordance with Part 10A Section 9.2.2, a surety equal to 125% of the cost of the construction of a stormwater device shall be paid to the Engineering Services Department.

## URBAN FORESTRY

22. A tree impact permit must be obtained for the approved streetscape tree installation in the right of way.

### **Prior to issuance of building occupancy permit:**

1. All Water, Sanitary Sewer and Reuse facilities shall be installed, inspected, tested and accepted by the City of Raleigh Public Utilities Department for operations and maintenance.
2. Next Step: Required public right-of-way infrastructure improvements shall be, at a minimum, sufficiently completed to provide the necessary, safe emergency and vehicular access.
3. Next Step: All street lights and street signs required as part of the development approval are installed.
4. Next Step: Copies of certified inspection reports for alleys and internal access ways are submitted to the City of Raleigh Development Services – Development Engineering program.
5. As-built drawings and associated forms for all Stormwater devices are accepted by the Engineering Services Department
6. Next Step: Final inspection of all required right of way tree plantings by Urban Forestry Staff.



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**EXPIRATION DATES:** The expiration provisions of UDO Section 10.2.8 E, including the ability to request extensions in the expiration date, apply to this site plan. If significant construction has not taken place on a project after administrative site review approval, that approval may expire and be declared void, requiring re-approval before permits may be issued. To avoid allowing this plan approval to expire the following must take place by the following dates:

**3-Year Expiration Date:** 11-13-2021

Obtain a valid building permit for the total area of the project, or a phase of the project.

**4-Year Completion Date:**

Within four years after issuance of the first building permit for the site plan, the construction of the entire site plan must be completed unless an applicant has been granted vested rights. Failure to complete construction within this specified time frame shall automatically void the approved site plan for which no building permits have been issued.

I hereby certify this administrative decision.

Signed:(Planning Dir./Designee)

Date: 11/13/18

Staff Coordinator: Michael Walters

# Design Adjustment Staff Response



**DEVELOPMENT  
SERVICES  
DEPARTMENT**

Per Section 10.2.18.C of the Unified Development Ordinance, the Development Services Director, or designee, shall consult with the heads of other City Departments regarding the review of the request. The Development Services Director, or designee, shall approve, approve with conditions or deny the request, but must do so within 60 days of the receipt of a completed application. Additional time may be necessary if a municipal or state entity is incorporated in the review process or if a detailed engineering study is submitted in conjunction with the request.

<b>PROJECT</b>	Project Name	Enterprise Condos	
	Development Case Number	SR-35-18	
	Transaction Number	551196	
	Design Adjustment Number	DA - 85 - 2018	
<b>STAFF RESPONSE</b>	<b>Staff recommendation based upon the findings in the applicable code(s):</b>		
	<input type="checkbox"/> UDO Art. 8.3 Blocks, Lots, Access	<input type="checkbox"/> UDO Art. 8.5 Existing Streets	
	<input type="checkbox"/> UDO Art. 8.4 New Streets	<input checked="" type="checkbox"/> Raleigh Street Design Manual	
	Staff SUPPORTS <input checked="" type="checkbox"/> DOES NOT SUPPORT <input type="checkbox"/> the design adjustment request.		
	<b>DEPARTMENTS</b>		
	<input type="checkbox"/> Dev. Services Planner	<input type="checkbox"/> City Planning	
	<input checked="" type="checkbox"/> Development Engineering	<input type="checkbox"/> Transportation	
	<input type="checkbox"/> Engineering Services	<input type="checkbox"/> Parks & Recreation and Cult. Res.	
	<input type="checkbox"/> Public Utilities		
	<b>CONDITIONS:</b>		
<div style="height: 150px; border: 1px solid black;"></div>			
Development Services Director or Designee Action: <input checked="" type="checkbox"/> APPROVE <input type="checkbox"/> APPROVE WITH CONDITIONS <input type="checkbox"/> DENY			

Authorized Signature

*HELEN W. RITCHIE, PE, MPA*  
ENGINEERING AND INFRASTRUCTURE MANAGER

Date

11/13/2018

\*The Development Services Director may authorize a designee to sign in his/her stead. Please print name and title next to signature.  
Appeal of the decision from the Development Services Director, or his or her designee, shall be made in writing within 30 days to the Board of Adjustment (see Section 10.2.18.C3b).

# Staff Response Raleigh Street Design Manual



DEVELOPMENT  
SERVICES  
DEPARTMENT

- A. The requested design adjustment meets the intent of the Raleigh Street Design Manual;  
YES ☒ NO ☐
- B. The requested design adjustment conforms with the Comprehensive Plan and adopted City plans;  
YES ☒ NO ☐
- C. The requested design adjustment does not increase congestion or compromise safety;  
YES ☒ NO ☐
- D. The requested design adjustment does not create additional maintenance responsibilities for the City; and  
YES ☒ NO ☐
- E. The requested design adjustment has been designed and certified by a Professional Engineer.  
YES ☒ NO ☐

## STAFF FINDINGS

Staff supports the request for an altered planting area for the street trees along the frontages of Clark Ave and Enterprise St. The proposed change does not alter the UDO/RSDM required pedestrian walkway depth, but rather elongates the tree grate area. The previously approved development, S-63-17, has granted a Design Adjustment for this altered streetscape. By approving the altered streetscape, the block's streetscape will remain consistent in that area. There are no issues with pedestrian access or ADA accessible ramps at street crossings due to the altered streetscape.

# Design Adjustment Application



**DEVELOPMENT  
SERVICES  
DEPARTMENT**

The purpose of this request is to seek a Design Adjustment from the Development Services Director, or designee, for a specific project only and, if granted, may be approved with special conditions and provisions. This application and all further action shall be consistent with Section 10.2.18 in the Unified Development Ordinance (UDO). The consideration and decision of this request shall be based solely on the conformance of the findings, as outlined in Sec. 8.3.6, Sec. 8.4.1.E and Sec. 8.5.1.G of the UDO or the Raleigh Street Design Manual.

PROJECT OWNER CONTACT REQUEST	<b>Project Name</b> Enterprise Condos		
	<b>Case Number</b> SR-35-18		
	<b>Transaction Number</b> 551196		
	<b>Name</b> Lambert Development Enterprise Street LLC		
	<b>Address</b> 5 Hanover Sq Floor 14		<b>City</b> New York
	<b>State</b> NY	<b>Zip Code</b> 10004-2614	<b>Phone</b> 212-785-0090 x225
	<b>Name</b> Rob Caudle		<b>Firm</b> WithersRavenel
	<b>Address</b> 137 S. Wilmington St. Suite 200		<b>City</b> Raleigh
	<b>State</b> NC	<b>Zip Code</b> 27601	<b>Phone</b> 919-238-0359
	<b>I am seeking a Design Adjustment from the requirements set forth in the following:</b>		
<input type="checkbox"/> UDO Art. 8.3 Blocks, Lots, Access		- See page 2 for findings	
<input type="checkbox"/> UDO Art. 8.4 New Streets		- See page 3 for findings	
<input type="checkbox"/> UDO Art. 8.5 Existing Streets		- See page 4 for findings	
<input checked="" type="checkbox"/> Raleigh Street Design Manual		- See page 5 for findings	
<b>Provide details about the request; (please attach a memorandum if additional space is needed):</b>			
This is a request to provide 4' x 25' street tree planters in place of the 4' x 6' street tree grates that are specified in the Raleigh Street Design Manual.			

It is the responsibility of the applicant to provide all pertinent information needed for the consideration of this request. Applicant must be the Property Owner.

By signing this document, I hereby acknowledge the information on this application is, to my knowledge, accurate.

sign →

Owner/Owner's Representative Signature

Date

CHECKLIST	
Signed Design Adjustment Application	<input checked="" type="checkbox"/> Included
Page(s) addressing required findings	<input checked="" type="checkbox"/> Included
Plan(s) and support documentation	<input checked="" type="checkbox"/> Included
Notary page (page 6) filled out; Must be signed by property owner	<input checked="" type="checkbox"/> Included
First Class stamped and addressed envelopes with completed notification letter	<input checked="" type="checkbox"/> Included

Submit all documentation, with the exception of the required addressed envelopes and letters to [designadjustments@raleighnc.gov](mailto:designadjustments@raleighnc.gov).

Deliver the addressed envelopes and letters to:  
Development Services, Development Engineering  
One Exchange Plaza, Suite 500  
Raleigh NC, 27601

<b>For Office Use Only</b>	<b>RECEIVED DATE:</b>	<b>DA -</b>
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# **Raleigh Street Design Manual**

## **Administrative Design Adjustment Findings**



**DEVELOPMENT  
SERVICES  
DEPARTMENT**

The Development Services Director may in accordance with *Sec. 10.2.18*, approve a design adjustment, subject to all of the following findings. Describe how each item is met:

- A. The requested design adjustment meets the intent of the Raleigh Street Design Manual;  
The proposed 4' x 25' street tree planters will provide a larger area to accommodate growth than the typical 4' x 6' street tree grates. The proposed street tree planters are consistent with the cross-sectional dimensions specified in the Street Type Overview section of the Raleigh Street Design Manual.
- B. The requested design adjustment conforms with the Comprehensive Plan and adopted City plans;  
The proposed street tree planters will have no affect on the Comprehensive Plan and adopted City plans.
- C. The requested design adjustment does not increase congestion or compromise safety;  
There will be a 8' clearance behind the proposed street tree planters.
- D. The requested design adjustment does not create additional maintenance responsibilities for the City; and  
The proposed street tree planters eliminates cost and maintenance responsibilities of metal grates. No additional maintenance responsibilities will be required.
- E. The requested design adjustment has been designed and certified by a Professional Engineer.  
A Professional Engineer has designed and certified the requested design adjustment (see plans).



# Individual Acknowledgement



DEVELOPMENT  
SERVICES  
DEPARTMENT

STATE OF New York  
COUNTY OF New York

INDIVIDUAL

I, Craig Wexler, a Notary Public do hereby certify that  
Henry A. Lambert personally appeared before me this day and  
acknowledged the due execution of the forgoing instrument.

This the 22 day of Aug, 2018.

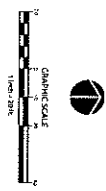
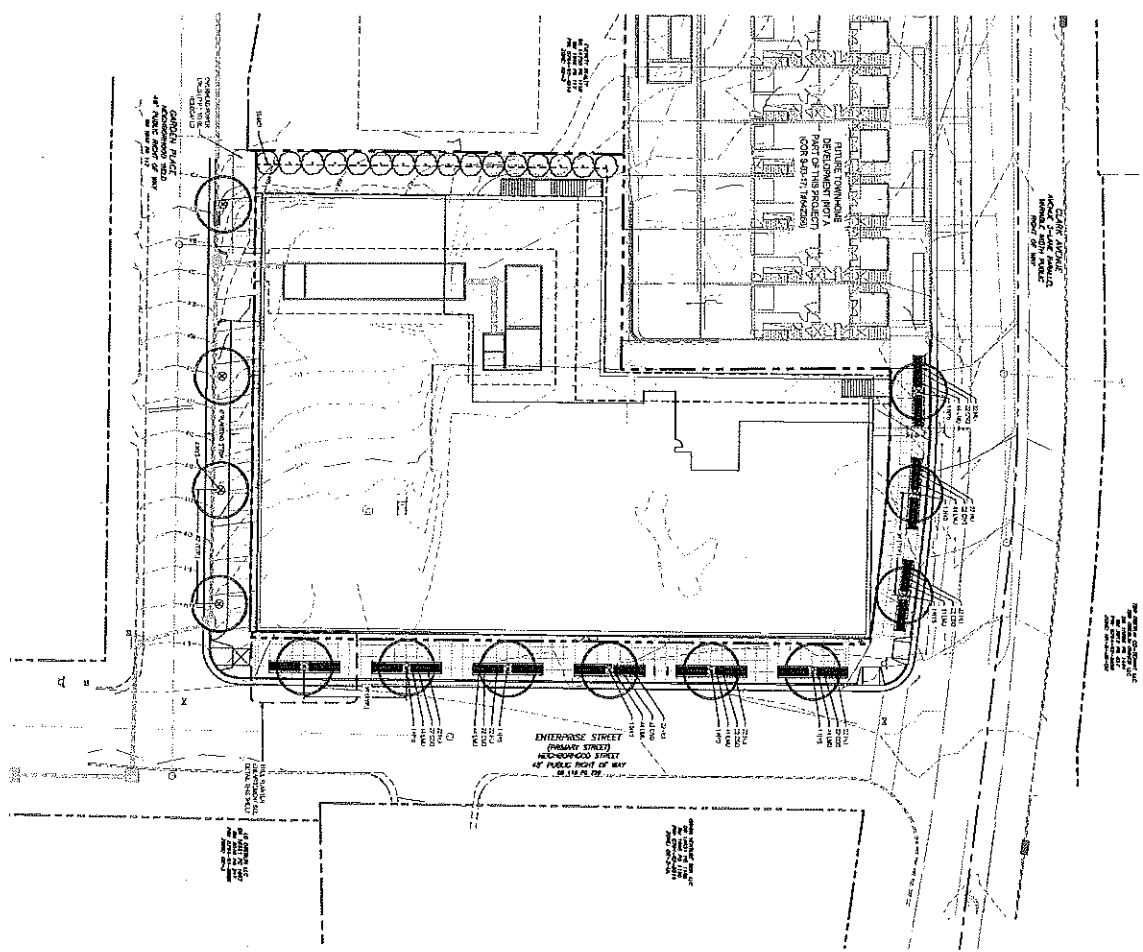
(SEAL)

Notary Public

A handwritten signature in cursive script, appearing to read "Craig Wexler".

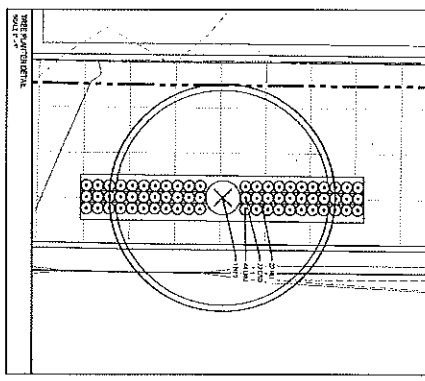
My Commission Expires: \_\_\_\_\_

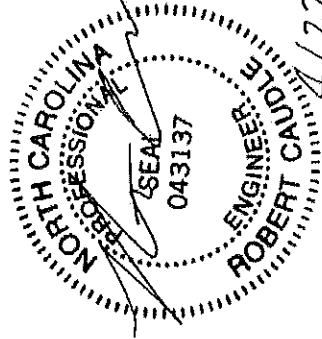
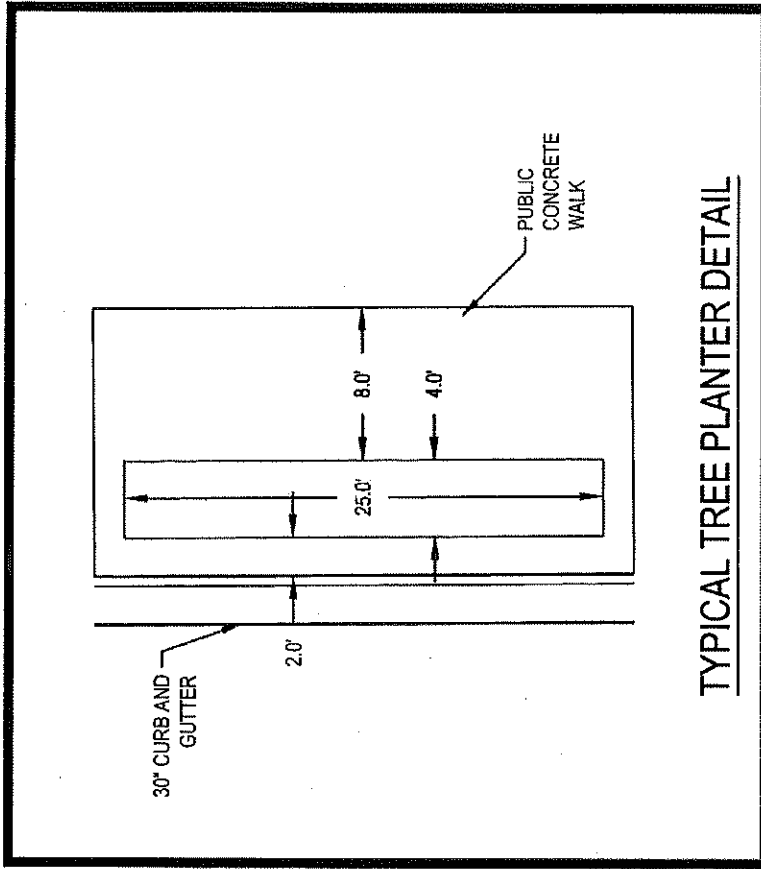
CRAIG WEXLER  
Notary Public, State of New York  
No. 01WE6071493  
Qualified in New York County  
Commission Expires March 18, 2022



CLARK AVENUE CONDOS PLANT SCHEDULE									
NO.	PLANT NAME	SIZE	QUANTITY	NOTES	NO.	PLANT NAME	SIZE	QUANTITY	NOTES
1	PLANT NAME	SIZE	QUANTITY	NOTES	11	PLANT NAME	SIZE	QUANTITY	NOTES
2	PLANT NAME	SIZE	QUANTITY	NOTES	12	PLANT NAME	SIZE	QUANTITY	NOTES
3	PLANT NAME	SIZE	QUANTITY	NOTES	13	PLANT NAME	SIZE	QUANTITY	NOTES
4	PLANT NAME	SIZE	QUANTITY	NOTES	14	PLANT NAME	SIZE	QUANTITY	NOTES
5	PLANT NAME	SIZE	QUANTITY	NOTES	15	PLANT NAME	SIZE	QUANTITY	NOTES
6	PLANT NAME	SIZE	QUANTITY	NOTES	16	PLANT NAME	SIZE	QUANTITY	NOTES
7	PLANT NAME	SIZE	QUANTITY	NOTES	17	PLANT NAME	SIZE	QUANTITY	NOTES
8	PLANT NAME	SIZE	QUANTITY	NOTES	18	PLANT NAME	SIZE	QUANTITY	NOTES
9	PLANT NAME	SIZE	QUANTITY	NOTES	19	PLANT NAME	SIZE	QUANTITY	NOTES
10	PLANT NAME	SIZE	QUANTITY	NOTES	20	PLANT NAME	SIZE	QUANTITY	NOTES

- GENERAL LANDSCAPE NOTES:**
1. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  2. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  3. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  4. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  5. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  6. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
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  16. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  17. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  18. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  19. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.
  20. ALL PLANTING TO BE IN ACCORDANCE WITH THE CITY OF RALEIGH'S LANDSCAPE DESIGN AND INSTALLATION MANUAL.







SR-35-18

TRANSACTION NO: 551196

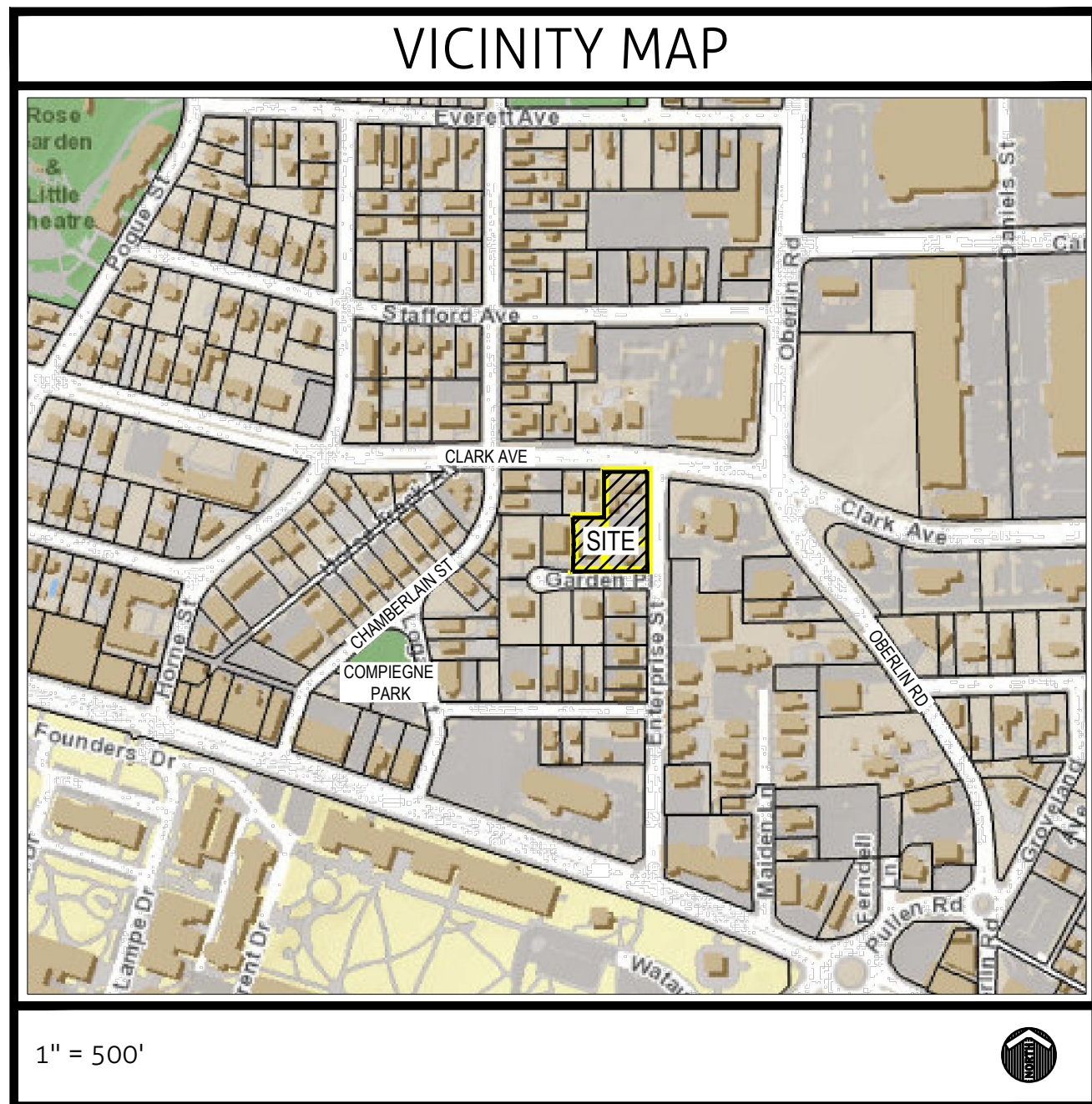
# ADMINISTRATIVE SITE REVIEW FOR CLARK AVENUE CONDOS

101 ENTERPRISE STREET, RALEIGH, NC 27607

1st Submittal: 04/11/18

2nd Submittal: 08/27/18

3rd Submittal: 09/21/18



**Administrative Site Review Application**  
(for UDO Districts only)

Development Services Customer Service Center | Exchange Plaza, Suite 400 | Raleigh, NC 27601 | (919) 996-3495 | (fax) 919-996-1831  
Litchford Satellite Office | 1320 - 130 Litchford Road | Raleigh, NC 27601 | (919) 996-4200

When submitting plans, please check the appropriate building type and include the Plan Checklist document.

BUILDING TYPE		FOR OFFICE USE ONLY	
<input type="checkbox"/> Detached	<input type="checkbox"/> General	Transaction Number	
<input type="checkbox"/> Attached	<input type="checkbox"/> Mixed Use	Assigned Project Controller	
<input type="checkbox"/> Apartment	<input type="checkbox"/> Open Lot	Assigned Team Leader	
<input type="checkbox"/> Townhouse			

Has your project previously been through the Due Diligence or Sketch Plan Review process? If yes, provide the transaction #

**GENERAL INFORMATION**

Development Name: **Clark Avenue Condos**

Zoning District: **OX-5-UL CU** Overlay District (if applicable): **SRPOD** Inside City Limits? ☒ Yes ☐ No

Proposed Use: **CONDOS**

Property Address(es): **2204 GARDEN PL., 101 ENTERPRISE ST., 2303 CLARK AV.** Major Street Location: **CLARK AVENUE**

Wake County Property Identification Number(s) for each parcel to which these guidelines will apply:

P.I.N. **0794-92-4283** P.I.N. **0794-92-5270** P.I.N. **0794-92-5371** P.I.N.

What is your project type? ☐ Apartment ☐ Community ☐ Hospital ☐ Motel/Hotel ☐ Office ☐ Mixed Residential ☐ Non-Residential Condo ☐ School ☐ Shopping Center ☐ Retail ☐ Industrial Building ☐ Church ☐ Telecommunication Tower ☐ Religious Institution ☐ Residential Condo ☐ Retail ☐ Cottage Court

Other: If other, please describe:

**WORK SCOPE**

Per City Code Section 10.2.8.D.1, summarize the project work scope. For additions, changes of use, or occupancy (per Chapter 6 of the UDO), indicate impacts on parking requirements.

Construction of 51 condo units and associated site improvements.

**DESIGN ADJUSTMENT OR ADMIN ALTERNATE**

Per City Code Chapter 8, summarize if your project requires either a design adjustment, or Section 10 - Alternate Administrative Act

**CLIENT/DEVELOPER/OWNER**

Company: **LAMBERT DEVELOPMENT ENTERPRISE STREET LLC** Name: **MR. KERRY BERMAN**

Address: **5 HANOVER SQUARE, 14TH FLOOR, NEW YORK, NY 10004**

Phone: **212.785.0090 X 225** Email: Fax:

**CONSULTANT**

Company: **WITHERSRAVENEL** Name: **MR. ROB CAUDLE, PE**

Address: **137 S. WILMINGTON STREET, SUITE 200, RALEIGH, NC 27601**

Phone: **919.469.3340** Email: **rcaudle@withersravenel.com** Fax:

PAGE 1 OF 3 WWW.RALEIGHNC.GOV REVISION 05.13.16

**DEVELOPMENT TYPE & SITE DATA TABLE** (Applicable to all developments)

Zoning Information	Building Information
Zoning District(s): <b>OX-5-CU-PL</b>	Proposed building use(s): <b>RES. CONDOS</b>
If more than one district, provide the acreage of each:	Existing Building(s) sq. ft. gross: <b>11,474 sq. ft.</b>
Overlay District: <b>SRPOD</b>	Proposed Building(s) sq. ft. gross: <b>101,686 sq. ft.</b>
Total Site Acres: <b>Inside City Limits: <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</b>	Total sq. ft. gross (existing & proposed): <b>101,686 sq. ft.</b>
Off street parking: Required: <b>35</b> Provided: <b>82</b>	Proposed height of building(s): <b>65'</b>
COA (Certificate of Appropriateness) case #:	# of stories: <b>5</b>
BOA (Board of Adjustment) case #:	Ceiling height of 1 <sup>st</sup> floor: <b>12'</b>
CUD (Conditional Use District) case #:	
<b>Stormwater Information</b>	
Existing Impervious Surface: <b>38/16,944</b> acres/square feet	Flood Hazard Area: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Proposed Impervious Surface: <b>81/35,065</b> acres/square feet	# Yes, please provide: <b>At Risk Soil</b>
Neuse River Buffer: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Flood Study: <b>FEMA Map Panel #</b>
<b>FOR RESIDENTIAL DEVELOPMENTS</b>	
1. Total # of Apartment, Condominium or Residential Units: <b>51</b>	5. Bedroom Units: <b>1br 20 2br 24 3br 7 4br or more</b>
2. Total # of Congregate Care or Life Care Dwelling Units:	6. Infill Development: <b>2.2.7</b>
3. Total Number of Hotel Units:	7. Open Space (only) or Amenity: <b>4,486 sq. ft.</b>
4. Overall Total # of Dwelling Units (1-6 Above): <b>51</b>	8. Is your project a cottage court? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
<b>SIGNATURE BLOCK</b> (Applicable to all developments)	
I, the undersigned, as the property owner(s), do hereby agree and affirm that this project is conforming to all application requirements applicable with the proposed development use.	
I hereby designate: <b>DAVID BROWN/ROB CAUDLE</b> to serve as my agent regarding this application, to receive and respond to administrative comments, to submit plans on my behalf and to represent me in any public meeting regarding this application.	
I have read, acknowledge and affirm that this project is conforming to all application requirements applicable with the proposed development use.	
Signed: <b>Kerry Berman</b> Date: <b>8/27/18</b>	
Printed Name: <b>Kerry Berman</b> Vice President	
Signed: _____ Date: _____	
Printed Name: _____	

**GENERAL NOTES**

- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH AND NCDD STANDARDS AND SPECIFICATIONS IF APPLICABLE.
- THE CONTRACTOR SHALL OBTAIN A RIGHT-OF-WAY PERMIT FOR ANY WORK WHICH REQUIRES THE CLOSURE OF A TRAVEL LANE(S), PARKING SPACE, OR SIDEWALK FROM RIGHT-OF-WAY SERVICES AT LEAST 48 HOURS IN ADVANCE.
- IF CONSTRUCTION PLANS FOR PUBLIC AND PRIVATE STREETS OR UTILITIES SHOWN ON THIS PLAN ARE REQUIRED, THEY MUST BE APPROVED BY THE PUBLIC WORKS DEPARTMENT AND PUBLIC UTILITIES DEPARTMENT PRIOR TO ISSUANCE OF PERMITS OR RECORDING OF ANY PLAN FOR THIS DEVELOPMENT.
- FIELD ADJUSTMENTS TO THIS PLAN MAY BE REQUIRED BY CITY OF RALEIGH INSPECTOR AS NEEDED DURING CONSTRUCTION.
- ALL SURVEY INFORMATION PROVIDED TO WITHERSRAVENEL, RALEIGH, NORTH CAROLINA OFFICE BY CARY, NORTH CAROLINA OFFICE IN DIGITAL FORMAT AND SUPPLEMENTAL INFORMATION WAS OBTAINED FROM WAKE COUNTY GIS.
- ALL PROPOSED CURB AND GUTTER WITHIN PUBLIC RIGHT OF WAY SHOWN ON PLANS TO BE 30" CITY OF RALEIGH STANDARD CONCRETE CURB AND GUTTER, AND ALL OTHER PROPOSED CURB AND GUTTER TO BE 24" CONCRETE CURB AND GUTTER UNLESS OTHERWISE STATED ON PLANS.
- ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, UNLESS OTHERWISE STATED ON PLANS.
- WITHIN THE SIGHT TRIANGLES SHOWN ON THIS PLAN, NO OBSTRUCTION BETWEEN 4 FEET AND 8 FEET IN HEIGHT ABOVE THE CURB LINE ELEVATION SHALL BE LOCATED IN WHOLE OR PART. OBSTRUCTIONS INCLUDE BUT ARE NOT LIMITED TO ANY BERM, FOLIAGE, FENCE, WALL, SIGN, OR PARKED VEHICLE.
- UNLESS NOTED, ACCESS ROUTE FOR EMERGENCY VEHICLES SHALL PROVIDE AN INSIDE TURNING RADIUS OF 28' MINIMUM.
- ALL HVAC UNITS SHALL BE SCREENED FROM VIEW OF THE PUBLIC RIGHT OF WAY.
- CONTRACTOR TO FIELD LOCATE AND VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND REPORT ANY DISCREPANCIES TO LANDSCAPE ARCHITECT PRIOR TO ANY CONSTRUCTION ACTIVITIES. CONTACT NC ONE AT 811 FOR FIELD LOCATION OF UNDERGROUND UTILITIES.
- HANDICAP PARKING SPACE(S) AND ACCESS (SIDEWALK) SHALL BE NO GREATER THAN TWO PERCENT (2%) PITCH IN ANY DIRECTION(S) AS PER ADA STANDARDS.
- PROVIDE SIGNAGE AND STRIPING OF HANDICAP SPACES AS PER ADA STANDARDS.
- ALL RETAINING WALLS GREATER THAN 30" IN HEIGHT TO INCLUDE SAFETY RAIL OR FENCE. NO RETAINING WALLS ARE PERMITTED IN THE RIGHT-OF-WAY UNLESS APPROVED BY ENCROACHMENT.
- THE MINIMUM CORNER CLEARANCE FROM THE CURB LINE OF INTERSECTING STREETS SHALL BE AT LEAST 50 FEET FROM THE POINT OF TANGENCY OF THE CURB FOR MIXED-USE/COMMERCIAL DRIVEWAY. NO DRIVEWAYS SHALL ENCRUCH ON THIS MINIMUM CORNER CLEARANCE.
- WE ACCESS RAMP(S) WILL BE PROVIDED IN ACCORDANCE WITH CITY OF RALEIGH PUBLIC WORKS DEPARTMENT STANDARDS, PROWAG STANDARDS AND ADAAG SPECIFICATIONS.
- ALL RAMP(S) AND HANDRAILS SHALL BE CONFORM TO ANSI STANDARDS.
- ALL ABOVE GROUND UTILITY DEVICES (TO INCLUDE BUT NOT LIMITED TO TELEPHONE AND CABLE PEDESTALS, ELECTRICAL TRANSFORMERS, BACKFLOW DEVICE HOTBOX, ETC) SHALL BE SCREENED FROM OFF-SITE VIEW BY EVERGREEN SHRUBS, FENCE, OR WALL.
- ALL SIDEWALKS MUST BE ACCESSIBLE TO PERSONS WHO ARE BLIND, HAVE LOW VISION AND PEOPLE WITH MOBILITY DISABILITIES. PEDESTRIAN EXISTING ROUTES AND ALTERNATE PEDESTRIAN ROUTES DURING CONSTRUCTION WILL BE REQUIRED TO BE COMPLIANT WITH THE PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG), 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- IF UNFORESEEN CONDITIONS DEVELOP DURING CONSTRUCTION, REFER TO "CITY OF RALEIGH STREET DESIGN MANUAL" AND CONTACT PUBLIC WORKS DEPARTMENT FOR FURTHER GUIDANCE.
- PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY OF RALEIGH PUBLIC WORKS DEPARTMENT / TRANSPORTATION FIELD SERVICES STAFF TO REVIEW THE SPECIFIC COMPONENTS OF THE PLAN AND OPERATION OF THESE FACILITIES DURING CONSTRUCTION. CONTACT ENGINEERING INSPECTIONS AT 919.996.2409 TO SET UP THE MEETING.
- THE CONTRACTOR SHALL CONDUCT THE WORK IN A SAFE MANNER AND WITH A MINIMUM AMOUNT OF INCONVENIENCE TO TRAFFIC.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SHALL ADHERE TO THE PROVISIONS OF THE MUTCD (MOST CURRENT EDITION).
- ANY TEMPORARY STREET CLOSURE MUST BE SUBMITTED A MINIMUM OF 2 WEEKS PRIOR TO THE START OF WORK. SEE "RIGHT-OF-WAY CLOSURES" ON WWW.RALEIGHNC.GOV. COMPLETE AND SEND THE RIGHT-OF-WAY OBSTRUCTION PERMIT TO RIGHTOFWAYSERVICES@RALEIGHNC.GOV.
- PRIOR TO CONSTRUCTION BEGINNING, ALL SIGNAGE AND TRAFFIC CONTROL SHALL BE IN PLACE.

**SOLID WASTE INSPECTION STATEMENT**

- SOLID WASTE SERVICES ARE TO BE PROVIDED BY A PRIVATE CONTRACTOR.
- THE DEVELOPER WILL COMPLY WITH ALL REQUIREMENTS SET FORTH IN THE SOLID WASTE MANUAL.
- THIS PROJECT HAS 6 ROLL-OUT CONTAINERS FOR RECYCLING THAT WILL BE STORED INSIDE THE BUILDING AND ROLLED OUT TO GARDEN PLACE ON PICK UP DAYS. THIS PROJECT HAS MINI MAC ROLLING CONTAINERS FOR SOLID WASTE WHICH WILL BE STORED IN THE PARKING GARAGE AND ROLLED OUT TO GARDEN PLACE ON PICK UP DAYS.

**FIRE DEPARTMENT NOTES**

- THE APPARATUS ROAD SURFACE IS REQUIRED TO BE WITHIN 150 FEET (NFPA 13R); NO SPRINKLING SYSTEM PROPOSED.
- FIRE HYDRANT SHALL BE LOCATED WITHIN 300' AS MEASURED ALONG THE PATH OF APPARATUS ACCESS ROAD (NCF 507.5.1).
- ALL NEW FIRE HYDRANTS SHALL BE EQUIPPED WITH NST THREADS AND WITH A 5 INCH STORZ CONNECTION.
- FIRE FLOW ANALYSIS MUST BE PROVIDED AT TIME OF BUILDING PERMITS PER THE 2012 NCF, SECTION 507.3.

**DEVELOPER/OWNER**

**LAMBERT DEVELOPMENT LLC**  
5 HANOVER SQUARE 14TH FLOOR  
NEW YORK, NEW YORK 10004  
CONTACT: KERRY A. BERMAN  
PHONE: (212) 785-0090 ext. 225  
EMAIL: KBERMAN@LAMBERTDEVELOPMENT.COM

**ARCHITECT**

**MHA WORKS ARCHITECTURE**  
501 WASHINGTON ST #G  
DURHAM, NORTH CAROLINA 27701  
CONTACT: JARED MARTINSON  
PHONE: 919-682-2870  
EMAIL: JMARTINSON@MHAWORKS.COM

**PREPARED BY**

**WithersRavenel**  
Engineers | Planners  
137 S. Wilmington Street | Suite 200 | Raleigh, NC 27601 | t: 919.469.3340 | license #: C-0832  
www.withersravenel.com

**PLANNER: DAVID BROWN, PLA**  
DBROWN@WITHERSRAVENEL.COM

**CIVIL ENGINEER: ROB CAUDLE, PE**  
RCAUDLE@WITHERSRAVENEL.COM

**ZONING CONDITIONS**

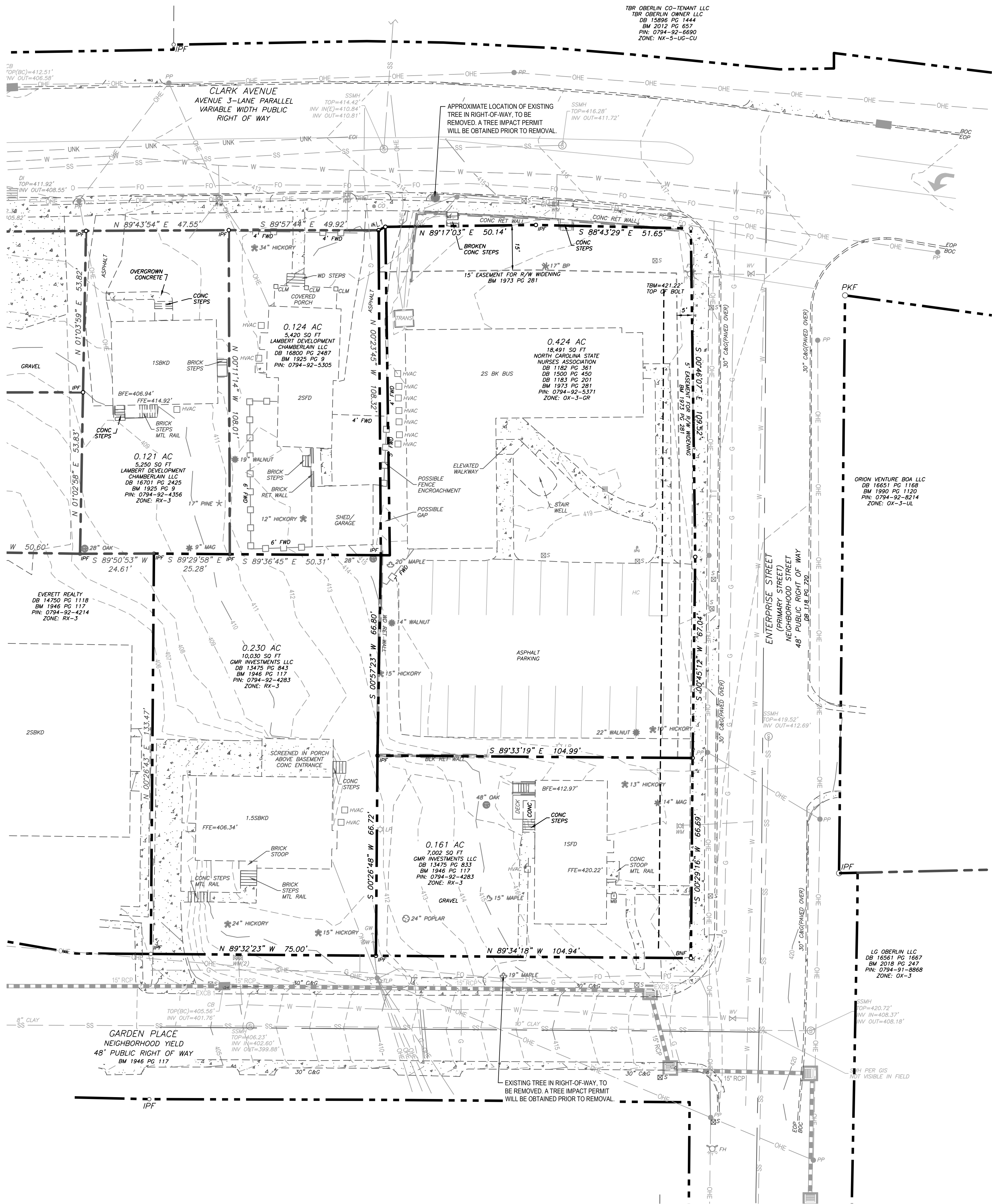
- IF THE PRIMARY USE OF THE PROPERTY IS NOT RESIDENTIAL, THE MAXIMUM BUILDING HEIGHT SHALL BE NO MORE THAN FOUR (4) STORIES.
- BUILDING FACADES SHALL BE CONSTRUCTED FROM ONE OR MORE OF THE FOLLOWING MATERIALS: GLASS, CONCRETE AND/OR CLAY BRICK, MASONRY, CEMENTITIOUS STUCCO, NATIVE AND MASONRY STONE, NATURAL WOOD, PRECAST CONCRETE, PHENOLIC PANEL, CEMENT PANEL, AND METAL PANEL AND/OR TRIM.
- THE FOLLOWING EXTERIOR BUILDING MATERIALS SHALL BE PROHIBITED: SYNTHETIC STUCCO SIDING (I.E. EIFS), MASONITE SIDING, VINYL SIDING, AND/OR CEMENTITIOUS LAP SIDING.
- CEMENT PANEL SIDING SHALL BE ALLOWED, BUT JOINTS SHALL BE CHANNEL REVEAL OR SIMILAR DETAIL. NO BATTERY STRIP JOINTS SHALL BE ALLOWED FOR CEMENT PANEL SIDING. CEMENT PANEL SIDING SHALL BE LIMITED TO NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE BUILDING(S) EXTERIOR SIDING, EXCLUSIVE OF PENETRATION OPENINGS (DOORS, WINDOWS, ETC).
- PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY FOR NEW DEVELOPMENT ON THE SUBJECT PROPERTY, THE OWNER SHALL PROVIDE A PAVED, PUBLIC "GREENWAY CONNECTOR" SIDEWALK ALONG THE FRONTAGES OF ENTERPRISE ST. AND CLARK AVE.
- RESIDENTIAL DWELLING UNIT COUNT SHALL BE LIMITED TO NO MORE THAN SIXTY (60) DWELLING UNITS (DU) ON THE SUBJECT PROPERTY.
- NO MORE THAN SIX (6) HVAC UNITS MAY BE LOCATED ON THE GROUND, PROVIDED THAT NO HVAC UNIT SHALL BE LOCATED BETWEEN THE BUILDING FACADE AND A PUBLIC RIGHT OF WAY.
- THE FOLLOWING LAND USES SHALL BE PROHIBITED ON THE SUBJECT PROPERTY: PARKING AS A PRINCIPAL USE, SPORTS ACADEMY, OUTDOOR SPORTS/ENTERTAINMENT FACILITY, TELECOMMUNICATION TOWER, AND/OR EMERGENCY SHELTER.
- RETAIL USE(S) LOCATED ON THE SUBJECT PROPERTY SHALL BE LIMITED TO NO MORE THAN FOUR THOUSAND SQUARE FEET (4,000 SF) GROSS FLOOR AREA.
- OFFICE USE(S) SHALL BE LIMITED TO NO MORE THAN NINE THOUSAND FIVE HUNDRED SQUARE FEET (9,500 SF) GROSS FLOOR AREA.
- FOR ANY DWELLING LOCATED ON THE SUBJECT PROPERTY, THE MAXIMUM BEDROOM COUNT IN A SINGLE DWELLING SHALL BE NO MORE THAN THREE (3) BEDROOMS.
- STRUCTURED PARKING SHALL BE SCREENED FROM OFFSITE VIEW, INCLUDING ADJACENT PARCELS AND/OR PUBLIC RIGHT-OF-WAY. SCREENING OF STRUCTURED PARKING SHALL BE NO LESS THAN 75% OPAQUE FOR ANY OPENING FACING ADJACENT PARCEL/PUBLIC RIGHT-OF-WAY OTHER THAN OPENING UTILIZED FOR VEHICULAR ACCESS POINT.
- STRUCTURED PARKING SCREENING SHALL INCLUDE, BUT IS NOT LIMITED TO: PIERCED-BRICK WALL, METAL SHUTTERS, AND SIMILAR ARCHITECTURAL ELEMENTS. EVERGREEN PLANTINGS MAY ALSO BE USED TO SCREEN STRUCTURED PARKING, BUT SUCH PLANTINGS, TOGETHER WITH ANY OTHER SCREENING MATERIAL, SHALL MEET THE 75% OPAQUE REQUIREMENT AT TIME OF INITIAL INSTALLATION OF SAME.
- VEHICLE ACCESS OPENINGS TO STRUCTURED PARKING FACING A PUBLIC RIGHT-OF-WAY SHALL BE SCREENED WITH AN ARCHITECTURAL ELEMENT (I.E. - GATE, DOOR, ROLL-DOWN SCREEN) TO PROVIDE NO LESS THAN FIFTY PERCENT (50%) OPAQUE.
- THE AVERAGE HEIGHT OF THE BUILDING MEASURED AT ENTERPRISE STREET (EAST ELEVATION) AND AT CLARK AVENUE (NORTH ELEVATION) SHALL BE NO GREATER THAN SIXTY-FIVE FEET (65') FROM FINISHED GRADE TO THE PEAKTOP OF THE ROOF FRAMING.
- NON-RESIDENTIAL USES SHALL HAVE DIRECT ADJACENCY AND PRIMARY ACCESS TO EITHER ENTERPRISE ST. AND/OR CLARK AVE.
- NON-RESIDENTIAL USES SHALL NOT EXTEND MORE THAN FORTY FEET (40') ALONG GARDEN PLACE, WHEN MEASURED FROM THE SOUTHEAST PROPERTY CORNER LOCATED AT THE INTERSECTION OF ENTERPRISE ST. & GARDEN PLACE.
- NON-RESIDENTIAL USES ALONG CLARK AVE. SHALL NOT BE LOCATED WITHIN FORTY FEET (40') OF THE WESTERN PROPERTY LINE.
- BUILDING TRANSPARENCY FOR RESIDENTIAL USE WHERE ADJACENT TO GARDEN PLACE, ENTERPRISE STREET, AND CLARK AVENUE SHALL BE NO LESS THAN THE FOLLOWING: GROUND FLOOR: 30% FOR "APARTMENT" BUILDING TYPE, AND UPPER STORIES: 30% FOR "APARTMENT" BUILDING TYPE.
- IF A FIVE-STORY BUILDING IS CONSTRUCTED ON THE SUBJECT PROPERTY, THE TRASH AND RECYCLE COLLECTION FACILITIES SHALL BE LOCATED WITHIN THE BUILDING OR AN ENCLOSED SERVICE COURTYARD.
- AT THE FIFTH STORY OF A BUILDING, A BUILDING STEP-BACK OF NO LESS THAN 8 FT. FROM THE AVERAGE FACADE PLANE SHALL BE PROVIDED ON THE GARDEN PLACE ELEVATION (SOUTH ELEVATION). THE STEP-BACK SHALL BE NO LESS THAN SIXTY-FIVE PERCENT (65%) OF THE OVERALL FIFTH (5<sup>TH</sup>) STORY FACADE.
- IF A FIVE (5) STORY RESIDENTIAL BUILDING IS CONSTRUCTED ON THE PROPERTY, IT SHALL BE COMPOSED OF STRUCTURAL STEEL AND/OR CONCRETE CONSTRUCTION (I.E., NOT STRUCTURAL WOOD). THIS CONDITION SHALL ONLY APPLY IF A FIVE (5) STORY RESIDENTIAL BUILDING IS CONSTRUCTED ON THE PROPERTY.
- A FIFTEEN FOOT (15') BUILDING STEP-BACK SHALL BE PROVIDED ALONG THE WESTERN BOUNDARY OF THE SUBJECT PROPERTY BUT ONLY WHERE ADJACENT TO THE PROPERTY AT 2208 GARDEN PLACE, WHICH PROPERTY IS DESCRIBED IN DEED BOOK 14750 PAGE 1118 (WAKE COUNTY PIN 0794924214).
- A BUILDING STEP-BACK SHALL BE PROVIDED ALONG THE WESTERN BOUNDARY OF THE SUBJECT PROPERTY WHERE ADJACENT TO THE PROPERTY AT 2208 GARDEN PLACE, WHICH PROPERTY IS DESCRIBED IN DEED BOOK 14750 PAGE 1118 (WAKE COUNTY PIN 0794924214). THE STEP-BACK SHALL BE NO LESS THAN FIFTY FEET (50') AS HORIZONTALLY MEASURED FROM THE COMMON PROPERTY LINE OF THE SUBJECT PROPERTY AND THE OTHER PROPERTY REFERENCED IN THE PRECEDING SENTENCE, AND SHALL OCCUR DIRECTLY ABOVE THE THIRD (3RD) STORY OF CONDITIONED SPACE.

**LEGEND**

EXISTING	PROPOSED

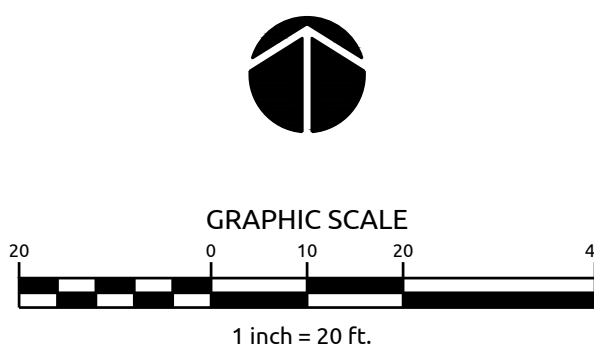
**PRELIMINARY**  
NOT APPROVED FOR CONSTRUCTION





## ZONING CONDITIONS

- IF THE PRIMARY USE OF THE PROPERTY IS NOT RESIDENTIAL, THE MAXIMUM BUILDING HEIGHT SHALL BE NO MORE THAN FOUR (4) STORIES.
- BUILDING FACADES SHALL BE CONTRASTED FROM ONE OR MORE OF THE FOLLOWING MATERIALS: GLASS, CONCRETE AND/OR CLAY BRICK, MASONRY, CEMENTITIOUS STUCCO, NATIVE AND MASONRY STONE, NATURAL WOOD, PRECAST CONCRETE, FRINGED PANEL, GCMET PANEL, AND METAL PANEL AND/OR TRIM.
- THE FOLLOWING EXTERIOR BUILDING MATERIALS SHALL BE PROHIBITED: SYNTHETIC STUCCO SIDING (I.E. EIFS), MASONITE SIDING, VINYL SIDING, AND/OR CEMENTITIOUS LAP SIDING.
- CEMENT PANEL SIDING SHALL BE ALLOWED, BUT JOINTS SHALL BE CHANNEL REVEAL OR SIMILAR DETAIL. NO BATTEN STRIP JOINTS SHALL BE ALLOWED FOR CEMENT PANEL SIDING. CEMENT PANEL SIDING SHALL BE LIMITED TO NO MORE THAN TWENTY-FIVE PERCENT (25%) OF THE BUILDING(S) EXTERIOR SIDING, EXCLUSIVE OF PENETRATION OPENINGS (DOORS, WINDOWS, ETC).
- PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY FOR NEW DEVELOPMENT ON THE SUBJECT PROPERTY, THE OWNER SHALL PROVIDE A PAVED, PUBLIC "GREENWAY CONNECTOR" SIDEWALK ALONG THE FRONTAGES OF ENTERPRISE ST. AND CLARK AVE.
- RESIDENTIAL DWELLING UNIT COUNT SHALL BE LIMITED TO NO MORE THAN SIXTY (60) DWELLING UNITS ON THE SUBJECT PROPERTY.
- NO MORE THAN SIX (6) HVAC UNITS MAY BE LOCATED ON THE GROUND, PROVIDED THAT NO HVAC UNIT SHALL BE LOCATED BETWEEN THE BUILDING FACADE AND A PUBLIC RIGHT OF WAY.
- THE FOLLOWING LAND USES SHALL BE PROHIBITED ON THE SUBJECT PROPERTY: PARKING AS A PRINCIPAL USE, SPORTS ACADEMY, OUTDOOR SPORTS/ENTERTAINMENT FACILITY.
- RETAIL USE(S) LOCATED ON THE SUBJECT PROPERTY SHALL BE LIMITED TO NO MORE THAN FOUR THOUSAND SQUARE FEET (4,000 SF) GROSS FLOOR AREA.
- OFFICE USE(S) SHALL BE LIMITED TO NO MORE THAN NINE THOUSAND FIVE HUNDRED SQUARE FEET (9,500 SF) GROSS FLOOR AREA.
- FOR ANY DWELLING LOCATED ON THE SUBJECT PROPERTY, THE MAXIMUM BEDROOM COUNT IN A SINGLE DWELLING SHALL BE NO MORE THAN THREE (3) BEDROOMS.
- STRUCTURED PARKING SHALL BE SCREENED FROM OFFSITE VIEW, INCLUDING ADJACENT PARCELS AND/OR PUBLIC RIGHT-OF-WAY. SCREENING OF STRUCTURED PARKING SHALL BE NO LESS THAN 75% OPAQUE FOR ANY OPENING FACING ADJACENT PARCELS/PUBLIC RIGHT-OF-WAY OTHER THAN OPENING UTILIZED FOR VEHICULAR ACCESS POINT.
- STRUCTURED PARKING SCREENING SHALL INCLUDE, BUT IS NOT LIMITED TO: PIERCED-BRICK WALL, METAL LATH SCREEN, AND SIMILAR ARCHITECTURAL ELEMENTS. EVERGREEN PLANTINGS MAY ALSO BE USED TO SCREEN STRUCTURED PARKING, BUT SUCH PLANTINGS, TOGETHER WITH ANY OTHER SCREENING MATERIAL, SHALL MEET THE 75% OPAQUE REQUIREMENT AT TIME OF INITIAL INSTALLATION OF SAME.
- VEHICLE ACCESS OPENINGS TO STRUCTURED PARKING FACING A PUBLIC RIGHT-OF-WAY SHALL BE SCREENED WITH AN ARCHITECTURAL ELEMENT (I.E. GATE, DOOR, ROLL-UP SCREEN) TO PROVIDE NO LESS THAN FIFTY PERCENT (50%) OPAQUE.
- THE AVERAGE HEIGHT OF THE BUILDING MEASURED AT ENTERPRISE STREET (EAST ELEVATION) AND AT CLARK AVENUE (NORTH ELEVATION) SHALL BE NO GREATER THAN SIXTY-FIVE FEET (65') FROM FINISHED GRADE TO THE PEAK OF THE ROOF FRAMING.
- NON-RESIDENTIAL USES SHALL HAVE DIRECT ADJACENCY AND PRIMARY ACCESS TO EITHER ENTERPRISE ST. AND/OR CLARK AVE.
- NON-RESIDENTIAL USES SHALL NOT EXTEND MORE THAN FORTY FEET (40') ADJACENT GARDEN PLACE, ENTERPRISE STREET, AND CLARK AVENUE SHALL BE NO LESS THAN THE FOLLOWING: GROUND FLOOR: 30% FOR "APARTMENT" BUILDING TYPE; AND UPPER STORY(S): 35% FOR "APARTMENT" BUILDING TYPE.
- IF A FIVE-STORY BUILDING IS CONSTRUCTED ON THE SUBJECT PROPERTY, THE TRASH AND RECYCLE COLLECTION FACILITIES SHALL BE LOCATED WITHIN THE BUILDING OR AN ENCLOSED SERVICE COURTYARD.
- AT THE FIFTH STORY OF A BUILDING, A BUILDING STEP-BACK OF NO LESS THAN 8 FT. FROM THE AVERAGE FACADE PLANE SHALL BE PROVIDED ON THE GARDEN PLACE ELEVATION (SOUTH ELEVATION). THE STEP-BACK SHALL BE NO LESS THAN SIXTY-FIVE PERCENT (65%) OF THE OVERALL FIFTH (5<sup>TH</sup>) STORY FACADE.
- IF A FIVE (5) STORY RESIDENTIAL BUILDING IS CONSTRUCTED ON THE PROPERTY, IT SHALL BE COMPRISSED OF STRUCTURAL STEEL AND/OR CONCRETE CONSTRUCTION (I.E., NOT STRUCTURAL WOOD). THIS CONDITION SHALL ONLY APPLY IF A FIVE (5) STORY RESIDENTIAL BUILDING IS CONSTRUCTED ON THE PROPERTY.
- A FIFTEEN FOOT (15') BUILDING SETBACK SHALL BE PROVIDED ALONG THE WESTERN BOUNDARY OF THE SUBJECT PROPERTY, BUT ONLY WHERE ADJACENT TO THE PROPERTY AT 2208 GARDEN PLACE, WHICH PROPERTY IS DESCRIBED IN DEED BOOK 1750 PAGE 118 (WAKE COUNTY PIN 074042414).
- A BUILDING STEP-BACK SHALL BE PROVIDED ALONG THE WESTERN BOUNDARY OF THE SUBJECT PROPERTY WHERE ADJACENT TO THE PROPERTY AT 2208 GARDEN PLACE, WHICH PROPERTY IS DESCRIBED IN DEED BOOK 1750 PAGE 118 (WAKE COUNTY PIN 074042414). THE STEP-BACK SHALL BE NO LESS THAN FIFTY FEET (50') AS HORIZONTALLY MEASURED FROM THE COMMON PROPERTY LINE OF THE SUBJECT PROPERTY AND THE OTHER PROPERTY REFERENCED IN THE PRECEDING SENTENCE, AND SHALL OCCUR DIRECTLY ABOVE THE THIRD (3RD) STORY OF CONDITIONED SPACE.



## EXISTING SITE DATA:

SITE ADDRESSES: 2204 GARDEN PLACE, 101 ENTERPRISE STREET, 2303 CLARK AVENUE, RALEIGH, NC 27607

PINS: 0794-92-4283, 0794-92-5270, 0794-92-5371

DEED BOOK: DB 013475 PG 00843, DB 013475 PG 00833, DB 001882 PG 00361

ZONING(S): OX-5-UL-CU

OVERLAY DISTRICT: SRPOD - SPECIAL RESIDENTIAL PARKING OVERLAY DISTRICT

GROSS ACREAGE: 0.82 AC

PROPOSED RW DEDICATION: 0.10 AC

NET ACREAGE: 0.72 AC

EXISTING USE: SINGLE FAMILY & COMMERCIAL

EXISTING IMPERVIOUS: 0.389 AC

FLOOD HAZARD AREA: NO

FEMA MAP: MAP # 372079400J, PANEL 0794, EFFECTIVE DATE 05/02/2006

ALLUVIAL SOILS: N/A

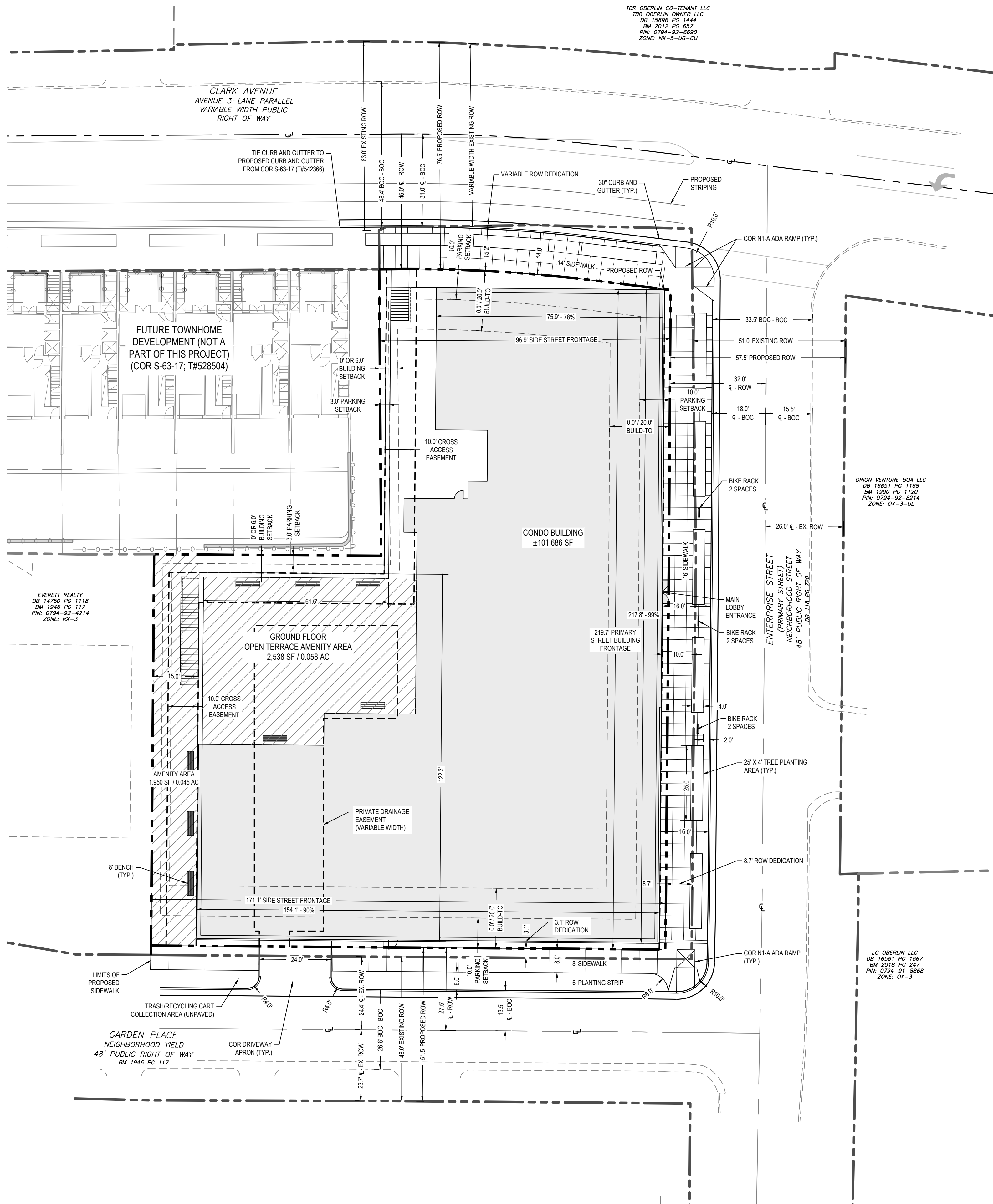
## EXISTING CONDITIONS AND DEMOLITION NOTES:

- ALL WORK SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL COORDINATE ALL REMOVAL EFFORTS WITH THE OWNER PRIOR TO COMMENCING WORK.
- SPECIAL CARE SHALL BE TAKEN TO PROTECT AND MAINTAIN ALL EXISTING FEATURES, INCLUDING BELOW GROUND, NOT MARKED FOR REMOVAL. IN THE EVENT OF ANY IMPACT TO SUCH FEATURES, THE CONTRACTOR SHALL PERFORM REPAIR AND/OR RESTORE TO ORIGINAL CONDITION AS OF START OF WORK.
- NO DEMOLITION DEBRIS IS TO BE STORED ON SITE. ALL DEMOLITION MATERIAL TO BE PROMPTLY REMOVED FROM THE SITE OR PLACED IN APPROPRIATE CONTAINERS.
- TRAFFIC CONTROL IS CONTRACTOR'S RESPONSIBILITY.
- SAWCUT EXISTING ASPHALT AT LOCATION SHOWN ON PLANS.
- SITE DEMOLITION SEQUENCE TO BE PROVIDED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN A RIGHT-OF-WAY PERMIT FOR ANY WORK WHICH REQUIRES THE CLOSURE OF A TRAVEL LANE(S) AND/OR SIDEWALK FROM THE TRANSPORTATION OPERATION DIVISION/PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL OBTAIN A TREE IMPACT PERMIT TO REMOVE ANY TREES EXISTING IN THE STREET RIGHTS-OF-WAY.
- ALL SIDEWALKS MUST BE ACCESSIBLE TO PERSONS WHO ARE BLIND, HAVE LOW VISION AND PEOPLE WITH MOBILITY DISABILITIES. PEDESTRIAN EXISTING ROUTES AND ALTERNATE PEDESTRIAN ROUTES DURING CONSTRUCTION WILL BE REQUIRED TO BE COMPLIANT WITH THE PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (POWAG), 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- IF UNFORESEEN CONDITIONS DEVELOP DURING CONSTRUCTION, REFER TO "CITY OF RALEIGH STREET DESIGN MANUAL" AND CONTACT PUBLIC WORKS DEPARTMENT FOR FURTHER GUIDANCE.
- PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY OF RALEIGH PUBLIC WORKS DEPARTMENT / TRANSPORTATION FIELD SERVICES STAFF TO REVIEW THE SPECIFIC COMPONENTS OF THE PLAN AND OPERATION OF THESE FACILITIES DURING CONSTRUCTION. CONTACT ENGINEERING INSPECTORS AT 919-996-2429 TO SET UP THE MEETING.
- THE CONTRACTOR SHALL CONDUCT THE WORK IN A SAFE MANNER AND WITH A MINIMUM AMOUNT OF INCONVENIENCE TO TRAFFIC.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SHALL ADHERE TO THE PROVISIONS OF THE MUTCD (MOST CURRENT EDITION).
- ANY TEMPORARY STREET CLOSURE MUST BE SUBMITTED A MINIMUM OF 2 WEEKS PRIOR TO THE START OF WORK. SEE "RIGHT-OF-WAY CLOSURES" ON WWW.RALEIGHNC.GOV. COMPLETE AND SEND THE RIGHT-OF-WAY OBSTRUCTION PERMIT TO RIGHTOFWAYSERVICES@RALEIGHNC.GOV.
- PRIOR TO CONSTRUCTION BEGINNING, ALL SIGNAGE AND TRAFFIC CONTROL SHALL BE IN PLACE.
- CONTRACTOR IS EXPLICITLY ADVISED TO FIELD LOCATE AND VERIFY LOCATION OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. LOCATION OF EXISTING UNDERGROUND UTILITIES SHOWN ON PLANS IS APPROXIMATE BASED ON BEST AVAILABLE INFORMATION AND SHOULD BE CONSIDERED APPROXIMATE.
- CONTRACTOR TO REPORT ANY DISCREPANCIES TO LANDSCAPE ARCHITECT/CIVIL ENGINEER PRIOR TO ANY CONSTRUCTION ACTIVITIES. CONTACT NC ONE AT 811 FOR FIELD LOCATION OF UNDERGROUND UTILITIES.
- ALL SURVEY INFORMATION PROVIDED BY WITHERSRAVENEL, CARY, NORTH CAROLINA, OFFICE IN DIGITAL FORMAT IN DECEMBER, 2015, AND SUPPLEMENTAL INFORMATION WAS OBTAINED FROM WAKE COUNTY GIS IN MARCH 2016.

## LEGEND:

- |    |                                 |
|----|---------------------------------|
| PP | EX. FIRE HYDRANT                |
| PP | EX. POWER POLE                  |
| PP | IRON PIPE SET                   |
| PP | IRON PIPE FOUND                 |
| PP | CALCULATED POINT                |
| PP | EX. WATER VALVE                 |
| PP | EX. CATCH BASIN                 |
| PP | EX. SANITARY MANHOLE            |
| PP | PROJECT BOUNDARY LINE           |
| PP | ADJACENT PROPERTY LINE          |
| PP | EX. RIGHT OF WAY                |
| PP | EX. OVERHEAD ELECTRIC LINE      |
| PP | EX. FIBER OPTIC LINE            |
| PP | EX. CABLE LINE                  |
| PP | EX. PHONE LINE                  |
| PP | EX. GAS LINE                    |
| PP | EX. SANITARY SEWER LINE         |
| PP | EX. STORM DRAINAGE LINE         |
| PP | EX. WATER LINE                  |
| PP | EX. UNKNOWN UNDERGROUND UTILITY |
| PP | EX. MAJOR CONTOUR LINE          |
| PP | EX. MINOR CONTOUR LINE          |

K:\17\17-00001\_170508 Clark Condos CAD\Administrative\Site Review\22.0 SITE PLAN.dwg Thursday, September 27, 2018 11:56:59 AM C:\AUL\E\_R08



ALL WORK WITHIN PUBLIC RIGHT-OF-WAY SHALL CONFORM WITH NCDOT STANDARD SPECIFICATIONS AND DETAILS.

GENERAL NOTES:

- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH AND NCDOT STANDARDS AND SPECIFICATIONS IF APPLICABLE.
- THE CONTRACTOR SHALL OBTAIN A RIGHT-OF-WAY PERMIT FOR ANY WORK WHICH REQUIRES THE CLOSURE OF A TRAVEL LANE(S), PARKING SPACE, OR SIDEWALK FROM RIGHT-OF-WAY SERVICES AT LEAST 48 HOURS IN ADVANCE.
- IF CONSTRUCTION PLANS FOR PUBLIC AND PRIVATE STREETS OR UTILITIES SHOWN ON THIS PLAN ARE REQUIRED, THEY MUST BE APPROVED BY THE PUBLIC WORKS DEPARTMENT AND PUBLIC UTILITIES DEPARTMENT PRIOR TO ISSUANCE OF PERMITS OR RECORDING OF ANY PLAT FOR THIS DEVELOPMENT.
- FIELD ADJUSTMENTS TO THIS PLAN MAY BE REQUIRED BY CITY OF RALEIGH INSPECTOR AS NEEDED DURING CONSTRUCTION.
- ALL SURVEY INFORMATION PROVIDED TO WITHERSRAVENEL, RALEIGH, NORTH CAROLINA OFFICE BY WITHERSRAVENEL, CARY, NORTH CAROLINA OFFICE IN DIGITAL FORMAT IN FEBRUARY, 2016, AND SUPPLEMENTAL INFORMATION WAS OBTAINED FROM WAKE COUNTY GIS IN FEBRUARY, 2016.
- ALL PROPOSED CURB AND GUTTER WITHIN PUBLIC RIGHT OF WAY SHOWN ON PLANS TO BE 30" CITY OF RALEIGH STANDARD CONCRETE CURB AND GUTTER, AND ALL OTHER PROPOSED CURB AND GUTTER TO BE 24" CONCRETE CURB AND GUTTER UNLESS OTHERWISE STATED ON PLANS.
- ALL DIMENSIONS SHOWN ARE TO BACK TO CURB, UNLESS OTHERWISE STATED ON PLANS.
- WITHIN THE SIGHT TRIANGLES SHOWN ON THIS PLAN, NO OBSTRUCTION BETWEEN 2 FEET AND 8 FEET IN HEIGHT ABOVE THE CURB LINE ELEVATION SHALL BE LOCATED IN WHOLE OR PART. OBSTRUCTIONS INCLUDE BUT ARE NOT LIMITED TO ANY BERM, FOLIAGE, FENCE, WALL, SIGN, OR PARKED VEHICLE.
- UNLESS NOTED, ACCESS ROUTE FOR EMERGENCY VEHICLES SHALL PROVIDE AN INSIDE TURNING RADIUS OF 28' MINIMUM.
- TRASH AND CARDBOARD DUMPSTER(S) ENCLOSURE SHALL COMPATIBLE WITH MATERIAL AND/OR COLOR OF THE PRINCIPAL BUILDING.
- ALL HVAC UNITS SHALL BE SCREENED FROM VIEW OF THE PUBLIC RIGHT OF WAY.
- CONTRACTOR TO FIELD LOCATE AND VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND REPORT ANY DISCREPANCIES TO LANDSCAPE ARCHITECT PRIOR TO ANY CONSTRUCTION ACTIVITIES. CONTACT NC ONE AT 811 FOR FIELD LOCATION OF UNDERGROUND UTILITIES.
- HANDICAP PARKING SPACE(S) AND HC ACCESS AISLE(S) SHALL BE NO GREATER THAN TWO PERCENT (2%) PITCH IN ANY DIRECTION(S) AS PER ADA STANDARDS.
- PROVIDE SIGNAGE AND STRIPING OF HANDICAP SPACES AS PER ADA STANDARDS.
- ALL RETAINING WALLS GREATER THAN 30" IN HEIGHT TO INCLUDE SAFETY RAIL OR FENCE. NO RETAINING WALLS ARE PERMITTED IN THE RIGHT-OF-WAY UNLESS APPROVED BY ENCROACHMENT.
- THE MINIMUM CORNER CLEARANCE FROM THE CURB LINE OF INTERSECTING STREETS SHALL BE AT LEAST 50 FEET FROM THE POINT OF TANGENCY OF THE CURB FOR MIXED-USE/COMMERCIAL DRIVEWAY. NO DRIVEWAYS SHALL ENCROACH ON THIS MINIMUM CORNER CLEARANCE.
- WC ACCESS RAMPS WILL BE PROVIDED IN ACCORDANCE WITH CITY OF RALEIGH PUBLIC WORKS DEPARTMENT STANDARDS, PROWAG STANDARDS AND ADAAG SPECIFICATIONS.
- ALL RAMPS AND HANDRAILS SHALL BE CONFORM TO ANSI STANDARDS.
- ALL ABOVE GROUND UTILITY DEVICES (TO INCLUDE BUT NOT LIMITED TO TELEPHONE AND CABLE PEDESTALS, ELECTRICAL TRANSFORMERS, BACKFLOW DEVICE HOTBOX, ETC) SHALL BE SCREENED FROM OFF-SITE VIEW BY EVERGREEN SHRUBS, FENCE, OR WALL.
- ALL SIDEWALKS MUST BE ACCESSIBLE TO PERSONS WHO ARE BLIND, HAVE LOW VISION AND PEOPLE WITH MOBILITY DISABILITIES. PEDESTRIAN EXISTING ROUTES AND ALTERNATE PEDESTRIAN ROUTES DURING CONSTRUCTION WILL BE REQUIRED TO BE COMPLIANT WITH THE PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG), 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- IF UNFORESEEN CONDITIONS DEVELOP DURING CONSTRUCTION, REFER TO "CITY OF RALEIGH STREET DESIGN MANUAL" AND CONTACT PUBLIC WORKS DEPARTMENT FOR FURTHER GUIDANCE.
- PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH THE CITY OF RALEIGH PUBLIC WORKS DEPARTMENT / TRANSPORTATION FIELD SERVICES STAFF TO REVIEW THE SPECIFIC COMPONENTS OF THE PLAN AND OPERATION OF THESE FACILITIES DURING CONSTRUCTION. CONTACT ENGINEERING INSPECTIONS AT 919.996.2409 TO SET UP THE MEETING.
- THE CONTRACTOR SHALL CONDUCT THE WORK IN A SAFE MANNER AND WITH A MINIMUM AMOUNT OF INCONVENIENCE TO TRAFFIC.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SHALL ADHERE TO THE PROVISIONS OF THE MUTCD (MOST CURRENT EDITION).
- ANY TEMPORARY STREET CLOSURE MUST BE SUBMITTED A MINIMUM OF 2 WEEKS PRIOR TO THE START OF WORK. SEE "RIGHT-OF-WAY CLOSURES" ON [WWW.RALEIGHNC.GOV](http://WWW.RALEIGHNC.GOV). COMPLETE AND SEND THE RIGHT-OF-WAY OBSTRUCTION PERMIT TO [RIGHTOFWAYSERVICES@RALEIGHNC.GOV](mailto:RIGHTOFWAYSERVICES@RALEIGHNC.GOV).
- PRIOR TO CONSTRUCTION BEGINNING, ALL SIGNAGE AND TRAFFIC CONTROL SHALL BE IN PLACE.

ROW DEDICATION 4,167 SF / 0.10 AC  
LOT 1 NEW ACREAGE 31,319 SF / 0.72 AC

LEGEND:



AMENITY AREA

AMENITY AREA

AMENITY AREA PROVIDED: 0.045 AC (SHOWN) + 0.058 AC (OPEN TERRACE) = 0.103 AC (TOTAL)  
TOTAL LOT SIZE: 0.719 AC  
0.103AC / 0.719 AC = 14.3%

PLAZA TREE & SEATING CALCULATIONS

AMENITY AREA REQUIRED: 31,319 SF \* 0.10 = 3132 SF

REQUIRED

SEATING REQUIRED: 3132 SF / 50 = 63 LF SEATING  
TREES REQUIRED: 3132 SF / 1000 = 3.1 TREES

PROVIDED

SEATING PROVIDED: EIGHT 8' BENCHES (64 LF)  
TREES PROVIDED: 3 (SEE SHEET L1.0)

BICYCLE PARKING REQUIREMENT

RESIDENTIAL (MULTI-UNIT LIVING)

REQUIRED

1 SPACE PER 20 UNITS, MIN. 4  
51 UNITS X 1 SPACE / 20 UNITS = 3 SPACES

PROVIDED

4 SPACES

BUILD-TO CALCULATIONS

REQUIRED

PRIMARY STREET: 50% MIN.  
SIDE STREET: 25% MIN.

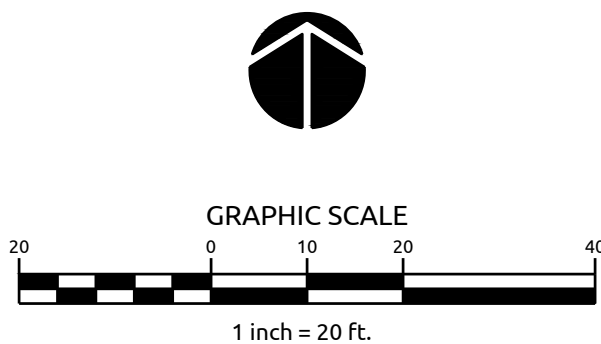
PROVIDED

PRIMARY STREET (ENTERPRISE ST.): 217.8' / 219.7' = 99%  
SIDE STREET (CLARK AVE.): 75.9' / 96.9' = 78%  
SIDE STREET (GARDEN PL.): 154.1' / 171.1' = 90%

NOTE:

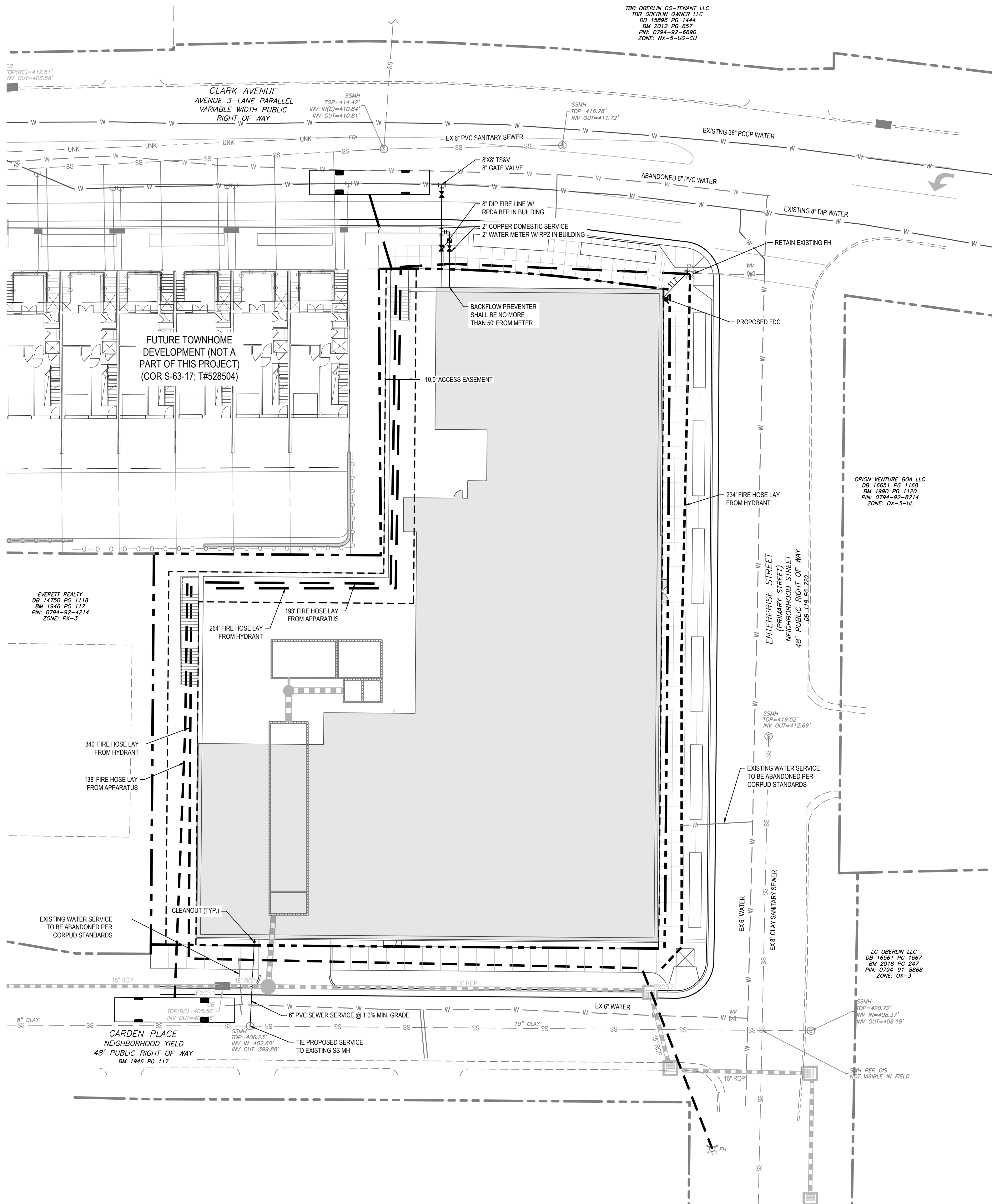
SITE IS TO BE RESIDENTIAL ONLY, 51 CONDO UNITS ARE PROPOSED.

SEE SHEET A101 FOR PARKING CALCULATIONS





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- STANDARD UTILITY NOTES (AS APPLICABLE):**
1. ALL MATERIALS & CONSTRUCTION METHODS SHALL BE IN ACCORDANCE WITH CITY OF RALEIGH DESIGN STANDARDS, DETAILS & SPECIFICATIONS (REFERENCE: CORPUS HANDBOOK CURRENT EDITION)
  2. UTILITY SEPARATION REQUIREMENTS
    - a) A DISTANCE OF 10' SHALL BE MAINTAINED BETWEEN SANITARY SEWER & ANY PRIVATE OR PUBLIC WATER SUPPLY SOURCE SUCH AS AN IMPOUNDED RESERVOIR USED AS A SOURCE OF DRINKING WATER. IF ADEQUATE LATERAL SEPARATION CANNOT BE ACHIEVED, FERROUS SANITARY SEWER PIPE SHALL BE SPECIFIED & INSTALLED TO WATERLINE SPECIFICATIONS. HOWEVER, THE MINIMUM SEPARATION SHALL NOT BE LESS THAN 2' FROM A PRIVATE WELL OR 5' FROM A PUBLIC WELL.
    - b) WHEN INSTALLING WATER &/OR SEWER MAINS, THE HORIZONTAL SEPARATION BETWEEN UTILITIES SHALL BE 10'. IF THIS SEPARATION CANNOT BE MAINTAINED DUE TO EXISTING CONDITIONS, THE VARIATION ALLOWED IS THE WATER MAIN IN A SEPARATE TRENCH WITH THE ELEVATION OF THE WATER MAIN AT LEAST 18" ABOVE THE TOP OF THE SEWER & MUST BE APPROVED BY THE PUBLIC UTILITIES DIRECTOR.
    - c) ALL DISTANCES ARE MEASURED FROM OUTSIDE DIAMETER TO OUTSIDE DIAMETER.
    - d) WHERE IT IS IMPOSSIBLE TO OBTAIN PROPER SEPARATION, OR ANYTIME A SANITARY SEWER PASSES OVER A WATERMAIN, DIP MATERIALS OR STEEL ENCASUREMENT EXTENDED 12" ON EACH SIDE OF CROSSING MUST BE SPECIFIED & INSTALLED TO WATERLINE SPECIFICATIONS.
    - e) 5.0' MINIMUM HORIZONTAL SEPARATION IS REQUIRED BETWEEN ALL SANITARY SEWER & STORM SEWER FACILITIES, UNLESS DIP MATERIAL IS SPECIFIED FOR SANITARY SEWER.
    - f) MAINTAIN 18" MIN. VERTICAL SEPARATION AT ALL WATERMAIN & RCP STORM DRAIN CROSSINGS; MAINTAIN 24" MIN. VERTICAL SEPARATION AT ALL SANITARY SEWER & RCP STORM DRAIN CROSSINGS. WHERE ADEQUATE SEPARATIONS CANNOT BE ACHIEVED, SPECIFY DIP MATERIALS & A CONCRETE CRADLE HAVING 8" MIN. CLEARANCE (PER CORPUS DETAILS 9-4.1 & 9-4.2).
    - g) ALL OTHER UNDERGROUND UTILITIES SHALL CROSS WATER & SEWER FACILITIES WITH 18" MIN. VERTICAL SEPARATION REQUIRED.
  3. ANY NECESSARY FIELD REVISIONS ARE SUBJECT TO REVIEW & APPROVAL OF AN AMENDED PLAN &/OR PROFILE BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT PRIOR TO CONSTRUCTION.
  4. CONTRACTOR SHALL MAINTAIN CONTINUOUS WATER & SEWER SERVICE TO EXISTING RESIDENCES & BUSINESSES THROUGHOUT CONSTRUCTION OF PROJECT. ANY NECESSARY SERVICE INTERRUPTIONS SHALL BE PRECEDED BY A 24 HOUR ADVANCE NOTICE TO THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT.
  5. 3.0' MINIMUM COVER IS REQUIRED ON ALL WATER MAINS & SEWER FORCEMAINS. 4.0' MINIMUM COVER IS REQUIRED ON ALL REUSE MAINS.
  6. IT IS THE DEVELOPER'S RESPONSIBILITY TO ABANDON OR REMOVE EXISTING WATER & SEWER SERVICES NOT BEING USED IN REDEVELOPMENT OF A SITE UNLESS OTHERWISE DIRECTED BY THE CITY OF RALEIGH PUBLIC UTILITIES DEPARTMENT. THIS INCLUDES ABANDONING TAP AT MAIN & REMOVAL OF SERVICE FROM ROW OR EASEMENT PER CORPUS HANDBOOK PROCEDURE.
  7. INSTALL 2" COPPER WATER SERVICES WITH METERS LOCATED AT ROW OR WITHIN A 2X2' WATERLINE EASEMENT IMMEDIATELY ADJACENT. **NOTE: IF (5) THE APPLICANT'S RESPONSIBILITY TO PROPERLY SIZE THE WATER SERVICE FOR EACH CONNECTION TO PROVIDE ADEQUATE FLOW & PRESSURE.**
  8. INSTALL 6" PVC SEWER SERVICES @ 0.5% MINIMUM GRADE WITH CLEANOUTS LOCATED AT ROW OR EASEMENT LINE & SPACED EVERY 75 LINEAR FEET MAXIMUM.
  9. PRESSURE REDUCING VALVES ARE REQUIRED ON ALL WATER SERVICES EXCEEDING 80 PSI. BACKWATER VALVES ARE REQUIRED ON ALL SANITARY SEWER SERVICES HAVING BUILDING DRAINS LOWER THAN 12' ABOVE THE NEXT UPSTREAM MANHOLE.
  10. ALL ENVIRONMENTAL PERMITS APPLICABLE TO THE PROJECT MUST BE OBTAINED FROM NCOW, USACE &/OR FEMA FOR ANY RIPARIAN BUFFER, WETLAND &/OR FLOODPLAIN IMPACTS (RESPECTIVELY) PRIOR TO CONSTRUCTION.
  11. NCDOT / RAILROAD ENCROACHMENT AGREEMENTS ARE REQUIRED FOR ANY UTILITY WORK (INCLUDING MAIN EXTENSIONS & SERVICE TAPS) WITHIN STATE OR RAILROAD ROW PRIOR TO CONSTRUCTION.
  12. GREASE INTERCEPTOR / OIL WATER SEPARATOR SIZING CALCULATIONS & INSTALLATION SPECIFICATIONS SHALL BE APPROVED BY THE CORPUS FOG PROGRAM COORDINATOR PRIOR TO ISSUANCE OF A BUILDING PERMIT. CONTACT TIM BEASLEY AT (919) 986-2334 OR TIMOTHY.BEASLEY@RALEIGHNC.GOV FOR MORE INFORMATION.
  13. CROSS-CONNECTION CONTROL PROTECTION DEVICES ARE REQUIRED BASED ON DEGREE OF HEALTH HAZARD INVOLVED AS LISTED IN APPENDIX B OF THE RULES GOVERNING PUBLIC WATER SYSTEMS IN NORTH CAROLINA. THESE GUIDELINES ARE THE MINIMUM REQUIREMENTS. THE DEVICES SHALL MEET AMERICAN SOCIETY OF SANITARY ENGINEERING (ASSE) STANDARDS OR BE ON THE UNIVERSITY OF SOUTHERN CALIFORNIA APPROVAL LIST. THE DEVICES SHALL BE INSTALLED AND TESTED (BOTH INITIAL & PERIODIC TESTING THEREAFTER) IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS OR THE LOCAL CROSS-CONNECTION CONTROL PROGRAM, WHICHEVER IS MORE STRINGENT. CONTACT JOANIE HARTLEY AT (919) 986-6923 OR JOANIE.HARTLEY@RALEIGHNC.GOV FOR MORE INFORMATION.

**NOTE:**  
ALL PRIOR EXISTING WATER AND SEWER TAPS SERVING THE REDEVELOPMENT SITE SHALL BE ABANDONED AT MAIN AND SERVICE, REMOVED FROM ROW AND EASEMENT PER CORPUS HANDBOOK PROCEDURE.

**NOTE:** BUILDING IS PROPOSED TO HAVE A 13R SPRINKLER SYSTEM.

WithersRavenel

Engineers | Planners

CLARK AVENUE CONDOS

UTILITY AND FIRE ACCESS PLAN

Job No. 02170508.00

Date 04/11/18

Drawn By WR

Designer WR

PRELIMINARY

NOT APPROVED FOR CONSTRUCTION

Revisions

1 COR COMMENTS 08/27/18

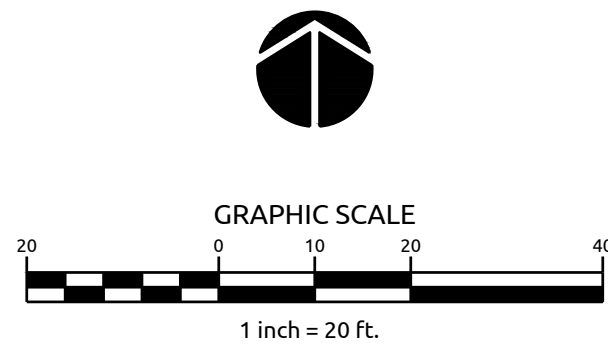
2 COR COMMENTS 09/21/18

Sheet No.

C4.0

RALEIGH, NC

1307 S Wilmington Street | Suite 200 | Raleigh, NC 27601 | t: 919-469-3940 | license #: C-0682 | www.withersravenel.com

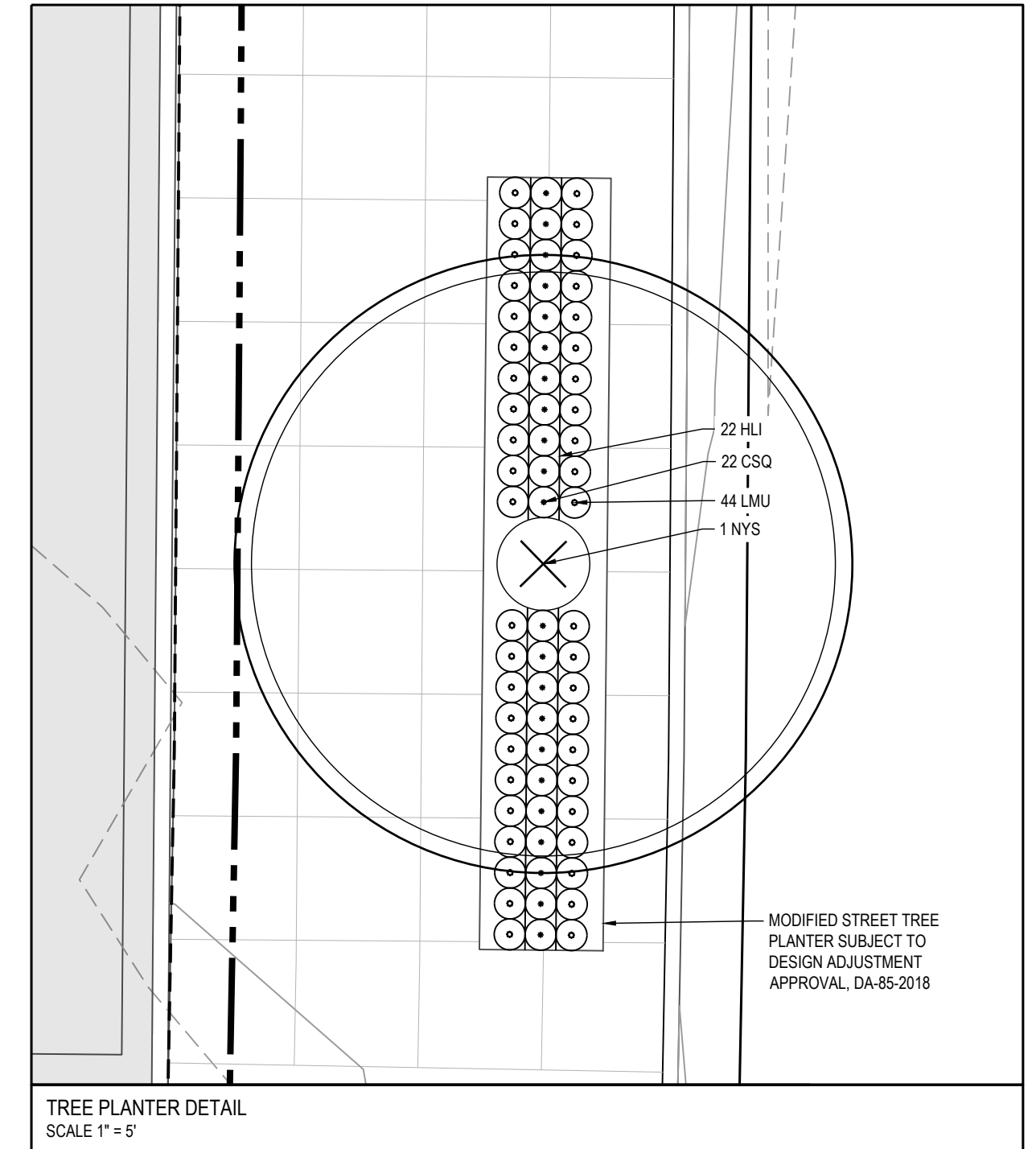


### GENERAL LANDSCAPE NOTES:

1. THIS PLAN IS FOR PLANTING PURPOSES ONLY. FOR INFORMATION REGARDING GRADING, WALL, ETC., REFER TO THE SITE AND GRADING PLANS.
2. ALL CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF RALEIGH STANDARDS AND SPECIFICATIONS AND NCDOT, IF APPLICABLE.
3. NO CHANGES TO ANY ASPECT OF THIS SITE PLAN, INCLUDING BUT NOT LIMITED TO LANDSCAPING, GRADING, BUILDING ELEVATIONS (ESPECIALLY COLOR), LIGHTING, OR UTILITIES WILL BE MADE WITHOUT THE APPROVAL OF THE GOVERNING MUNICIPALITY.
4. THE SITE SHALL BE STABILIZED AND SEEDED PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY (CO).
5. ALL ABOVE-GROUND HVAC EQUIPMENT AND UTILITY DEVICES (TO INCLUDE, BUT NOT LIMITED TO TELEPHONE AND CABLE PEDESTALS, ELECTRICAL TRANSFORMERS, BACKFLOW-DEVICE HOTBOX, ETC) SHALL BE SCREENED FROM OFF-SITE VIEW BY EVERGREEN SHRUBS, FENCE, OR WALL.
6. ALL PLANTS AND PLANTING PROCEDURES SHALL MEET OR EXCEED A.N.I.A. STANDARDS AS SET FORTH IN "AMERICAN STANDARD FOR NURSERY STOCK", 2004 OR LATER EDITION.
7. ALL TREES AND SHRUB SHALL BE FULL, WELL-BRANCHED PLANTS WHICH ARE CHARACTERISTIC TO THE SPECIES.
8. ALL PLANT BEDS TO BE MULCHED WITH A MINIMUM OF 3" DOUBLE SHREDDED HARDWOOD MULCH, DYED BROWN; 2" MAXIMUM PARTICLE SIZE. NO PINE STRAW PERMITTED. ALL PLANT GROUPING SHALL BE MULCHED AS ONE BED.
9. APPLY PER-EMERGENT HERBICIDE TO ALL NEW PLANTING BEDS AT MANUFACTURER'S RECOMMENDATIONS PRIOR TO INSTALLATION OF ANY PLANT MATERIAL.
10. TOPSOIL SHALL BE SPREAD OVER THE ENTIRE LANDSCAPE AREA TO A DEPTH OF AT LEAST 4 INCHES IN LAWN AREAS AND 6 INCHES IN SHRUB AND GROUND COVER BEDS.
11. FINAL LOCATION OF ALL TURF AREAS SHALL BE APPROVED OWNER OR LANDSCAPE ARCHITECT.
12. LOCATION OF PLANT MATERIAL TO BE REVIEWED BY LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
13. VERIFICATION OF PLANT QUANTITIES SHOWN ON THIS PLAN SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR SHALL VERIFY ALL EXISTING AND PROPOSED UTILITIES PRIOR TO INSTALLATION OF ANY PLANT MATERIAL. IF CONFLICTS OCCUR, PLANT LOCATIONS SHOULD BE ADJUSTED AND APPROVED BY OWNER AND/OR LANDSCAPE ARCHITECT BEFORE ANY HOLE IS DUG.
15. WITHIN THE SIGHT TRIANGLES SHOWN ON THIS PLAN, NO OBSTRUCTION BETWEEN 2 FEET AND 8 FEET IN HEIGHT ABOVE THE CURB LINE ELEVATION SHALL BE LOCATED IN WHOLE OR PART. OBSTRUCTIONS INCLUDE BUT ARE NOT LIMITED TO ANY BERM, FOLIAGE, FENCE, WALL, SIGN, OR PARKED VEHICLE.

**NOTE:**

1. ALL HVAC IS LOCATED ON THE ROOF AND SCREENED FROM VIEW BY A PARAPET WALL.
2. O.V.H. ELECTRIC LINES EXIST ON GARDEN PLANE.
3. THE CITY OF RALEIGH WILL NOT BE RESPONSIBLE FOR MAINTENANCE NOR REPLACEMENT OF GROUND COVER PLANTS IN RIGHT-OF-WAY.
4. TREES TO BE PLANTED ACCORDING TO PRCR-03 CITY OF RALEIGH DETAIL AND ADDITIONALLY PRCR-07 CITY OF RALEIGH DETAIL FOR TREES IN TREE PLANTERS.







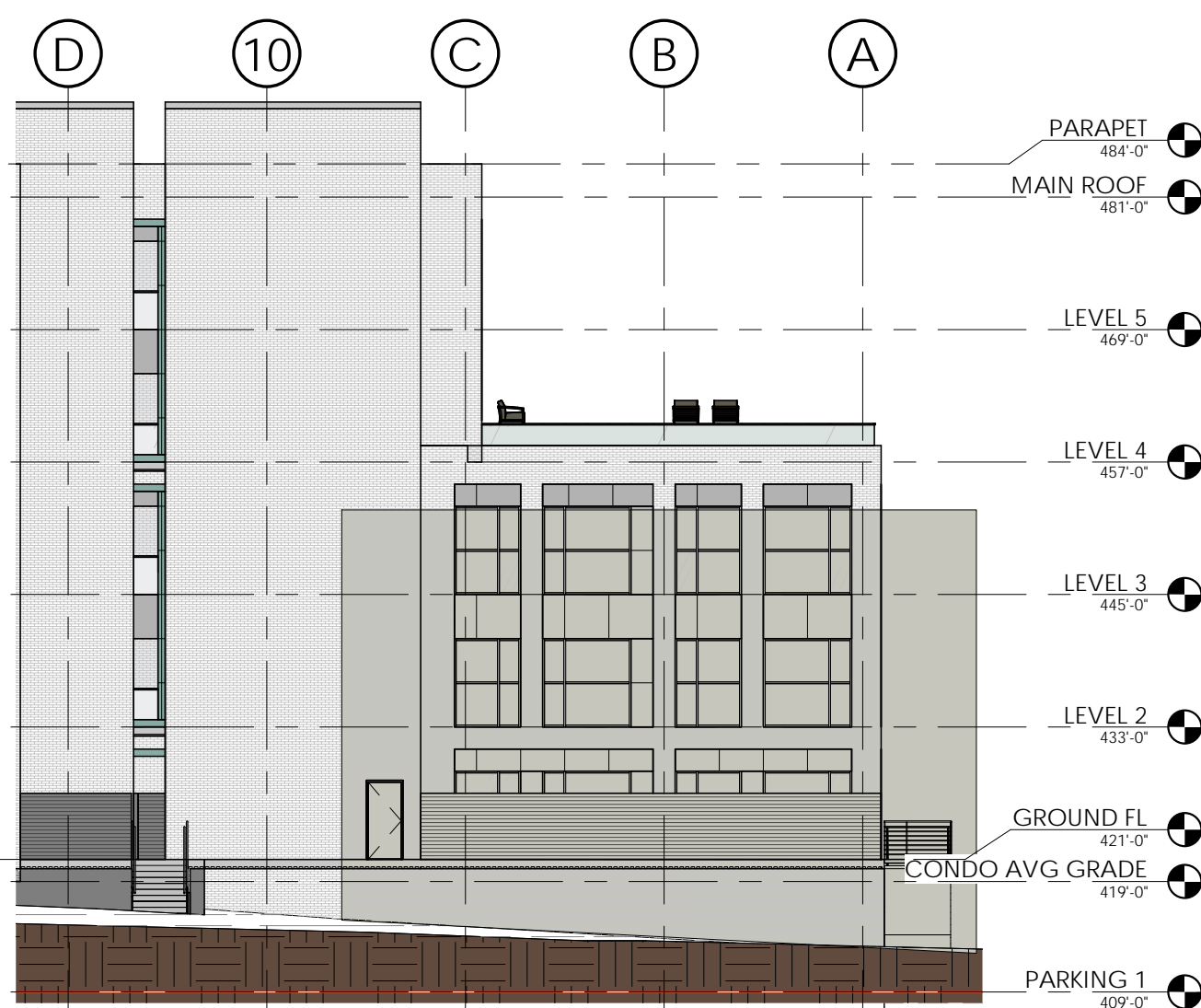
BUILDING ELEVATION - WEST 2  
1/16" = 1'-0" ⑥



BUILDING ELEVATION - WEST 1  
1/16" = 1'-0" ⑤



BUILDING ELEVATION - NORTH (CLARK AVE)  
1/16" = 1'-0" ④



BUILDING ELEVATION - NORTH 2  
1/16" = 1'-0" ③



BUILDING ELEVATION - SOUTH (GARDEN PL)  
1/16" = 1'-0" ②

TRANSPARENCY CALCULATIONS	
ENTERPRISE STREET ELEVATION	
GROUND FLOOR:	
TOTAL AREA:	2,611 SF
TRANSPARENT AREA:	1,343 SF = 53% > 35% MIN REQ'D
FLOORS 2-5:	
TOTAL AREA:	11,086 SF
TRANSPARENT AREA:	5,036 SF = 45% > 35% MIN REQ'D
LONGEST BLANK WALL:	4' < 35' MAX ALLOWED
CLARK AVENUE ELEVATION	
GROUND FLOOR:	
TOTAL AREA:	902 SF
TRANSPARENT AREA:	448 SF = 50% > 35% MIN REQ'D
FLOORS 2-5:	
TOTAL AREA:	3,834 SF
TRANSPARENT AREA:	1,448 SF = 38% > 35% MIN REQ'D
LONGEST BLANK WALL:	4' < 35' MAX ALLOWED
GARDEN PLACE ELEVATION	
GROUND FLOOR:	
TOTAL AREA:	1,040 SF
TRANSPARENT AREA:	920 SF = 50% > 35% MIN REQ'D
FLOORS 2-5:	
TOTAL AREA:	6,891 SF
TRANSPARENT AREA:	3,005 SF = 44% > 35% MIN REQ'D
FLOORS BASEMENT:	
TOTAL AREA:	10,045 SF
TRANSPARENT AREA:	4,010 SF = 40.5% > 35% MIN. REQ'D
LONGEST BLANK WALL:	6' < 35' MAX ALLOWED



BUILDING ELEVATION - EAST (ENTERPRISE ST)  
1/16" = 1'-0" ①

FINISH LEGEND - EXTERIOR MATERIALS

	<b>UM01</b> 04 2000 UNIT MASONRY MATERIAL: CLAY MASONRY UNIT SIZE: MODULAR UNIT NOTE: PRIMARY FIELD BRICK
	<b>UM02</b> 04 2000 UNIT MASONRY MATERIAL: CLAY MASONRY UNIT SIZE: MODULAR UNIT NOTE: SECONDARY COLOR
	<b>PP01</b> 07 42 33 PHENOLIC PANEL MATERIAL: Phenolic Panel
	<b>MP01</b> 07 4213.2 METAL COMPOSITE MATERIAL DESCRIPTION: Metal Composite Metal TYPE: Fire Retardant Core SIZE: Refer to Sections and Elevations THICKNESS: 0.157" Material, 2" Panel STYLE: Route and Return
	<b>ES01</b> 08 4113 Entrances and Storefronts MANUFACTURER: Old Castle Building Envelope TYPE: FG-3000 Center Set SIZE: 2' x 4 1/2" NOTE: Refer to Elevations for Glazing Types
	<b>GL01</b> 08 8000 Glazing DESCRIPTION: Heat Strengthened Insulated Glass MANUFACTURER: Guardian TYPE: SunGuard SNX 51/23 SIZE: 1" Unit Width NOTE: Temper as indicated on Elevations
	<b>GL01*</b> 08 8000 Glazing DESCRIPTION: Heat Strengthened Insulated Glass MANUFACTURER: Guardian TYPE: SunGuard SNX 51/23 SIZE: 1" Unit Width Apply Translucent Film on Surface 4 NOTE: Temper as indicated on Elevations