

# WEST MORGAN SMALL AREA STUDY

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Final Study  
(adopted by Raleigh City Council on June 7, 2011)

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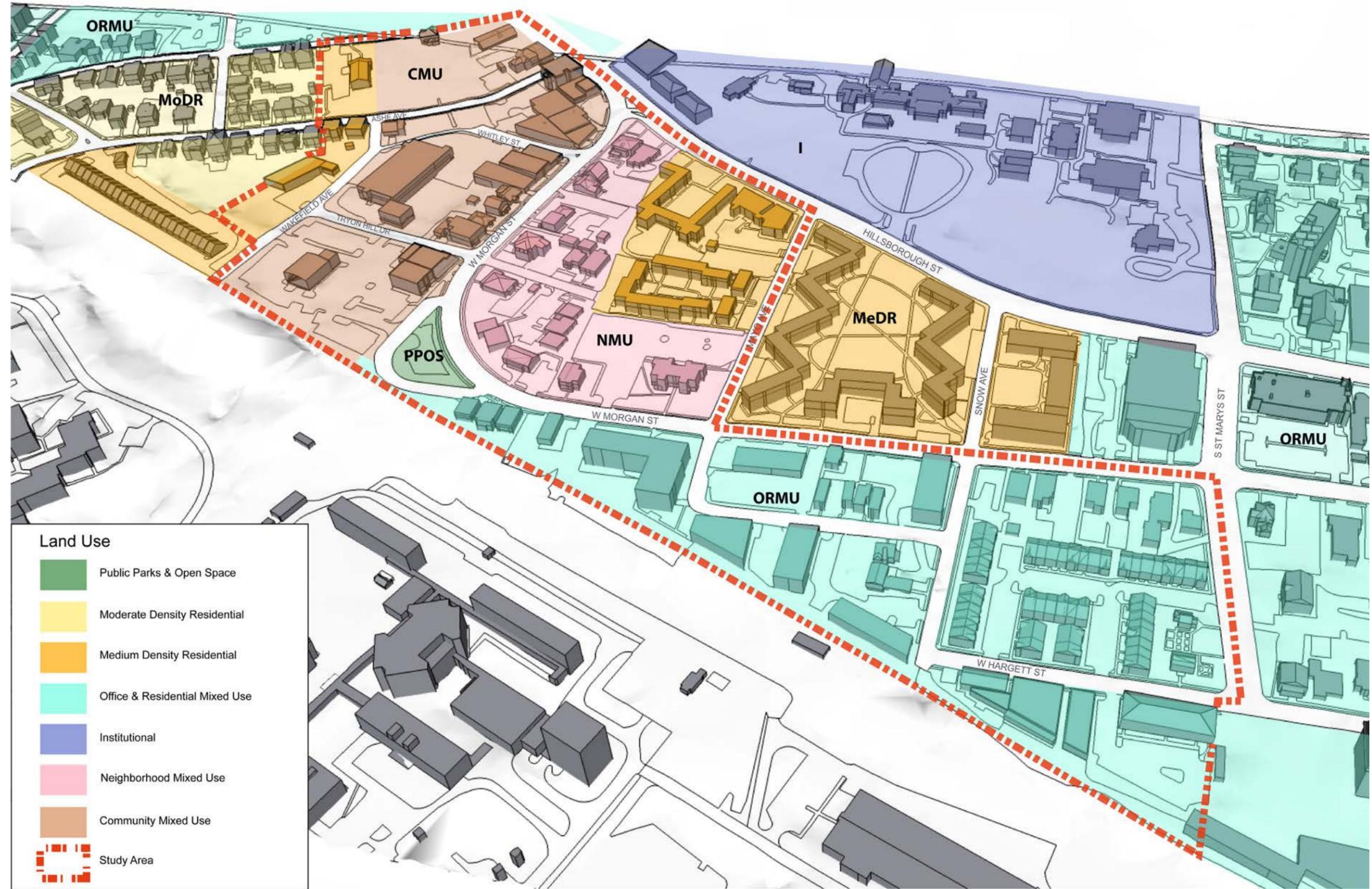
# Land Use

## Recommended Outcome

- LU-1. Require all new structures to be primarily accessed and oriented toward public streets.
- LU-2. Encourage vertical mixed-use new development.
- LU-3. Encourage new residential development that includes unit types, open spaces, and amenities conducive to family living.

## Recommended Actions

- LU-4. Amend the Future Land Use Map to include land use classifications: (CMU, NMU, ORMU, MeDR, PPOS) as depicted in the adjacent diagram.
- LU-5. Conduct a study to determine the feasibility of creating a new FLUM category that allows for dense pedestrian friendly mixed use that is primarily residential in character.
- LU-6. Explore text changes that would:
  - Allow on-street spaces to be counted towards parking totals for adjacent parcels
  - Further reduce or eliminate parking requirements for all uses and potentially establish parking maximums
  - Allow for shared parking on new and existing projects identified in this study.
  - Create a fee-in-lieu option.



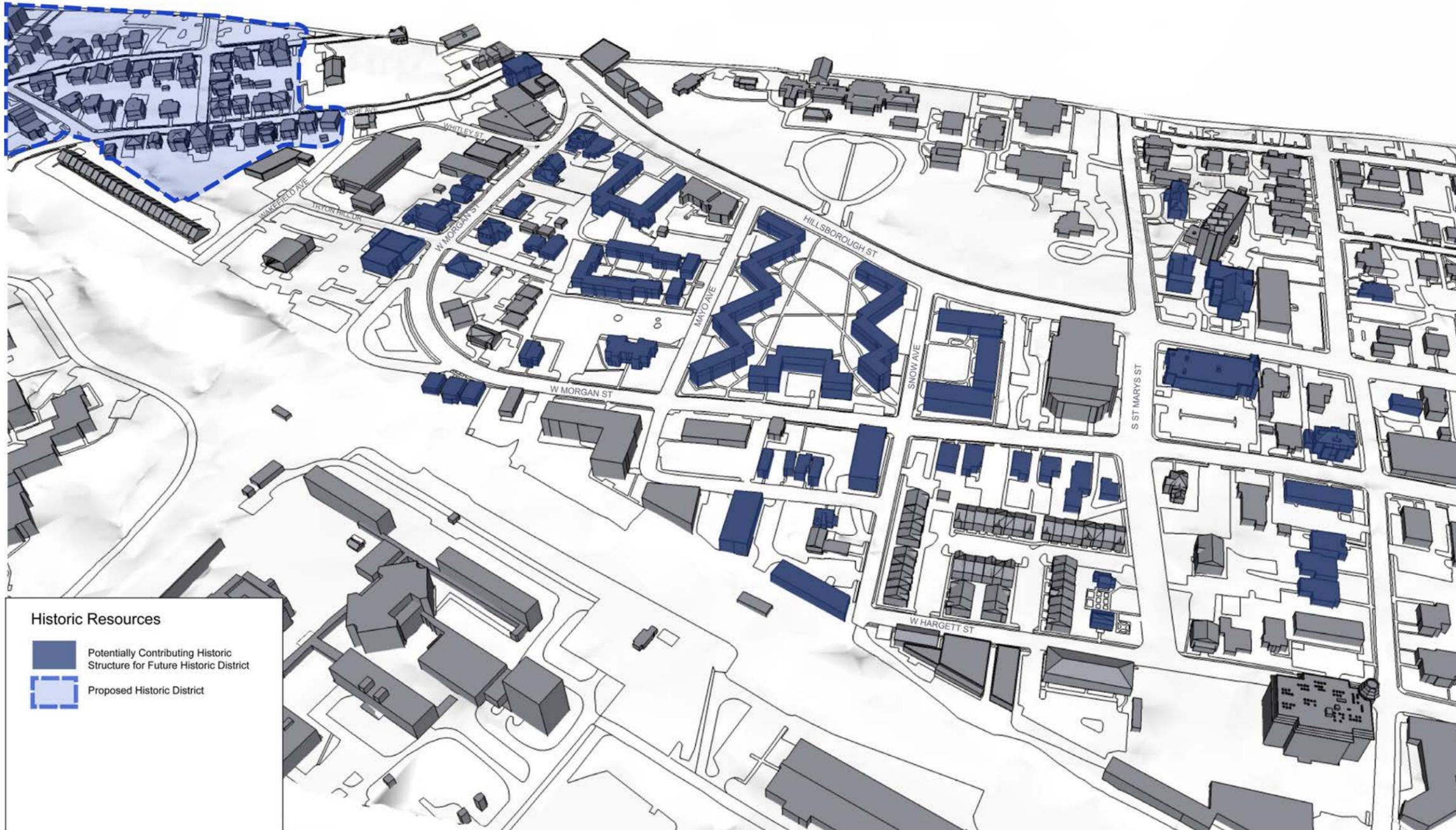
## Historic Resources

### Recommended Outcome

- HR-1. The existing garden apartments and historic single-family structures should be preserved.
- HR-2. Preserve the existing gardens and courtyards associated with the garden apartment complexes in the community.
- HR-3. Encourage the adaptive reuse of structures within the district.

### Recommended Actions

- HR-4. Create a National and/or Local Historic District for the West Morgan area and Pullen Park Neighborhood.
- HR-5. Work with Preservation North Carolina and landowners to explore feasibility and desirability of conservation easements as a tool to protect historic resources.
- HR-6. Explore desirability of a Neighborhood Conservation Overlay District for the Pullen Park neighborhood.



**Historic Resources**

- Potentially Contributing Historic Structure for Future Historic District
- Proposed Historic District



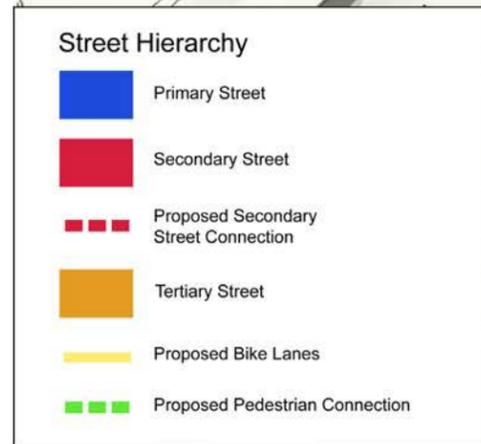
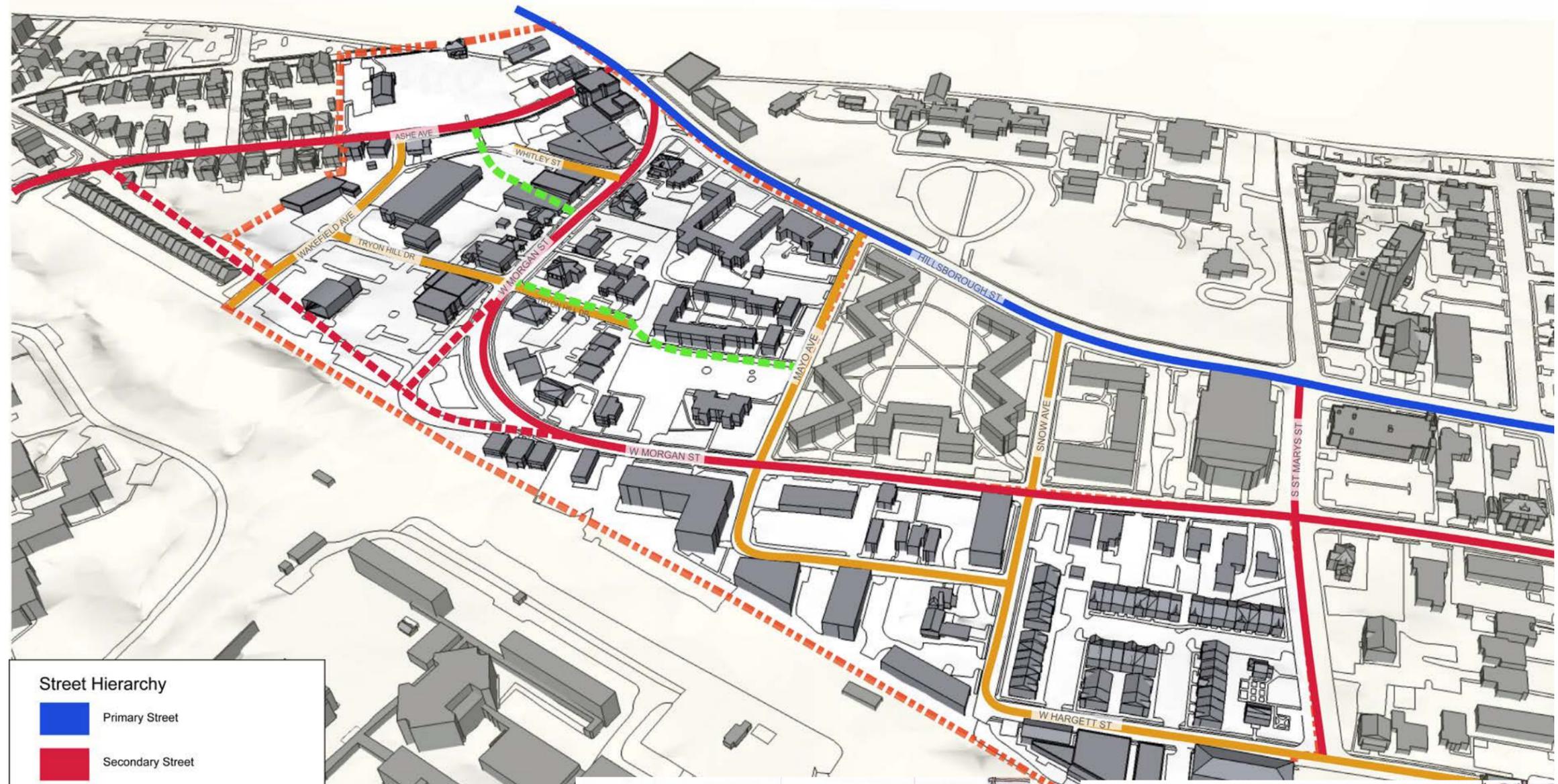
# Transportation Infrastructure

## Recommended Outcome

- T-1. Improve transit facilities in the district.
- T-2. Create a greenway connection in addition to or as an alternative to the Ashe Avenue connector.
- T-3. Improve bicycle and pedestrian facilities.

## Recommended Outcome

- T-4. Conduct a feasibility study and pursue funding for the Ashe Avenue connector. Include a greenway option in the study.
- T-5. As part of the Triangle Transit commuter rail study, explore the possibility of an in-street option using West Morgan Street and/or Hargett Street. Include transit stops and intersection redesign in the study.
- T-6. Design and fund roadway improvements focused on pedestrian and bicycle facilities.
- T-7. Prioritize construction of bicycle lanes on Ashe Avenue, West Morgan Street, and Hillsborough Street, and sharrows on St. Mary's Street as called for in the City wide bicycle plan.
- T-8. Identify and fund opportunities for improved bus stops and shelters.



ASHE AVENUE SECTION



West Morgan Street Section



Hillsborough Street Section

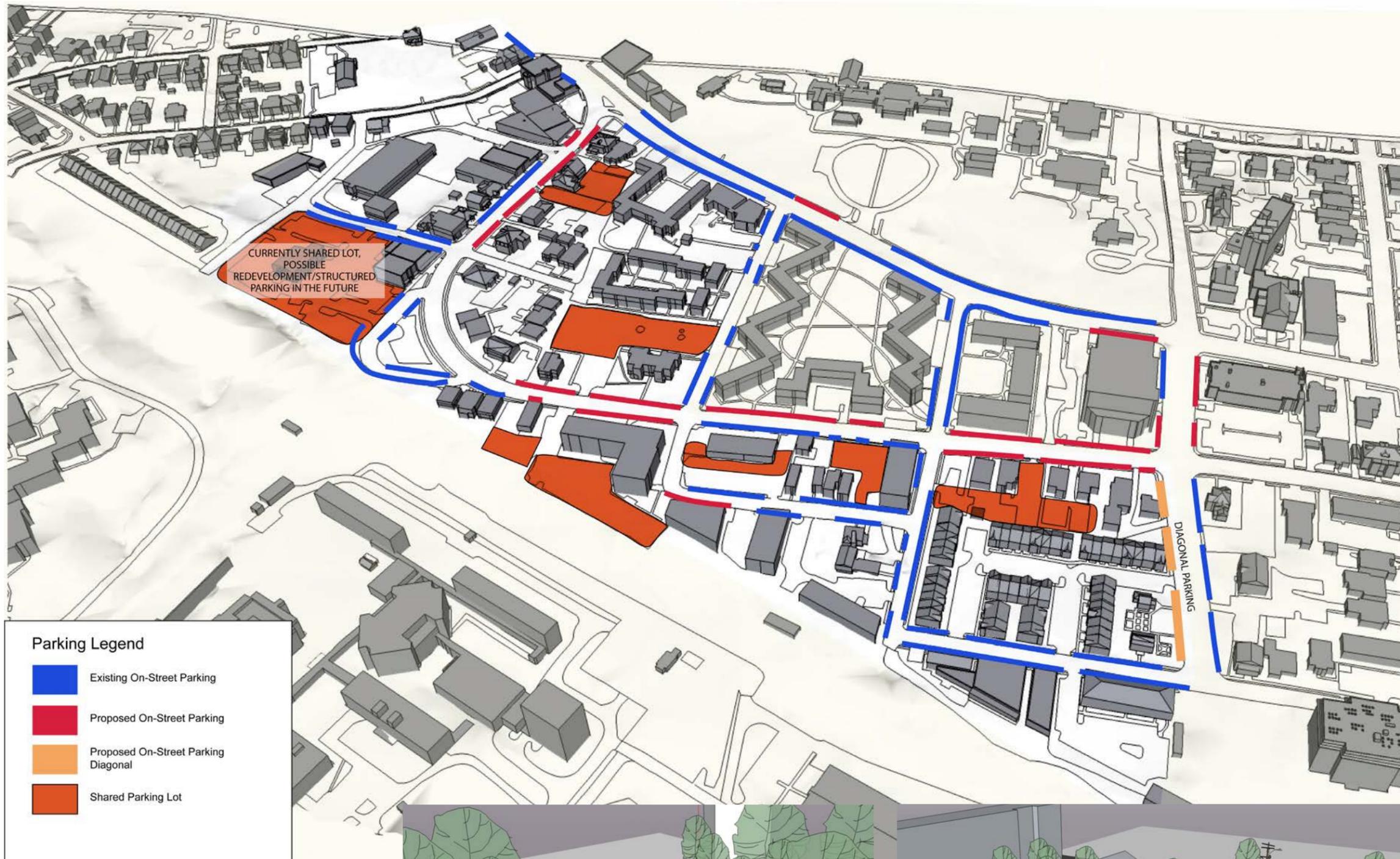
# Parking

## Recommended Outcome

- P-1. Parking within the district should be provided in the most efficient manner possible, so as to discourage the construction of unnecessary lots and decks, and to encourage alternative modes of transportation.
- P-2. On-street parking should be maximized to limit the need for new off-street spaces. On-street parking should be prioritized over additional road capacity.
- P-3. Parking, when provided, should not be placed adjacent to the right-of-way and should be placed underground or wrapped with an active ground floor use, and should be screened at the upper levels.
- P-4. Convene a meeting of property owners to discuss feasibility of allowing residential parking at night in office parking lots.

## Recommended Actions

- P-5. Conduct a parking inventory of the area to determine where additional on-street spaces can be created, and if a meter or permit system would be desirable.
- P-6. Conduct a traffic analysis to determine the feasibility of removing a traffic lane on Morgan Street and Hillsborough Street in order to create additional on-street parking on both sides of West Morgan Street.
- P-7. Create additional parking on St. Mary's Street through the use of angled parking adjacent to the Joel Lane House.
- P-8. Work with existing parking lot owners to consolidate small surface parking lots to eliminate multiple curb cuts.

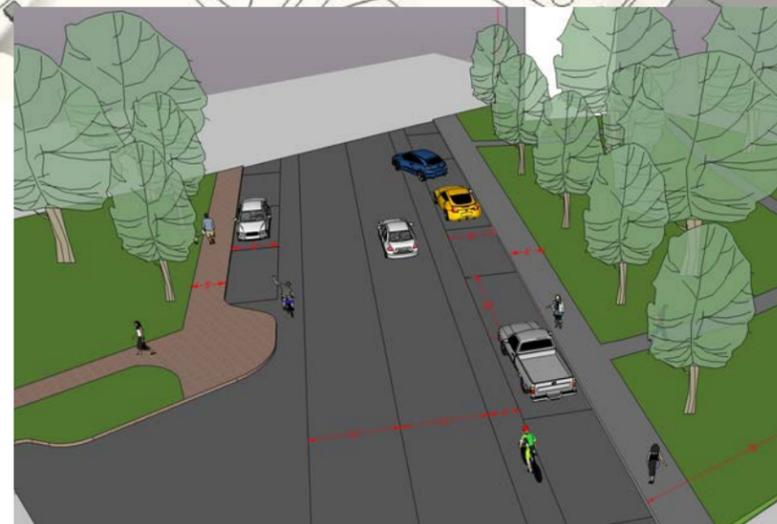


**Parking Legend**

- █ Existing On-Street Parking
- █ Proposed On-Street Parking
- █ Proposed On-Street Parking Diagonal
- █ Shared Parking Lot

**Additional on-street parking**

St. Mary's Parallel	13
St. Mary's Diagonal	35
Hillsborough Street	13
West Morgan Street	84
(with restriping)	
<b>TOTAL</b>	<b>145</b>



# Form

## Recommended Outcome

F-1. Building setbacks should respond to the existing urban character. Setbacks on Hillsborough Street east of Morgan Street should be 20-30', and setbacks on all other streets should be 15'.

F-2. Building heights can vary based on context, with lower heights where adjacent uses are predominately low height residential.

F-3. Building heights at street edges should be limited to form a generally consistent street wall of 3-4 stories, with any additional height stepped back from the street.

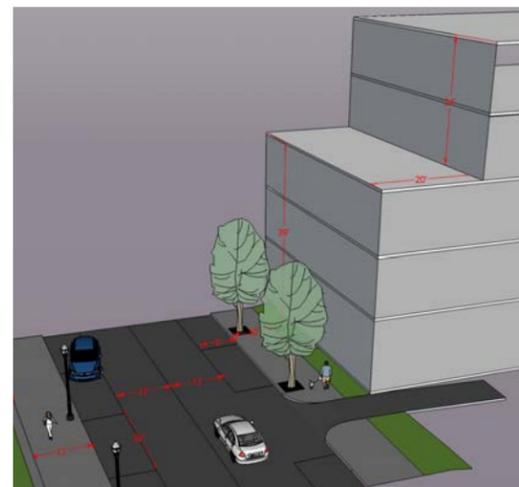
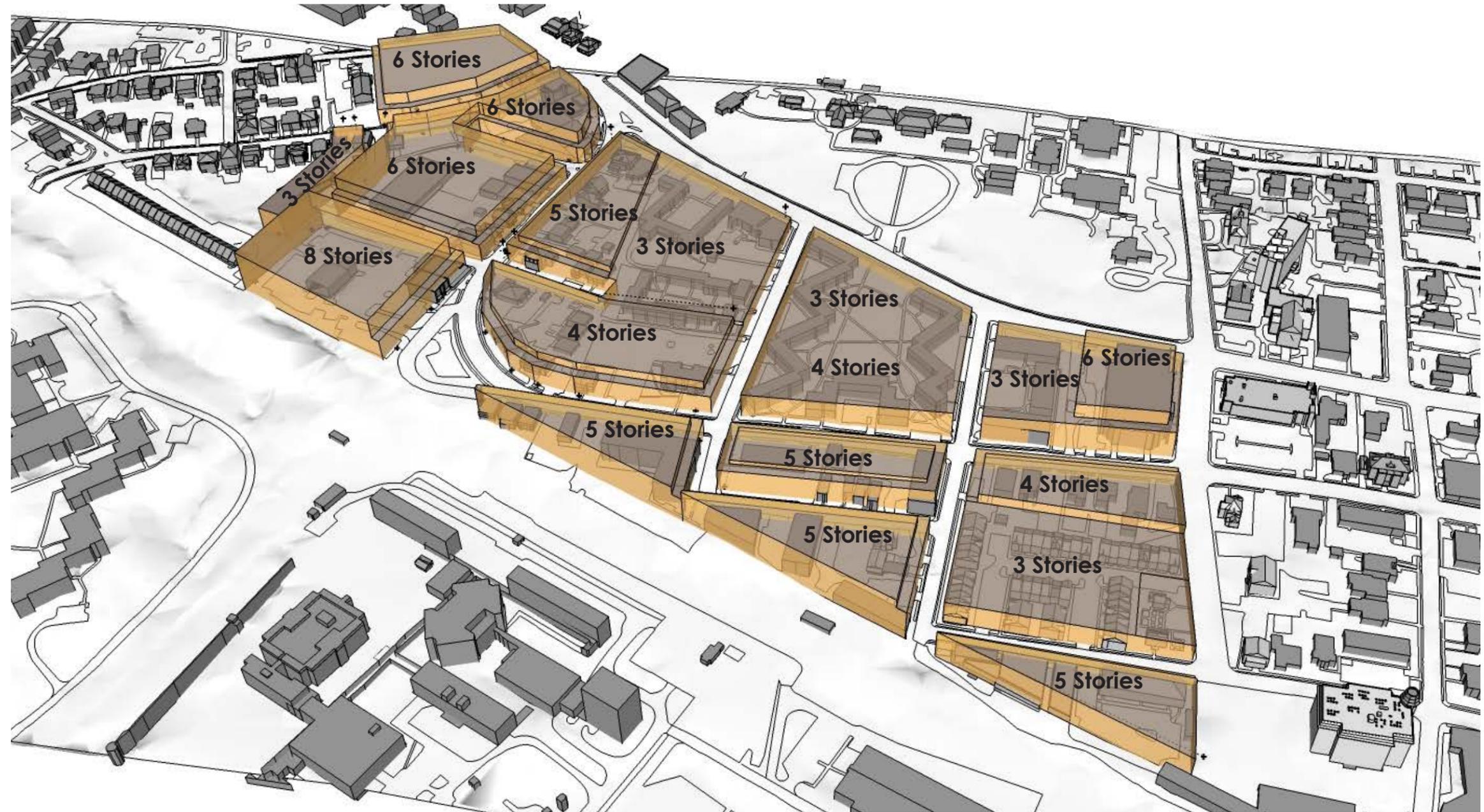
F-4. Taller structures should mitigate the impact of their height through the use of setbacks, stepbacks, smaller footprints, and smaller scale uses at the street edge.

## Recommended Actions

F-5. Create a PBOD, so as to allow for vertical mixed use pedestrian oriented projects, and include height restrictions and open space requirements in the Streetscape and Parking Plan.

F-6. Recommend the district for inclusion in any discussion of form-based codes or transition zones as part of the UDO project.

F-7. Streetscape and Parking Plan should address architectural character elements that contribute significantly to the overall character of the district.



Typical 15' Setback with 3 Story Stepback along street frontage



The varied style, material and scale of residential and commercial structures contribute to creating the character of the district. New development should continue those characteristics.

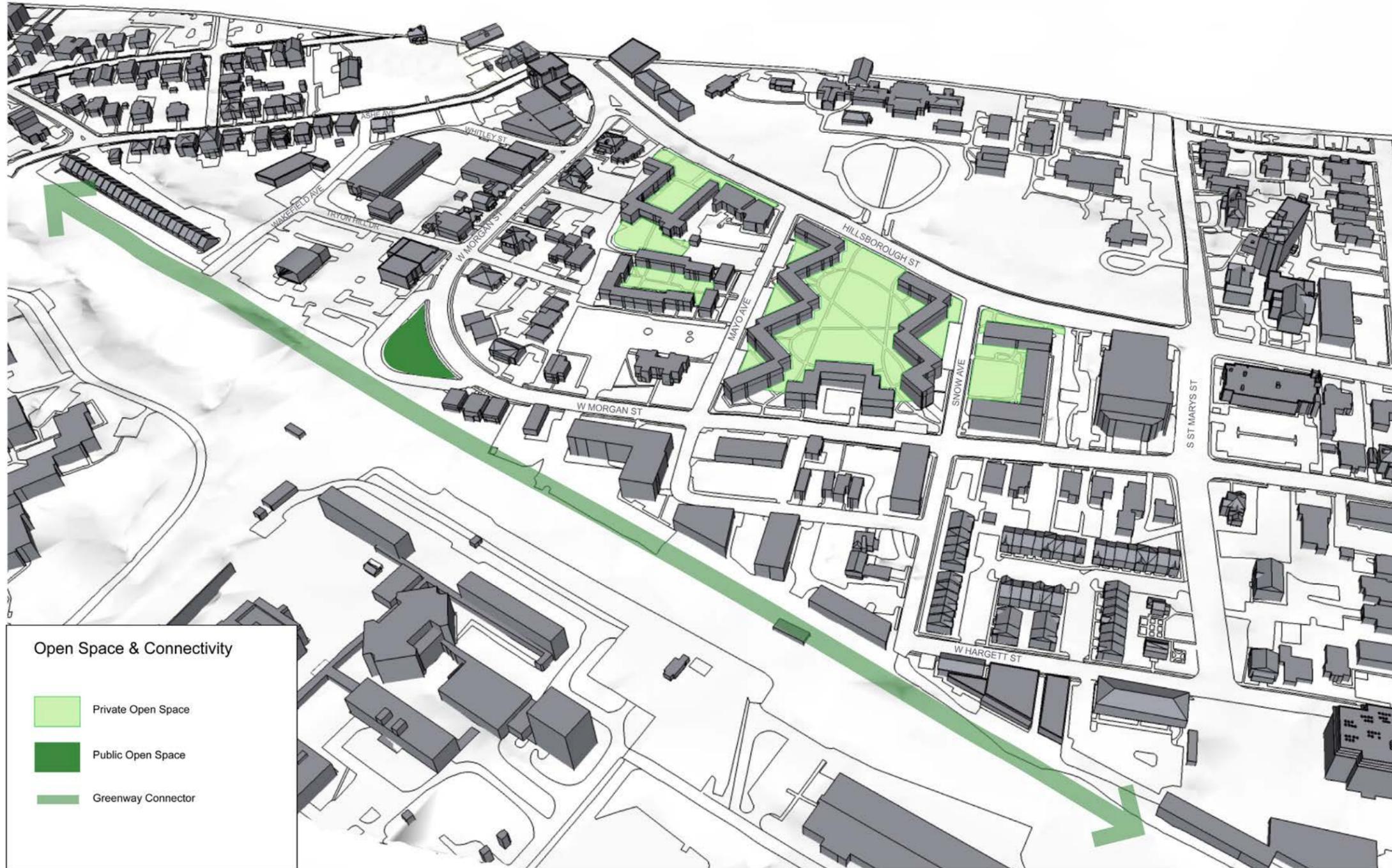
## Open Space

### Recommended Outcome

- OS-1. Encourage the inclusion of usable and centrally located open space in new residential development.
- OS-2. Increase the connection of the West Morgan District to Pullen Park and the Dorothea Dix Greenway.
- OS-3. Use open space to mitigate increased building height and density by locating open space in a manner that reduces the perceived bulk of new structures.

### Recommended Actions

- OS-4. As part of the UDO project, require additional usable open space for new multifamily residential development in the new UDO.
- OS-5. Conduct a study to determine additional pedestrian route improvement to existing routes to Pullen Park and Dorothea Dix Greenway.
- OS-6. Include open space as a tool for density impact mitigation in transitional density zones in the new UDO.



# Public Realm Improvements

## Recommended Outcome

PR-1. The public realm should have a different character based on the adjacent land uses- there should be commercial streetscapes and residential streetscapes.

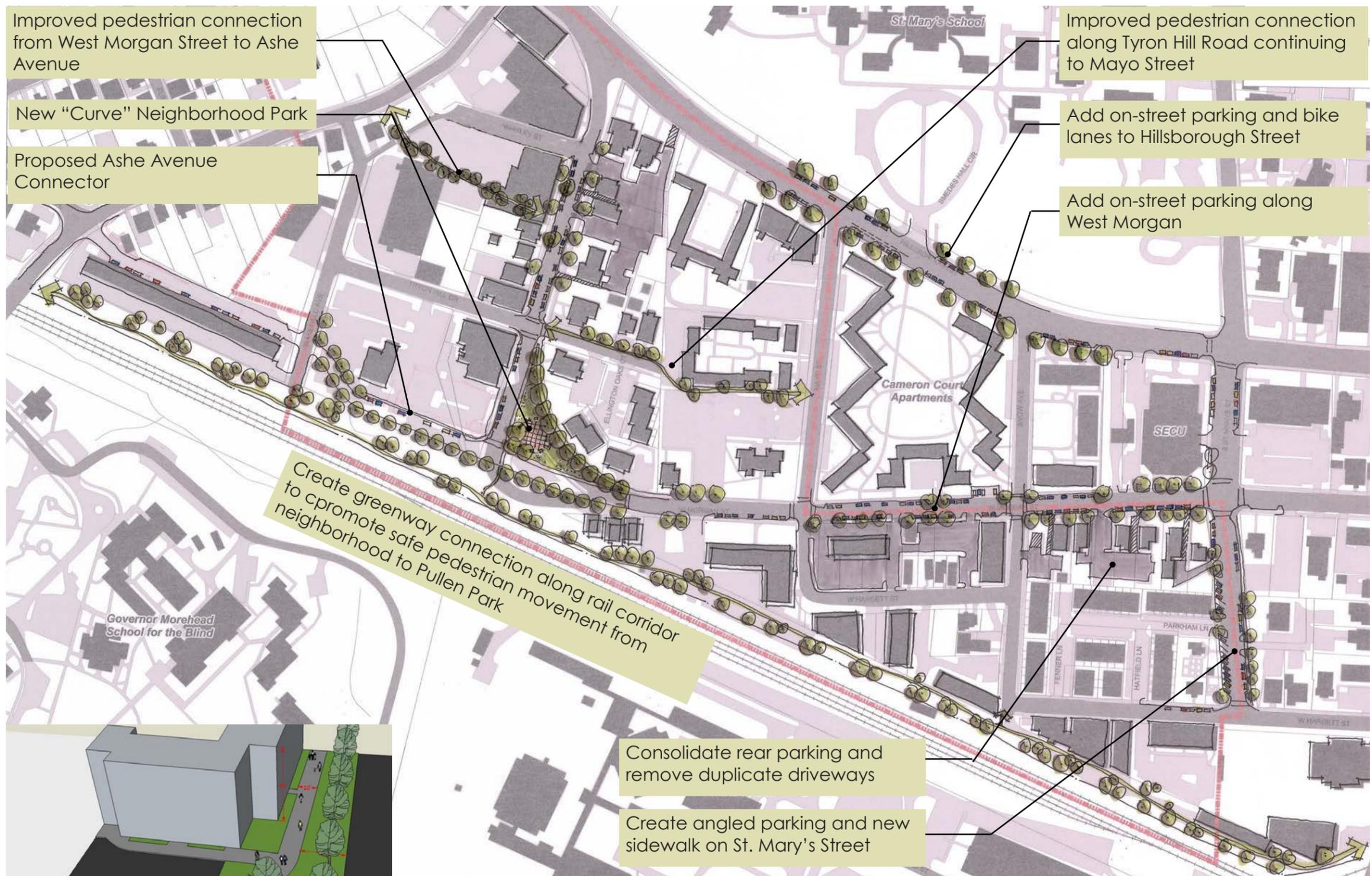
PR-2. The West Morgan District should be pedestrian-oriented, with wide sidewalks, new lighting, underground utilities, street trees, limited driveways, crosswalks, slow traffic with bike lanes, and active ground floor uses.

PR-3. Denser new development should include green/open space and other public realm improvements to mitigate density.

## Recommended Actions

PR-4. Draft a streetscape plan that regulates ground-floor uses, sidewalk width, lighting, underground utilities, crosswalks, street trees, and other amenities. The streetscape plan should include different street typologies based on adjacent land uses.

PR-5. Establish contact with the State of NC Property Office to determine future of or interest in redevelopment of Division of Prisons office and parking lots.



Tryon Hill Pedestrian Connection



Location for the new "Curve" Park



Greenway Connection Along Rail Corridor



Create on-street parking on both sides of West Morgan