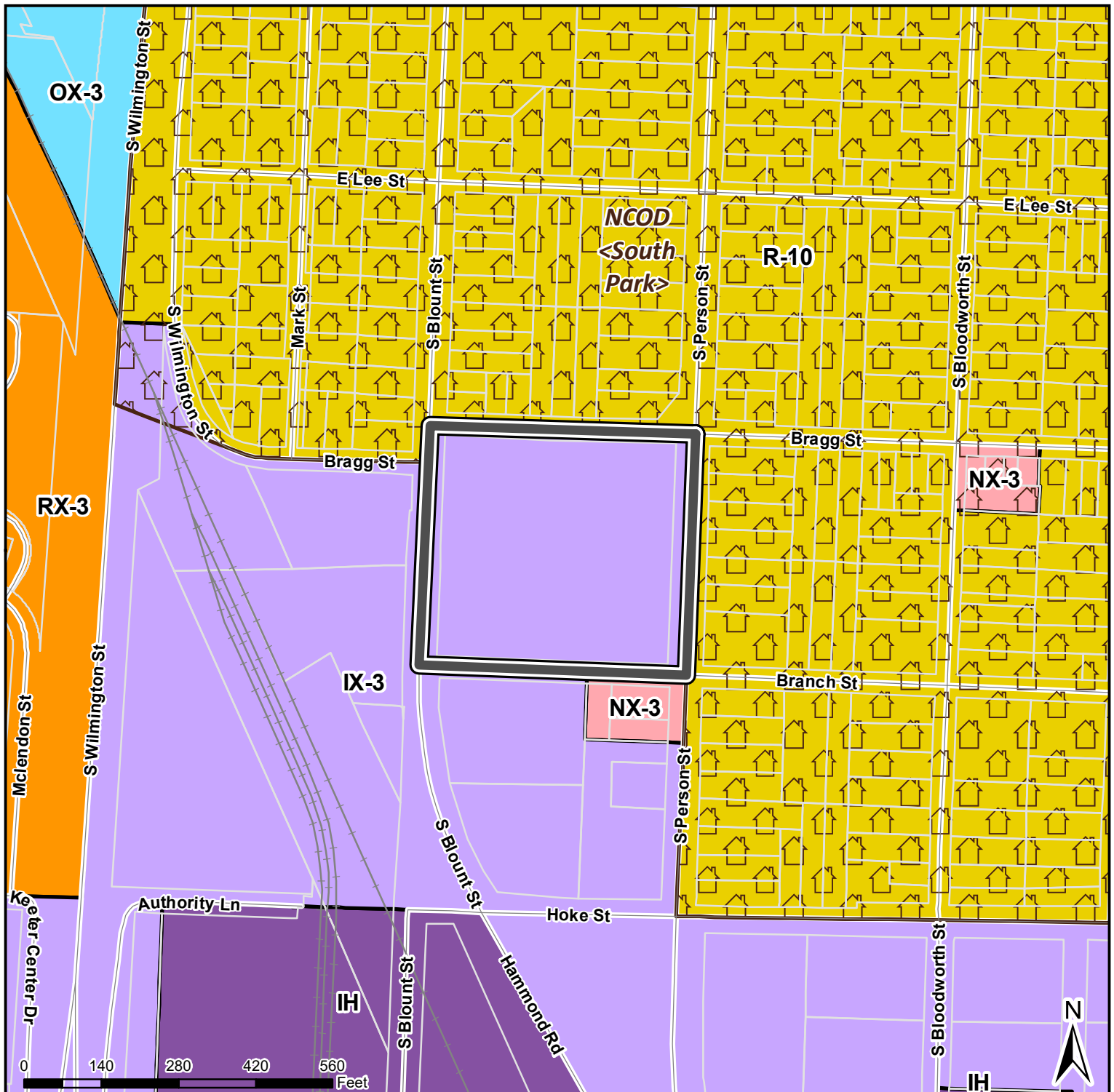
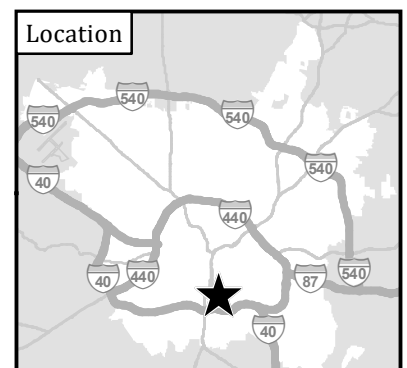


Existing Zoning

Z-14-2019



Property	1208 S Blount St
Size	3.84 acres
Existing Zoning	IX-3
Requested Zoning	PD





Raleigh

MEMO

TO: Ruffin L. Hall, City Manager

FROM: Ken Bowers, AICP; Jason Hardin, AICP

DEPARTMENT: Planning and Development

DATE: October 19, 2020

SUBJECT: Public hearing for November 4, 2020 – Rezoning Z-14-19

On October 6, 2020, City Council authorized the public hearing for the following item:

Z-14-19 Blount Street PD, acres in the block bounded by South Blount Street, Bragg Street, Branch Street, and South Person Street, consisting of Wake County PIN 1703738654. . Approximately 3.8 acres are requested by SM Raleigh LLC to be rezoned.

Current zoning: Industrial Mixed Use-Three Stories (IX-3)

Requested zoning: Planned Development (PD)

The proposed PD uses Neighborhood Mixed Use (NX) as a base district. It prohibits vehicle fuel sales, vehicle sales, and vehicle repair. Among other provisions, it limits residential units to 100 and commercial space to 10,000 square feet; limits height to five stories and 55 feet, with the top floor restricted to use as a rooftop deck; includes on-site art that recognizes the history and significance of the South Park neighborhood; and specifies that open space would be accessible to the public.

The request **is consistent** with Future Land Use Map.

The request is **consistent** with the Comprehensive Plan.

The **Planning Commission** voted 6-2 in support of the request. One of the commission members who voted against the request mentioned the lack of specifically dedicated affordable housing units.

The **Central CAC** voted in favor of the rezoning (Yes-16, No-0) on November 4, 2019.

Attached are the Planning Commission Certified Recommendation (including Staff Report and Traffic Study Worksheet), the Petition for Rezoning, the PD narrative and plan sheets, and the Neighborhood Meeting Report.

Municipal Building
222 West Hargett Street
Raleigh, North Carolina 27601

One Exchange Plaza
1 Exchange Plaza, Suite 1020
Raleigh, North Carolina 27601

City of Raleigh
Post Office Box 590 • Raleigh
North Carolina 27602-0590
(Mailing Address)



RALEIGH PLANNING COMMISSION

CERTIFIED RECOMMENDATION

CR#12034

CASE INFORMATION: Z-14-19 (MP-1-19) S. BLOUNT ST

Location	S. Blount Street, at the southeast corner of its intersection with Bragg Street Address: 1208 S. Blount Street PIN: 1703738654 iMaps , Google Maps , Directions from City Hall
Current Zoning	IX-3
Requested Zoning	PD
Area of Request	3.84 acres
Corporate Limits	The site is within Raleigh's corporate limits.
Property Owner	SM Raleigh, LLC 4020 Westchase Boulevard, Suite 470 Raleigh, NC 27607
Applicant	Worth Mills, Longleaf Law Partners 2235 Gateway Access Point, Suite 201 Raleigh, NC 27607
Citizens Advisory Council (CAC)	Central CAC
PC Recommendation Deadline	September 28, 2020

COMPREHENSIVE PLAN GUIDANCE

Future Land Use	Neighborhood Mixed Use
Urban Form	None
Consistent Policies	Policy LU 1.2 Future Land Use Map and Zoning Consistency Policy LU 4.4 Reducing Vehicle Miles Traveled Through Mixed-use Policy LU 5.4 Density Transitions Policy LU 7.5 High-Impact Commercial Uses Policy LU 8.1 Housing Variety Policy LU 8.10 Infill Development Policy LU 11.4 Rezoning/Development of Industrial Areas Policy UD 7.3 Urban Design Guidelines Policy EP 1.1 Greenhouse Gas Reduction Policy H 1.8 Zoning for Housing Policy AP-SP 2 South Park Focal Point
Inconsistent Policies	None

PROPOSED MODIFICATION OF NX STANDARDS

Article 7.1 Parking. Vehicle parking will be provided at DX requirements, which do not require parking and limit parking to no more than two spaces per unit.

Existing code requirement: One space is required per bedroom, and one space per 10 units is required for visitor parking. One space per 300 square feet is required for retail use, and one per 400 for office use.

Article 3.2 Base Dimensional Standards. Outdoor amenity area will not be required.

Existing code requirement. An amenity area of at least 10 percent of the site is required for each of the potential building types. PD requirements include at least a 10 percent open space area.

Article 3.2.4 Base Dimensional Standards

	Proposed	Existing
Apartment Building Type		
Minimum lot area	No minimum	10,000 square feet
Primary build-to percentage	Minimum 55 percent	Minimum 70 percent
Side build-to percentage	Minimum 20 percent	Minimum 35 percent
Ground floor elevation	0 feet	2 feet
Ground floor transparency	20 percent minimum	20 percent minimum
Upper floor transparency	15 percent minimum	15 percent minimum
Blank wall maximum	No maximum along Bragg and Branch; 35 linear feet along Blount and Person	35 linear feet
General Building Type		
Setback from alley	4 feet	5 feet
Blank wall maximum	20 feet	30 feet
Mixed Use Building Type		
Setback from alley	4 feet	5 feet
Ground story height	11 feet	13 feet
Ground story transparency	33 percent min.	50 percent min.

ADDITIONAL STANDARDS SPECIFIED BY PD

Uses prohibited	Vehicle fuel sales, vehicle sales, vehicle repair
Total residential units	100
Total commercial space	10,000 square feet
Maximum height, Tract A (all but southeast corner of site)	Five stories/55 feet, with the top story limited to rooftop deck and access
Maximum height, Tract B	Three stories/50 feet
Units with primary access to public street	40 percent
Facade materials	Will consist of one or more of glass, concrete, masonry, stone, and stucco. Synthetic stucco and vinyl will not be used, except as windows or trim
Art	A local artist will create on-site art that "recognizes history and significance of the South Park neighborhood"

FUTURE LAND USE MAP CONSISTENCY

The rezoning case is ☒ **Consistent** ☐ **Inconsistent** with the Future Land Use Map.

COMPREHENSIVE PLAN CONSISTENCY

The rezoning case is ☒ **Consistent** ☐ **Inconsistent** with the 2030 Comprehensive Plan.

PUBLIC MEETINGS

Neighborhood Meeting	CAC	Planning Commission	City Council
3/28/2019 (5 attendees)	8/7/19 11/4/19 (16 yes, 0 no)	6/30/20; 9/8/20; 9/22/20	10/6/20

PLANNING COMMISSION RECOMMENDATION

- ☒ The rezoning case is **Consistent** with the relevant policies in the Comprehensive Plan, and **Approval** of the rezoning request is reasonable and in the public interest.
- ☐ The rezoning case is **Consistent** with the relevant policies in the comprehensive Plan, but **Denial** of the rezoning request is reasonable and in the public interest.
- ☐ The rezoning is **Inconsistent** with the relevant policies in the Comprehensive Plan, and **Denial** of the rezoning request is reasonable and in the public interest.
- ☐ The rezoning case is **Inconsistent** with the relevant policies in the Comprehensive Plan, but **Approval** of the rezoning request is reasonable and in the public interest due to changed circumstances as explained below. Approval of the rezoning request constitutes an amendment to the Comprehensive Plan to the extent described below.

Reasonableness and Public Interest	The plan is consistent with several key policies, including those related to density transitions, housing variety, infill development, and zoning for housing.
Change(s) in Circumstances	N/A
Amendments to the Comprehensive Plan	N/A
Recommendation	Approval
Motion and Vote	Motion: Fox Second: Winters In Favor: Bennett, Fox, Hicks, Lampman, Tomasulo, Winters Opposed: Mann, McIntosh

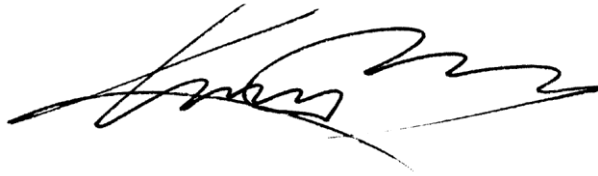
**Reason for Opposed
Vote(s)**

Commissioner Mann expressed concern about the lack of specifically dedicated affordable units in the project.

ATTACHMENTS

1. Staff report
2. Rezoning Application

This document is a true and accurate statement of the findings and recommendations of the Planning Commission. Approval of this document incorporates all of the findings of the attached Staff Report and Comprehensive Plan Amendment Analysis.



Ken A. Bowers, AICP
Planning and Development Deputy Director

Date: September 22, 2020

Staff Coordinator: Jason Hardin: (919) 996-2657; Jason.Hardin@raleighnc.gov



ZONING STAFF REPORT – CASE Z-14-19

Planned Development District (MP-1-19)

OVERVIEW

A Planned Development (PD) is a zoning district that is intended to provide higher design quality by allowing modifications to certain Unified Development Ordinance (UDO) standards in exchange for greater detail of development. To create a PD, applicants start with a standard zoning district, in this case Neighborhood Mixed Use (NX), and modify the development requirements of that district and offer specific detail of how the resulting development will provide higher design quality. These modifications are outlined in a Master Plan that accompanies the rezoning petition. Where the Master Plan does not specify a modification to the UDO, the unmodified standards in the UDO apply. This staff report analyzes the policies in the 2030 Comprehensive Plan for consistency, typical of any rezoning request, and also provides an analysis of the proposed modifications to the UDO standards as proposed in the Master Plan.

The proposal seeks to rezone a 3.8-acre parcel just south of downtown and on the edge of the South Park neighborhood. The site occupies a complete block bounded by South Blount, South Person, Bragg, and Branch streets. It occupies a transitional area between industrial uses that border the rail line to the west and residential uses to the north and east.

The site is currently vacant. It was formerly used as an industrial site, most recently as a bus depot. It is within a tenth of a mile of South Wilmington Street, which is designated as a bus rapid transit corridor between downtown Raleigh and Garner. A planning process to determine the final alignment for that corridor is underway.

The property is currently zoned Industrial Mixed Use-Three Stories (IX-3). The request is for Planned Development (PD) with a base district of Neighborhood Mixed Use (NX). The request limits development to 100 residential units and 10,000 square feet of commercial space. The residential units would be within the Apartment building type. The commercial space would be limited to the southeast corner of the site, across Branch Street from existing retail space.

The area is bordered by a range of uses, with residential to the north and east, and a mix of retail, industrial, and residential to the south and west. The Bragg Street park is across the street to the north.

Existing zoning reflects the pattern of current uses, with industrial zoning (IX-3) on the site and to the west and south, and residential zoning (Residential-10) to the north and east. The South Park Neighborhood Conservation Overlay District, which regulates lot dimensions, height, and parking location, is applied to the adjacent residential zones.

The Future Land Use Map designates the subject site as Neighborhood Mixed Use, making the request consistent with that category in terms of permitted uses. Areas to the south and

west are also designated as Neighborhood Mixed Use; those to the north and east are designated as Moderate Density Residential.

The site is designated as a Core Transit area on the Urban Form Map, reflecting the plans for BRT along South Wilmington Street. This designation calls for development to support walkability, with buildings closer to the street and parking behind the building. The PD addresses both of those considerations, making it consistent with the map.

In comparison to existing industrial zoning, the PD would significantly limit non-residential uses, while also making residential uses more feasible by allowing them on the ground floor. It would allow two additional stories of height, but only one more floor of occupiable space; the fifth floor would be limited to rooftop uses and access only. Additionally, height is limited to 55 feet, which is less than the limit for four stories and close to the 50 feet allowed in IX-3.

A Planned Development, or PD, allows the modification of many code standards. In this case modifications include allowing less parking than would otherwise be required and reducing standards for transparency (the percentage of a wall that has windows and doors) and blank wall size. Parking would be accessed from internal alleys, and a central corridor of open space connecting Bragg Street to Branch Street would be open to the public.

Update for September 22, 2020

Following the Sept. 8 Planning Commission meeting, the applicant made a number of revisions to the plan. The revisions addressed comments from the meeting and improved consistency with the Comprehensive Plan. The revisions, which have been incorporated into the tables on page 2 of the Certified Recommendation, include:

- Removing the exemptions from transparency standards
- Removing the exemption from blank wall standards along Blount and Person streets, along which most of the building area would be oriented. The exemption would remain along Bragg and Branch streets.
- Specifying that only certain building materials would be used in facades – glass, concrete, masonry, stone, and stucco
- Specifying that a local artist will be used to create art “that recognizes the history and significance of the South Park neighborhood.”

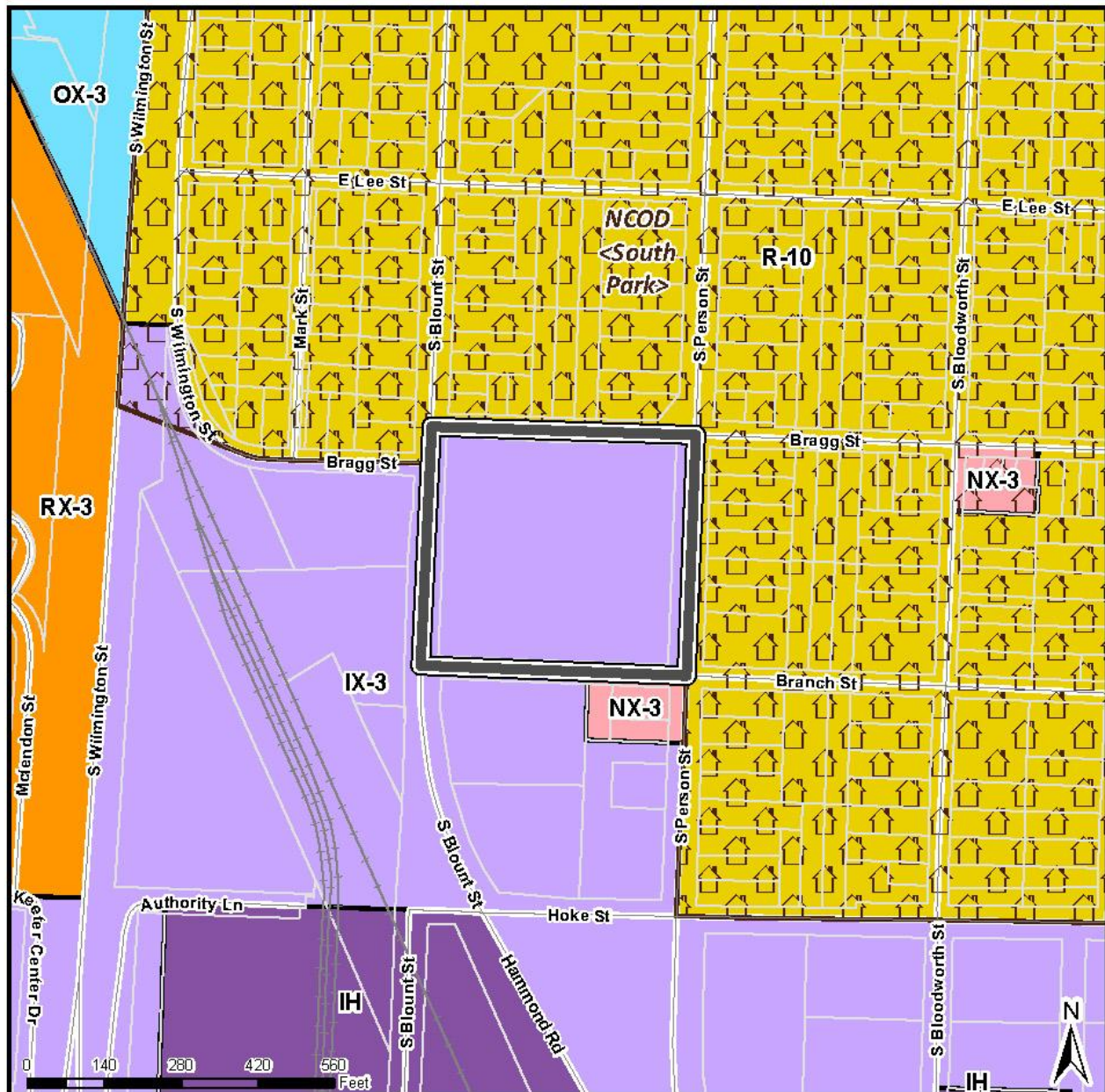
The changes led to a determination that the proposal is now consistent with the one policy, UD 7.3 Urban Design Guidelines, with which it had previously been inconsistent.

OUTSTANDING ISSUES

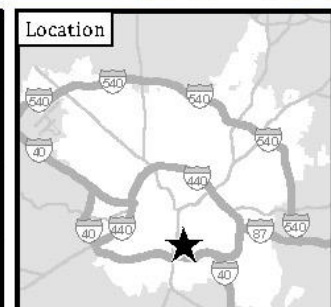
Outstanding Issues	1. None	Suggested Mitigation	1. None
---------------------------	---------	-----------------------------	---------

Existing Zoning

Z-14-2019



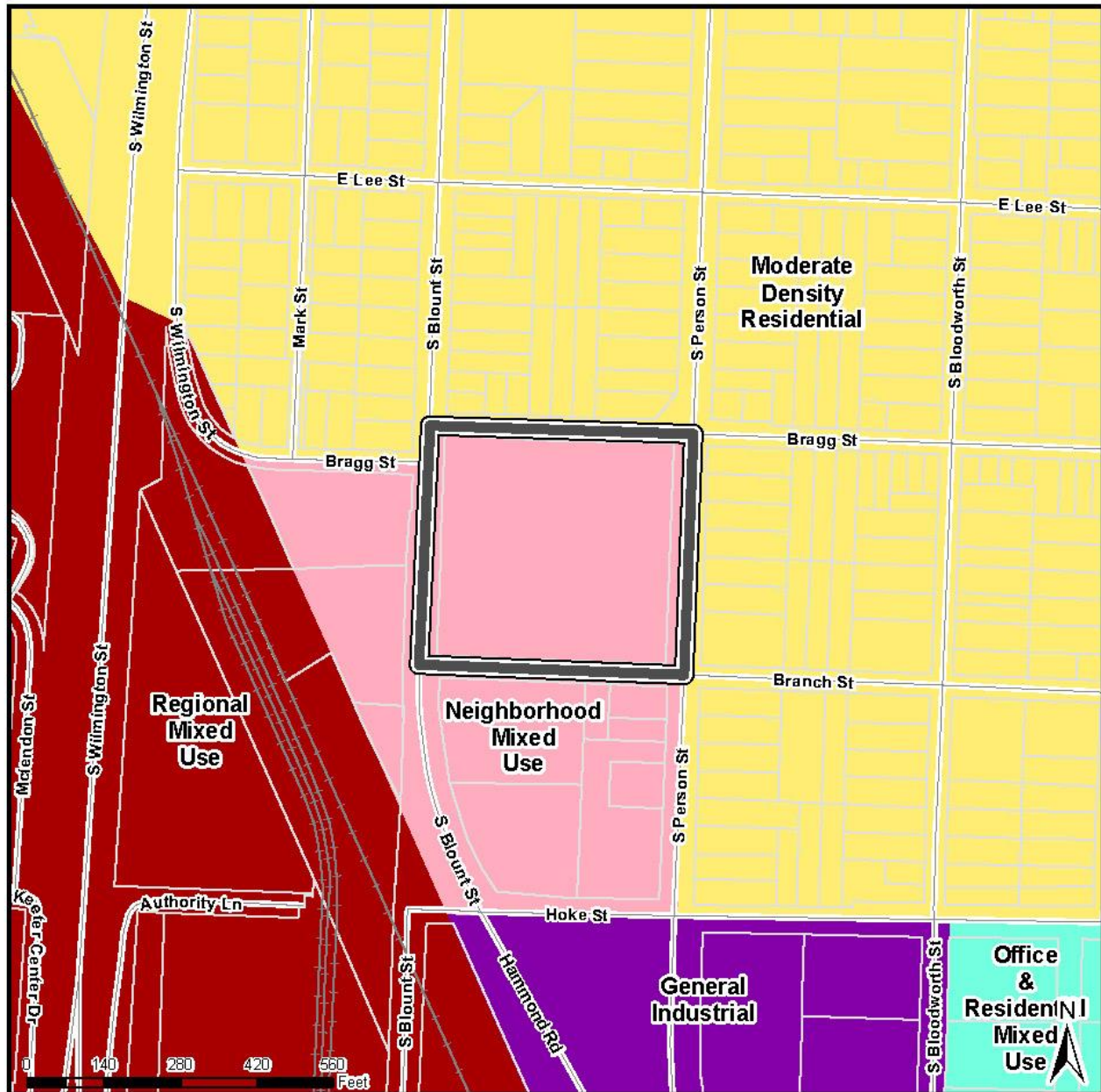
Property	1208 S Blount St
Size	3.84 acres
Existing Zoning	IX-3
Requested Zoning	PD



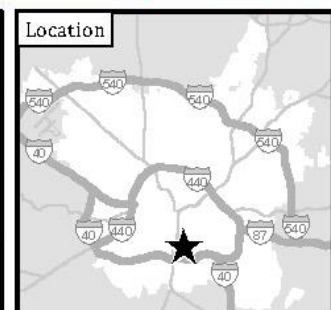
Map by Raleigh Department of City Planning (redkhowh): 4/17/2019

Future Land Use

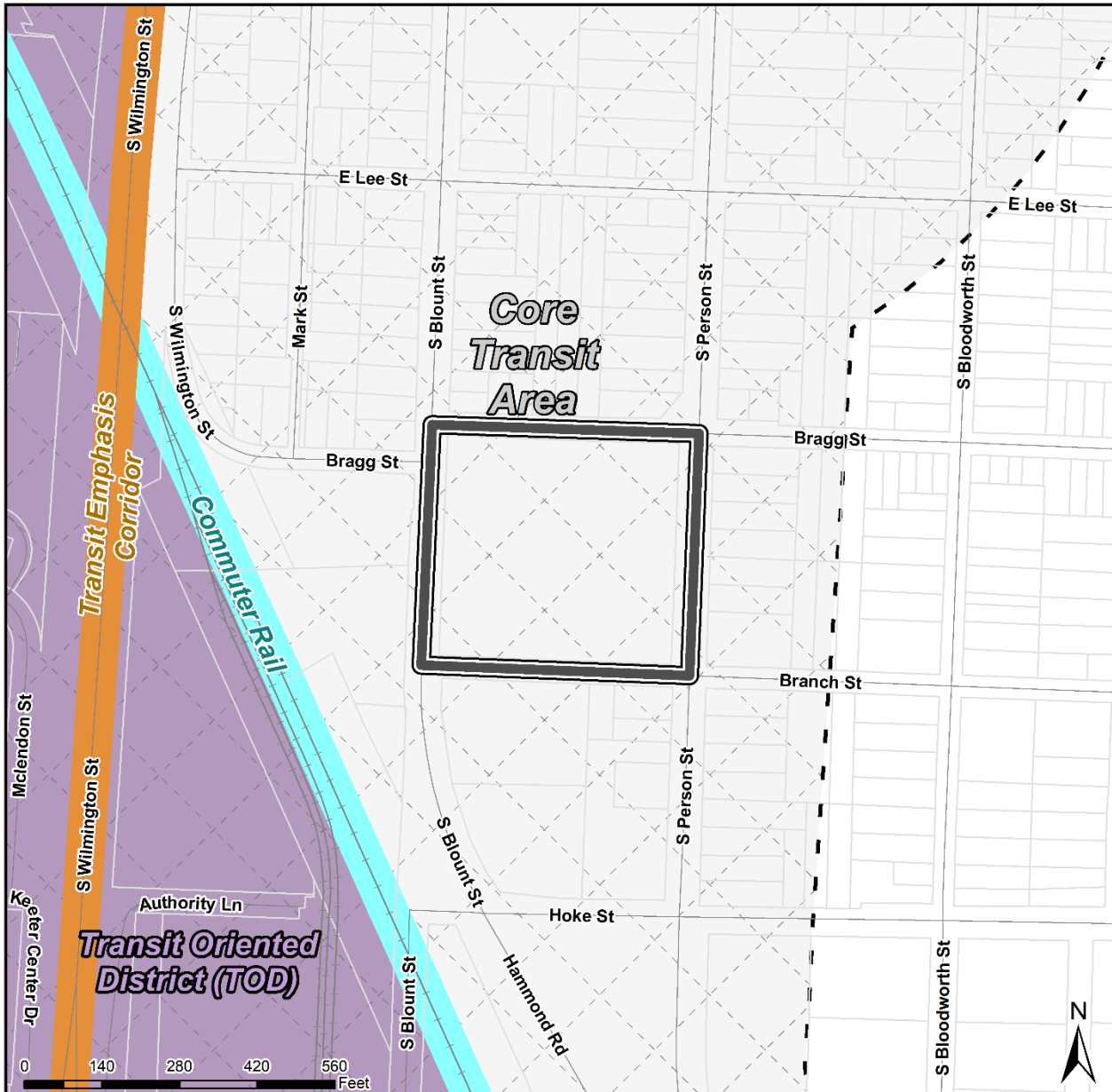
Z-14-2019



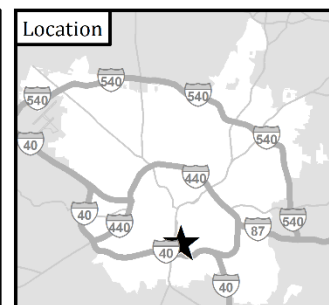
Property	1208 S Blount St
Size	3.84 acres
Existing Zoning	IX-3
Requested Zoning	PD



Map by Raleigh Department of City Planning (redkhowh): 4/17/2019



Property	1208 S Blount St
Size	3.84 acres
Existing Zoning	IX-3
Requested Zoning	PD



Map by Raleigh Department of City Planning (aullr); 6/24/2020

MASTER PLAN STANDARDS

Unless noted in this section, the master plan will use standards set by the base district of Neighborhood Mixed Use (NX)

SETBACKS/HEIGHT:

	Setbacks	Building Height
Tract A	0'	5 Stories/55' (fifth story limited to stairs, landing, open air deck)
Tract B	0'	3 Stories/50'

PARKING:

Parking requirements for the PD area will be those set for the Downtown Mixed Use (DX) district in UDO Section 7.1.3.A, or one space per unit after the first 16 units and no spaces for the first 10,000 square feet of nonresidential space. There will be no maximum number of parking spaces.

DEVELOPMENT INTENSITY:

Land use intensities for the development are allocated by tract. Limited amounts of development entitlement may be transferred among tracts according to the administrative process described in UDO Section 4.7.6.

	Max Dwelling Units	Max Commercial Floor Area
Tract A	100	none
Tract B	none	10,000
Total	100	10,000

PEDESTRIAN CIRCULATION:

The pedestrian network shown on sheet C5.00 would allow access throughout the development and with the surrounding sidewalk network. The common amenity area is also linked to the pedestrian network.

PHASING:

No phasing, built as a single phase.

OPEN SPACE:

10 percent of overall site area.

TREE CONSERVATION:

Tree conservation areas are not anticipated to be required at time of site review due to the lack of trees on the site.

STREET TYPOLOGY:

Street sections are provided on sheet C5.01. One shows a design for Blount and Person streets. It aligns with code requirements. The second would regulate the construction of new private alleys within the development. All streets in and adjacent to the rezoning site are proposed to be constructed according to these sections. Additional detail and analysis is contained in the transportation section below.

The block perimeter standard contained in UDO Article 8.3 is not proposed to be modified. The existing public streets bounding the site meet the block perimeter standard of the base district of Neighborhood Mixed Use (NX).

BUILDING TYPES:

	<i>Permitted Building Types</i>
Tract A (all but southeast corner)	Apartment
Tract B (southeast corner)	General or Mixed Use

COMMON SIGNAGE PLAN:

The master plan does not include a common signage plan. Signage in the site would be regulated by the base district of Neighborhood Mixed Use (NX).

ANALYSIS OF MASTER PLAN RESPONSES TO GENERAL DESIGN PRINCIPLES

(UDO Section 4.7.5. A-N provides a set of design principles that are to be used to evaluate PD rezoning applications.)

- A. When at least 20 residential units are proposed, the project includes a variety of housing stock that serves a range of incomes and age groups, and may include detached houses, attached houses, townhouses, apartments and dwelling units above first floor commercial spaces.

The master plan would allow apartments, but not other residential building types. This principle applies more clearly to larger master plans, not a relatively compact site as is the case here.

- B. Uses are compact and well-integrated, rather than widely separated and buffered.

The master plan allows for residential and commercial uses adjacent to each other. The reduced building setbacks proposed by the master plan may enhance accessibility between uses.

- C. Compatibility among different uses is achieved through effective site planning and architectural design.

Commercial uses are contained within a corner building, a traditional pattern of incorporating a mix of uses in urban places.

- D. A variety of business types are accommodated, from retail and professional offices to live-work. Office uses vary from space for home occupations to conventional office buildings. Retail uses range from corner stores to larger format supermarkets.

This principle applies more clearly to larger master plans.

- E. Special sites, such as those at a terminated vista, are reserved for public or civic buildings and spaces that serve as symbols of the community, enhancing community identity.

This principle applies more clearly to larger master plans. The project would include a central courtyard, creating a public gathering place.

- F. The project includes a variety of street types designed to be accessible to the pedestrian, bicycle and automobile. Streets are connected in a way that encourages walking and reduces the number and length of automobile trips.

This principle applies more clearly to larger master plans. The plan does include alleys and a central pedestrian way. The pedestrian areas will be accessible to the public.

- G. Bicycle circulation is accommodated on streets and on dedicated bicycle paths, greenways or trails with adequate bicycle parking facilities being provided at appropriate locations.

This principle applies more clearly to larger master plans. The only streets contained entirely within the development are alleys.

- H. Building facades spatially delineate the streets and civic spaces, and mask parking lots.

Parking would be accessed by internal alleys, allowing building facades to front the street.

- I. Architecture and landscape design are based on the local climate, topography, history and building practice.

The plan does not specify architecture or landscape design.

- J. The project includes open space as a significant element of the project's design. Formal and informal, active and passive open spaces are included. Open spaces may include,

but are not limited to, squares, plazas, greens, preserves, farmers markets, greenways and parks.

The plan includes a central courtyard, creating a public gathering place.

- K. The project is compatibly integrated into established adjacent areas, and considers existing development patterns, scale and use.

The property is a former industrial site and is currently a vacant lot. It is bordered by industrial uses on two sides and residential uses on the other two sides. The site serves as a transitional area between those two uses. The proposed development serves as a reasonable transition by restricting more intensive commercial uses compared to the existing IX zoning and limited commercial uses to the southeastern corner of the site while also allowing more housing opportunities.

- L. The project is a clearly identifiable or legible place with a unique character or unique tradition.

This principle may apply more clearly to larger master plans.

- M. Public art, including but not limited to, monuments, sculpture and water features, is encouraged.

The plan specifies that a local artist will be used to create on-site art that recognizes the history and significance of the South Park neighborhood.

- N. Entertainment facilities, including but not limited to, live music venues and theatres, are encouraged.

This principle applies more clearly to larger master plans.

COMPREHENSIVE PLAN

Determination of the conformance of a proposed use or zone with the Comprehensive Plan includes consideration of the following questions:

- A. Is the proposal consistent with the vision, themes, and policies contained in the Comprehensive Plan?

Yes. The proposal is consistent with the broad themes of the plan, particularly Coordinating Land Use and Transportation, as well as several specific policies, particularly those that encourage additional housing supply, mixed-use development, and greenhouse gas reduction.

- B. Is the use being considered specifically designated on the Future Land Use Map in the area where its location is proposed?

Yes. Uses include residential, office, retail, restaurant, and other commercial uses. In designating allowed uses, the proposed PD uses a base of Neighborhood Mixed Use zoning, which is consistent with the Future Land Use Map category of Neighborhood Mixed Use. It restricts auto-oriented uses, including vehicle fuel sales and vehicle sales and repair.

- C. If the use is not specifically designated on the Future Land Use Map in the area where its location is proposed, is it needed to service such a planned use, or could it be established without adversely altering the recommended land use and character of the area?

The uses, including residential uses, office, retail, and other commercial uses, are designated on the Future Land Use Map.

- D. Will community facilities and streets be available at City standards to serve the use proposed for the property?

Yes, existing infrastructure is adequate.

Future Land Use

Future Land Use designation: Neighborhood Mixed Use

The rezoning request is

☒ **Consistent** with the Future Land Use Map.

☐ **Inconsistent**

The proposal uses a base district of Neighborhood Mixed Use, which is fully compatible with the FLUM designation in terms of allowed uses.

The designation also includes guidance for height (table LU-2 in the Comprehensive Plan). The guidance is based on context. In areas within a quarter-mile of a bus rapid transit station, recommended heights are taller. BRT is currently in the planning stages along a southern corridor connecting downtown with Garner. The final alignment has not been determined, but previous planning has identified Wilmington Street as the preferred corridor. The site is one-tenth of a mile from Wilmington Street and approximately a quarter-mile from likely station locations. Given that, five stories for much of the site is appropriate. The northern and eastern edges of the site, however, also are near a lower-scale residential area, which suggests that four stories is more appropriate.

The PD would allow height of five stories, but with the fifth floor limited to a rooftop deck and means to access the deck. Overall height is limited to 55 feet, which is closer to a three-story height maximum (50 feet) than four story maximum (62 feet). Given those limitations, and given the potential proximity to BRT, the height is consistent with the Plan guidance.

Urban Form

Urban Form designation: Core Transit Area

The rezoning request is

☒ **Consistent** with the Urban Form Map.

☐ **Inconsistent**

The Urban Form Map identifies the site as within a “Core Transit Area,” which is within a quarter-mile of a corridor proposed for bus rapid transit. This indicates a more walkable urban form, with buildings closer to the street and minimal if any parking between the building and street. The PD specifies that parking will be accessed through internal alleys and provides a build-to range that ensures buildings will be at least relatively close to the street. This is consistent with the Core Transit guidance for an urban or hybrid frontage. Those provisions make the proposal consistent with the map.

Compatibility

The proposed rezoning is

☒ **Compatible** with the property and surrounding area.

☐ **Incompatible.**

The proposal would allow apartments and commercial space on a vacant former industrial site that is bordered on the east and west by major streets. Single-story detached houses are present along Bragg Street and Person Street. It is compatible with the area, although additional measures to address perceived scale and avoid blank walls, particularly along the Bragg Street side, which is a much narrower street than Person, would improve compatibility.

Public Benefits of the Proposed Rezoning

- The proposal would add housing and other uses in a walkable area that is well-served by transit and near downtown.
- The proposal would add to housing supply. While the existing IX zoning allows housing, the IX prohibition on first-floor housing can serve as an impediment.
- The proposal allows housing types that are relatively more affordable than new detached houses.
- The proposal would eliminate the potential for industrial uses, which are not as desirable for areas with such close proximity to a BRT corridor.
- The project would incorporate art that recognizes the history and significance of the South Park neighborhood.

Detriments of the Proposed Rezoning

- The proposal may be perceived as slightly out of scale with nearby residential areas, particularly along Bragg Street.

Policy Guidance

The rezoning request is **consistent** with the following policies:

LU 1.2 Future Land Use Map and Zoning Consistency

The Future Land Use Map shall be used in conjunction with the Comprehensive Plan policies to evaluate zoning consistency including proposed zoning map amendments and zoning text changes.

- The proposal, which uses Neighborhood Mixed Use as the starting point for PD modifications, is consistent with the FLUM category of Neighborhood Mixed Use.

LU 4.4 Reducing Vehicle Miles Traveled Through Mixed-use

Promote mixed-use development that provides a range of services within a short distance of residences as a way to reduce the growth of vehicle miles traveled (VMT).

- The proposal ensures a mix of uses, by specifying that both residential and nonresidential space is included. The resulting mix of uses will reduce VMT.

Policy LU 5.4 Density Transitions

Low- to medium-density residential development and/or low-impact office uses should serve as transitional densities between lower-density neighborhoods and more intensive commercial and residential uses. Where two areas designated for significantly different development intensity abut on the Future Land Use Map, the implementing zoning should ensure that the appropriate transition occurs on the site with the higher intensity.

- The proposal would serve as a largely residential transition between industrial zoning and uses to the south and residential to the north and east.

Policy LU 7.5 High-Impact Commercial Uses

Ensure that the city's zoning regulations limit the location and proliferation of fast food restaurants, sexually-oriented businesses, late night alcoholic beverage establishments, 24-hour mini-marts and convenience stores, and similar high impact commercial establishments that generate excessive late night activity, noise, or otherwise affect the quality of life in nearby residential neighborhoods.

- The proposal, by using NX as a base, prohibiting fuel sales, and restricting commercial uses to the southeastern corner of the site, limits the above uses adjacent to existing residential neighborhoods.

Policy LU 8.1 Housing Variety

Accommodate growth in newly developing or redeveloping areas of the city through mixed-use neighborhoods with a variety of housing types.

- The proposal would allow apartments next to an area largely characterized by detached houses.

Policy LU 8.10 Infill Development

Encourage infill development on vacant land within the city, particularly in areas where there are vacant lots that create “gaps” in the urban fabric and detract from the character of a commercial or residential street. Such development should complement the established character of the area and should not create sharp changes in the physical development pattern.

- The proposal would facilitate development on a vacant block near downtown. It would be largely residential and only slightly taller than what is allowed under adjacent zoning categories.

Policy LU 11.4 Rezoning/Development of Industrial Areas

Allow the rezoning and/or redevelopment of industrial land for non-industrial purposes when the land can no longer viably support industrial activities or is located such that industry is not consistent with the Future Land Use Map. Examples include land in the immediate vicinity of planned transit stations.

- Industrial use are not compatible with the Neighborhood Mixed Use designation.

Policy EP 1.1 Greenhouse Gas Reduction

Promote best practices for reducing greenhouse gas emissions as documented through the U.S. Mayors' Climate Protection Agreement.

- By allowing higher density in a walkable area served by transit and by allowing more energy-efficient housing types, the proposal would serve to reduce Raleigh's carbon emissions on a per capita basis.

Policy H 1.8 Zoning for Housing

Ensure that zoning policy continues to provide ample opportunity for developers to build a variety of housing types, ranging from single-family to dense multi-family. Keeping the market well-supplied with housing will moderate the costs of owning and renting, lessening affordability problems, and lowering the level of subsidy necessary to produce affordable housing. In areas characterized by detached houses, accommodations should be made for additional housing types while maintaining a form and scale similar to existing housing.

- The proposal would add to the housing supply and allow a housing type other than detached houses, which tend to be more expensive.

The rezoning request had been **inconsistent** with the following policy, but following a revision is now consistent:

UD 7.3 Urban Design Guidelines - #26

The ground level of the building should offer pedestrian interest along sidewalks. This includes windows, entrances, and architectural details. Signage, awnings, and ornamentation are encouraged.

- A revision made on September 11, 2020 remove the exemption from the blank wall standard along the two primary streets, Blount and Person, which the majority of the building area would face. An additional revision removed the exemption from

transparency requirements for the Apartment building type. Those adjustments make the project generally consistent with this policy.

Area Plan Policy Guidance

The rezoning request is **consistent** with the following Area Plan policies:

AP-SP 2 South Park Focal Point

Encourage commercial, office, and residential uses in the area defined by Person Street, Bragg Street, Hammond Road, and Hoke Street in order to create a neighborhood focal point and economic development opportunity for the South Park area.

- The proposal would allow denser residential and commercial space in the block. While the amount of allowed commercial space is modest, 10,000 square feet is sufficient to create a neighborhood focal point.

HOUSING AFFORDABILITY & ENERGY EFFICIENCY ANALYSIS

Carbon Footprint: Transportation

	City Average	Site	Notes
Transit Score	30	48	In addition to existing transit, Wilmington Street is a potential BRT corridor.
Walk Score	30	50	The rezoning would improve the score by introducing a greater mix of uses.

Source: Walk Score is a publicly available service that measures pedestrian friendliness by analyzing population density and road metrics such as block length and intersection density. The higher the Transit Score or Walk Score, the greater the percentage of trips that will be made on transit or by walking, and the smaller the carbon footprint. The scores also correlate with shorter vehicle trips, which also produce less carbon.

Summary: The site is more walkable and transit-served than the city on average, and the rezoning and planned transit improvements would improve the scores. Adding housing here would decrease the city's carbon emissions on a per-capita basis.

Carbon/Energy Footprint: Housing

Housing Type	Average Annual Energy Use (million BTU)	Permitted in this project?
Detached House	82.7	No
Townhouse	56.5	No
Small Apartment (2-4 units)	42.1	Yes
Larger Apartment	34.0	Yes

Source: U.S. Energy Information Administration, 2015 survey. Statistics for residential structures in the South.

Summary: The rezoning would only allow the most energy-efficient building type, apartments. By facilitating the construction of housing on the site, the rezoning would decrease the city's carbon emissions on a per-capita basis.

Housing Supply and Affordability

Does it add/subtract from the housing supply?	Adds	The current IX zoning does not allow ground-floor residential uses.
Does it include any subsidized units?	No	
Does it permit a variety of housing types beyond detached houses?	Yes	The proposal allows the apartment and mixed-use building types.
If not a mixed-use district, does it permit smaller lots than the average?*	N/A	This is a mixed-use zoning district.
Is it within walking distance of transit?	Yes	

**The average lot size for detached residential homes in Raleigh is 0.28 acres.*

Summary: The rezoning would facilitate the development of apartments, which tend to be more affordable than detached houses.

IMPACT ANALYSIS

Historic Resources

The site is not located within a National Register Historic District or Raleigh Historic Overlay District. It is adjacent to the East Raleigh / South Park National Register Historic District. It does not include nor is adjacent to any National Register individually-listed properties or Raleigh Historic Landmarks

Impact Identified: None requiring mitigation

Parks and Recreation

1. This site is not directly impacted by any existing or proposed greenway trails, corridors, or connectors.
2. Nearest existing park access is provided by Bragg St. Park (45 feet) and Junious N. Sorrell Park (0.2 miles).
3. Nearest existing greenway trail access if provided by Little Rock Greenway Trail (0.3 miles).
4. Current park access level of service in this area is graded an A letter grade.
5. Raleigh's Comprehensive Plan has many policies encouraging the addition of park elements in the context of developments such as this. The PD master plan provides some direction orienting this development toward the adjacent Bragg St. Park and integrating public realm enhancements into the site design. This could be enhanced with the addition of amenities not currently provided at nearby public parks, but which may be in demand due to the increased development entitlement proposed by this rezoning, such as a small dog park.

The master plan should be reviewed with consideration of the following Comprehensive Plan policies:

- a. Comp Plan PR 1.7 New Parks in Growth Centers "Create new urban parks and enhance existing urban parks throughout Growth Centers using proactive planning, partnerships and innovative approaches"
- b. Comp Plan PR 4.8 Private Parks "Encourage the provision of tot lots, pocket parks, and other privately-held and -maintained park spaces within residential developments to complement public park facilities"
- c. Comp Plan PR 5.4 Improving Park Access "Public spaces should be included in private developments that can connect to and benefit from their proximity to public infrastructure and spaces such as greenway trails, public sidewalks, and plazas"
- d. Comp Plan PR 5.5 Encourage Public Open Space in Rezonings "Encourage the provision of publicly accessible open space during the consideration of zoning petitions"
- e. Comp Plan AC 1.1 Public Art and Neighborhood Identity "Encourage the use of public art to create a neighborhood identity."

Impact Identified: None requiring mitigation

Public Utilities

	Maximum Demand (current use)	Maximum Demand (current zoning)	Maximum Demand (proposed zoning)
Water	0	39,800 gpd	26,040 gpd
Waste Water	0	39,800 gpd	26,040 gpd

1. The proposed rezoning would add approximately 26,040 gpd to the wastewater collection and water distribution systems of the City. There are existing sanitary sewer and water mains adjacent to the proposed rezoning area
2. At the time of development plan submittal, a Downstream Sewer Capacity Study may be required to determine adequate capacity to support the proposed development. Any improvements identified by the study would be required to be permitted prior to the issuance of Building Permit & constructed prior to release of a Certificate of Occupancy
3. Verification of water available for fire flow is required as part of the Building Permit process. Any water system improvements recommended by the analysis to meet fire flow requirements will also be required of the Developer

Impact Identified: None requiring mitigation

Stormwater

Floodplain	No FEMA floodplain present
Drainage Basin	Rocky/Walnut
Stormwater Management	Subject to stormwater regulations under Article 9 of the UDO
Overlay District	None present

1. No downstream structural impacts identified.
2. Lot subject to UDO 9.2 and UDO 9.4

Impact Identified: None requiring mitigation

Transportation

Site Location and Context

Location

The Z-14-19 site is located south of Downtown Raleigh, encompassing the entire block bounded by South Blount, Bragg, South Person, and Branch Streets.

Area Plans

The Z-14-19 site is located within the boundaries of the South Park Area Plan.

Policy AP-SP 1 of this Area Plan calls for pedestrian improvements along Blount and Person Streets, as well as other locations in the area plan. The sidewalk along South Person Street was completed by the City of Raleigh as a Capital Improvement Program Project in the Spring of 2018.

The site is also within the study area of the Blount Street – Person Street Corridor Study. This plan proposes three sets of improvements to the streets:

1. Completion of missing sidewalk segments and installation of streetscape improvements such as curb extensions and medians. While this Z-14-19 site already has contiguous sidewalks, there is not currently a programmed project to install streetscape improvements near this site. It would be consistent with the Blount Street – Person Street Corridor Study to install curb extensions at the corners of the block.
2. Installation of a bicycle lane and a consistent two-lane street cross section in a one-way configuration. This project was recently completed through a City of Raleigh Capital Improvement Program Project.
3. The third improvement is conversion of Blount and Person Streets to two-way operations. This project was funded by the 2017 Transportation Bond and is entering the design phase as a City of Raleigh Capital Improvement Program Project.

The site is also near border of the Southern Gateway Corridor Study and the adjacent to the Wake BRT: Southern Corridor study area. For more details on the implication of these plans to this case, refer to the transit section below.

Existing and Planned Infrastructure

Streets

The subject property is bounded on the east by South Person Street and the west by South Blount Street. These streets are NCDOT maintained and are specified as a 4-lane parallel parking in Map T-1 of the Comprehensive Plan. This street typology includes two general travel lanes and a bicycle lane in each direction alongside a parallel parking lane and divided by a landscaped median. It is the City of Raleigh's practice to designate two halves of a one-way pair system together as a median divided street section, thus, each street includes two lanes, a bicycle lane, and parallel parking. Master Plan sheet C5.01 correctly shows the applicable street cross section for South Blount and South Person Streets. The correct cross section is a 45-foot-wide street on a 77-foot-wide right-of-way.

Based on the dimensions on construction plans for the recently completed restriping project, existing dimensions of Blount and Person Street are as follows:

- **Blount Street:**
 - The northern half of the block is approximately 41 feet wide (measured from the back of the existing curbs). This width widens on the west side as Blount Street approaches Hoke Street and Hammond Road.
 - At the northern half of the block, the current striping plan includes on-street parking on both sides of the street and a bicycle lane between the curb parking on the east and the rightmost southbound travel lane. The width dimensions for the parking lanes, travel lanes, and the bicycle lane are all minimums and do not meet width standards in the Raleigh Street Design Manual.
 - Frontage widening required of this Planned Development to Blount Street would be approximately two feet of additional street width on the east side.
 - Without redevelopment of the lot at the southwest corner of Bragg and Blount Street (Neighbor to Neighbor Ministries), the resulting street width would be three feet below standards in the Raleigh Street Design Manual.
 - In order to achieve a higher quality of project design, the Master Plan *could* provide additional widening of Blount Street on the northern portion of the site in order to provide a full 45 feet wide street for the entire block.
- **Person Street:**
 - The entire block is approximately 45 feet wide (measured from the back of the existing curbs).
 - The striping plan includes on-street parking on both sides of the street and a bicycle lane between the curb parking on the east and the rightmost northbound travel lane. The existing striping modifies the standards in the Raleigh Street Design Manual, providing ten-foot travel lanes and a two-foot buffer for the bicycle lane.
 - There does not appear to be any frontage widening on Person Street required of this Planned Development.

The site is bounded on the north by Bragg Street and the south by Branch Street. These streets are city-maintained and are not designated in Map T-1 of the Comprehensive Plan. Given the proposed density and the context, they should be considered Neighborhood Streets. The typical section for Bragg and Branch Streets is provided with a note, but not a graphic on Master Plan sheet C5.01. The dimensions for Bragg and Branch Streets shown on Master Plan sheet C3.00 appear to conflict with the curb to curb width requirements.

In accordance with UDO section 8.3.2, the maximum block perimeter for Planned Development Zoning districts is 4,000 feet. The base district of NX has maximum block perimeter standards of 2,500 feet for heights 5 stories and over and 3,000 feet for heights under 5 stories. The block perimeter for Z-14-19 is approximately 1,640 feet, matching the pattern of other nearby blocks.

Pedestrian Facilities

There are existing sidewalks on South Blount and Person Streets near the site and along its frontage. The Master Plan includes improvement of the sidewalk to 16 feet width. There are no current sidewalks along the Branch or Bragg Street frontages. Sidewalks will be required at the time of site plan.

Staff suggests construction of curb extensions at the four corners of the site, as suggested in the Blount Street – Person Street Corridor Study. A similar improvement was made to Moore Square as a part of the park improvement project completed in 2019. This improvement would be consistent with adopted plan as well as improve the pedestrian environment of the Master Plan.

Master Plan Sheet C5.00 shows pedestrian access within the site. One route connects Bragg Street to Branch Street through the central amenity area on a north-south orientation. Two routes connect the two alleys along an east-west orientation. The southmost of the east-west pedestrian access routes connects to the sidewalk on South Person Street in Tract B of the PD. It is not clear if these pedestrian accesses will have public access easement or what their design will be. Staff suggests using the standards of the Pedestrian Passage (UDO Section 8.4.8.B).

Bicycle Facilities

There are one-way bicycle lanes on Blount and Person Streets. In the BikeRaleigh Plan, Bragg Street is planned to be an east-west neighborhood bikeway connecting several north-south bikeway and greenway trail corridors in Southeast Raleigh.

The site is within the Phase I bikeshare service area. Existing nearby stations are at the intersection of Person and South Streets, at the Performing Arts Center, and at John Chavis Memorial Park. Each of these locations is approximately a half mile from the site. Other nearby stations are at the Walnut Creek Wetland Park, Dix Park, and the North Carolina State Farmers' Market. Bikeshare station spacing guidelines indicate that an additional station in the vicinity of this PD may be appropriate. A bikeshare station can likely be accommodated within Branch and/or Bragg Streets in space that would be otherwise dedicated to on-street parking as specified in the standards for a Neighborhood Street. Alternatively, a station could be accommodated within the development's open space.

Transit

The site is currently served with by two different GoRaleigh services. Route 21 operates on South Blount Street in the southbound direction while route 13 operates on South Person in the northbound direction. Both Services operate every 30 minutes.

The Southern Gateway Corridor Study recommended that Bus Rapid Transit between Downtown Raleigh and Garner follow South Wilmington Street. The plan also makes redevelopment recommendations for the "Cargill" focus area, which is less than 1000 feet from the Master Plan site. The City of Raleigh is currently studying alternative alignments and station locations through the Wake BRT: Southern Corridor planning process.

It is uncertain at this time, but if the South Wilmington Street BRT alignment is selected, a BRT station would serve the Cargill focus area near Hoke Street and the Master Plan would be within a half mile of BRT Service. All current BRT scenarios include a station near the intersection of Wilmington and Lenoir Streets; this station will be approximately 0.6 mile from the Master Plan site.

Access

Access to the subject site should be via the Branch and Bragg streets given driveway spacing standards for major streets. The Master Plan specifies two alleys connecting Branch to Bragg street and no vehicle access to South Blount or South Person Streets. The alleys are specified to the standards of UDO Section 8.4.7.C (Mixed Use Alley).

Other Projects in the Area

The refer to the Area Plans section for details on projects to implement the Blount Street – Person Street Corridor Study. There are no other programmed transportation projects near the site.

Traffic Impact Analysis (TIA)

This is a proposed Planned Development zoning district. As such, a traffic study is required for case Z-14-19. A TIA was submitted and reviewed by city staff. The rezoning would result in a decrease in average daily vehicle trips. A memo outlining the findings is included with these materials.

Impact Identified: None requiring mitigation

Urban Forestry

1. Street tree plantings are being shown consistent with UDO requirements.
2. Per the plan, there are no potential tree conservation areas on the site.

Impact Identified: None

Impacts Summary

The rezoning would result in lower vehicular trips compared to existing zoning. It would result in higher demand for infrastructure and services in comparison to the existing vacant lot, but adequate infrastructure is available.

Mitigation of Impacts

No impacts require mitigation.

CONCLUSION

The proposed PD is consistent with the Future Land Use Map and Urban Form Map and several key policies, including those involving infill development, reducing vehicle miles traveled, providing more housing supply and choice, and addressing high-impact commercial uses near residential neighborhoods.

Because of the site's walkability and access to transit and because the apartment building type is the most energy-efficient housing type, residents would have a smaller carbon footprint, making it consistent with the Greenhouse Gas Reduction policy and the Comprehensive Plan's theme of environmental protection.

A PD allows the modification of code standards. Generally, the proposed modifications are consistent with the Comprehensive Plan, such as reducing the amount of required vehicle parking in a walkable and transit-rich area. However, the modifications to transparency and blank wall standards are inconsistent with the Plan's urban design guidelines. While the proposal is generally consistent with the Plan, addressing that issue would improve overall consistency and result in a better outcome.

CASE TIMELINE

Date	Action	Notes
4/17/19	Submitted	Resubmittal required
11/4/19	Central CAC vote	16-0 in favor
4/21/20	Resubmitted	Addressed previous issues, but revised TIA required to match changed commercial entitlement
6/1/20	Revised TIA provided	Approved on 6/20
6/24/20	Technical revisions	
6/30/20	On PC agenda	For deferral

APPENDIX

SURROUNDING AREA LAND USE/ ZONING SUMMARY

SUBJECT PROPERTY		NORTH	SOUTH	EAST	WEST
Existing Zoning	IX-3	R-10	IX-3/NX-3	R-10	IX-3
Additional Overlay	-	South Park NCOD	-	South Park NCOD	-
Future Land Use	Neighborhood Mixed Use	Moderate Residential	Neighborhood Mixed Use	Moderate Residential	Neighborhood Mixed Use
Current Land Use	Vacant	Residential/ park	Retail/vehicle storage/ residential	Residential	Residential/ vacant
Urban Form	Core Transit	Core Transit	Core Transit	Core Transit	Core Transit/ Commuter Rail Corridor

CURRENT VS. PROPOSED ZONING SUMMARY

	EXISTING ZONING	PROPOSED ZONING
Zoning	IX-3	PD (NX base)
Total Acreage	3.84	3.84
Setbacks:		
Blount/Person	3'	5'-55' build-to
Bragg/Branch	3'	5'-55' build-to
Residential Density:	27 units/acre	26 units/acre
Max. # of Residential Units	103	100
Max. Gross Office SF	130,000	10,000
Max. Gross Retail SF	103,000	10,000
Max. Gross Industrial SF	347,000	-

*The development intensities for proposed zoning districts were estimated using an impact analysis tool. The estimates presented are only to provide guidance for analysis.

Relevant minutes from Sept. 22, 2020 Planning Commission meeting.

AGENDA ITEM (E): OLD BUSINESS

AGENDA ITEM (E) 1: Z-14-19/MP-1-19 – 1208 South Blount Street

This case is located 1208 South Blount Street, consisting of the block created by Blount, Person, Bragg and Branch Streets.

This is a request to be rezoned from Industrial Mixed Use-Three Stories (IX-3) to Planned Development (PD). The PD uses Neighborhood Mixed Use (NX) as a base district. It prohibits vehicle fuel sales, vehicle sales, and vehicle repair. It limits residential units to 100 and commercial space to 10,000 square feet. It limits height to five stories and 55 feet, with the top floor restricted to use as a rooftop deck.

Without objection Mr. O'Haver has requested to be recused from this case as his firm is involved in the rezoning.

Planner Hardin gave a brief overview of the case regarding updates made to conditions; transparency and blank walls removed; maximum blank wall along Blount Street; additional provisions; specifying materials used in facades; provision to require applicant to use a local artist; median income and analysis sales prices for detached houses to address affordable housing

There discussion regarding amenity areas and opens space and how the space in the center of the site would be used.

Worth Mills gave a brief overview of the open area regarding his belief that there would be a space for people to sit and enjoy themselves; residents would have their own personal space but they are trying to create an open and active space for the residents.

There was also discussion regarding outreach with the neighborhood leaders.

There was further discussion regarding the open space being open to the public.

Planner Hardin spoke regarding the PD document addresses the open space concerns and states that additional language can be added to address how the blank wall articulation will work.

There was further discussion regarding how the neighborhood common spaces would be maintained and could the art be tied to home-owners association.

Mr. Mills responded that there would be some sort of homeowner association.

Planner Hardin responded that language could be added to state that the public art would be maintained by the homeowners association.

There was further discussion regarding clarification of accessibility and maintenance of public art; number of proposed units; concern of unaffordability and open space.

Ms. Fox made a motion to approve the case with technical clarifications. Ms. Winters seconded the motion.

Commissioners how do you vote?

Bennett (Aye) Fox (Aye) Hicks (Aye) Lampman (Aye) Mann (Nay) McIntosh (Nay) O'Haver (recused) Chair Tomasulo (Aye) Winters (Aye). The vote was not unanimous 6-2.

**CAROLINA COACH MIXED USE
MASTER PLAN
OCTOBER 5, 2020**

1. INTRODUCTION

This document and the associated plan sheets submitted herewith (collectively, the Master Plan) are provided pursuant to the Unified Development Ordinance for the Planned Development district for the Carolina Coach Mixed Use development (the “Project”). The Project concerns the 3.84 acres located on the southeast quadrant of the S. Blount Street and Bragg Street intersection. The property is located a few blocks south of the Downtown area, which is bounded by Martin Luther King Jr. Boulevard to the south; the railroad tracks from Raleigh Union Station are approximately 300 feet west of the property. The Master Plan proposes a mixed-use development consisting of residential, commercial and open space land uses with building heights of five (5) stories maximum. Commercial space will be located at block intersections and/or along the Bragg Street frontage.

2. STATEMENT OF INTENT

The proposed development meets the intent of the PD District set forth in UDO Section 4.1.1.F. in multiple ways. The PD District allows the applicant to modify various UDO standards that better enable the applicant to develop a more compact, pedestrian-friendly, mixed-use project than would otherwise be possible without the modifications.

3. COMPREHENSIVE PLAN CONFORMANCE

The Future Land Use Map identifies the property as Neighborhood Mixed Use. Neighborhood Mixed Use encourages a mix of commercial uses that serve the immediately surrounding neighborhood, residential uses and mixed uses. Neighborhood Mixed Use encourages pedestrian-oriented mixed-use developments, and acknowledges that four-to-five-story buildings may be appropriate in walkable areas with pedestrian-oriented businesses. Additionally, the property is located in the South Park Area Plan boundaries, and identified as a portion of the “South Park Focal Point”. The focal point encourages commercial, office and residential uses to create neighborhood focal point and economic development opportunity for the South Park area. The Project is consistent with this policy guidance given the proposed commercial, residential and open space land uses that work together to further the area plan’s mission for a South Park focal point.

4. URBAN DESIGN GUIDELINE CONFORMANCE

The property is located within a Core/Transit area on the Urban Form Map, but is also within 150’ of a low- to moderate-density residential community; thus, the property should be considered a General area to evaluate the Project’s proposed height. Properties zoned Neighborhood Mixed Use and considered General areas are recommended to cap building height at four stories and sixty-two feet (62’). While the Project proposes a maximum height of five stories, only four stories are to be used as livable space. Additionally, the Project limits maximum

building height to fifty-five feet (55'). By placing use restrictions on the fifth story and lowering the maximum building height (in feet) below that of a typical four-story building, the Project conforms with relevant Urban Design principles.

The Project complies with many of the Urban Design Guidelines. First, the Person Street right-of-way and building setbacks provide an urban street design by placing buildings close to the street. The majority of the project's building mass will be 4 stories, with provisions made for rooftop access. The property encompasses an entire block and has no abutting properties. Bragg Street (planned for a 64'-wide right-of-way) and S. Person Street (planned for a 77'-wide right-of-way) provide adequate separation from the property to the single-family residences across both streets. This buffer follows the intent of the UDO's Neighborhood Transition regulations, and is consistent with the Urban Design guidance in Policy UD 2.4 Transitions in Building Intensity. Second, the north-south orientation of the condominium units will line Blount and Person Streets with buildings and primary entrances per Policy UD 2.1 Building Orientation, which allows for green space accessibility along Branch and Bragg Streets; this site layout will enhance the physical definition of the block and increase pedestrian interest per Urban Design Guidelines 6 and 7. Third, additionally, the Project's sidewalk improvements will promote walkability along the entire block perimeter, with primary pedestrian access to the open area on the less-travelled Bragg and Branch Streets. Lastly, all of the aforementioned streets will be lined with street trees that both buffer residences from the right-of-way and offer shade for pedestrians. In sum, the Project's adherence to the Urban Design Guidelines should create a space that is welcoming to both residents and South Park neighbors.

5. GENERAL DESIGN PRINCIPLES CONFORMANCE

The Project complies with many of the General Design Principles set forth in UDO section 4.7.5. Specifically, the Project includes the opportunity to provide condominium units with rooftop terrace access alongside commercial space, and it proposes a compact, well-integrated mix of land uses instead of such uses being widely separated and buffered. The Master Plan also incorporates a green space within the center of the development to create an active open space and to break down the scale of the development to compliment the nearby single-family residential uses. The Master Plan also ensures compatibility with surrounding land uses by acting as a transition in both use and scale from industrial uses to the west and to lower-density South Park Historic District to the east. Additionally, the Project's open space – with its north-south orientation – provides access via Branch and Bragg Streets, and creates continuity in active space with nearby Bragg Street Park. The Project anticipates providing a mix of residential and non-residential uses. The urban design, build-to standards and parking standards ensure that buildings will front along the streets and parking will be located behind buildings, creating a pedestrian-friendly environment.

6. BLOCK PERIMETER

The block perimeter shall be established by the intersections of Bragg, Person, Branch and Blount Streets. This block perimeter totals approximately 1,650 feet, and conforms to the Block Perimeter standards in Section 8.3.2 of the UDO.

7. STREET SECTIONS

Map T-1 of the Comprehensive Plan identifies both S. Blount Street and S. Person Street as Avenue 4-Lane, Parallel Parking. Currently, both are one-way streets. It is the City of Raleigh's practice to designate two halves of a one-way pair system together as a median divided street section. Thus, each street includes two travel lanes, a bicycle lane, and parallel parking on both sides of the street, resulting in a forty-five feet (55')-wide street and a seventy-seven feet (77')-wide right-of-way. While not designated on Map T-1, Bragg Street and Branch are considered Neighborhood Streets by the Transportation Department. The improvements for all four streets are shown on Master Plan sheet C5.01.

8. LAND USES & INTENSITY

- A. Base District. The base district for this Planned Development district shall be the Neighborhood Mixed Use (NX) district. All permitted, limited and special principal uses and accessory uses permitted in the -NX district shall be permitted, limited and/or special uses on the property in accordance with the UDO, except as otherwise modified by this Master Plan.
- B. Overall Maximum Development Intensity. The total amount of development on the Property shall not exceed the intensities for each use as set forth below:
 - i. Commercial– 10,000 SF, which can be located in Mixed-Use and General building types
 - ii. Residential Dwelling Units – 100 units, which can be located in Apartment building types
 - iii. General Notes
 - 1. The term “commercial” shall not act as a limitation or prohibition on those allowable Principal uses for NX- districts as shown in UDO Section 6.1.4., unless specifically prohibited in Section 10 of this Master Plan.
 - 2. Any principal use other than a residential dwelling unit shall be classified as “commercial” for this Master Plan.
- C. Tract A Maximum Development Intensity: The total amount of development on the Property shall not exceed the intensities for each use as set forth below:
 - i. Residential Dwelling Units – 100 units, which can be located in Apartment building types
- D. Tract B Maximum Development Intensity: The total amount of development on the Property shall not exceed the intensities for each use as set forth below:
 - i. Commercial – 10,000 SF, which can be located in Mixed-Use and General Building types

9. MODIFICATIONS

- A. Parking Standards – The Planned Development shall comply with the Downtown District (DX-) vehicle parking standards in UDO Section 7.1.3. It is expressly

- understood that the vehicle parking maximum standard of UDO Section 7.1.3.A.1.a. shall not apply. All parking to be accessed by private alley.
- B. Vehicle Parking Lot Islands – The vehicle parking lot island requirements in UDO Section 7.1.7 shall not apply to parking spaces located within a building's footprint.
 - C. Utility Easement – There shall be no five-foot (5') utility easement requirement along Person Street and Blount Street.
 - D. Street sections will be those shown on Sheet C.5.01 Typical Roadway Sections.
 - E. 10% of the gross site area will be open space, and will be located on Tract A.
 - F. Outdoor amenity area will not be required for Tract A or Tract B.
 - G. The following base dimensional standards, also found in Table 1 of Sheet C3.00, shall apply for any Apartment building type:
 - i. The minimum lot area shall not apply.
 - ii. The maximum lot area shall not apply.
 - iii. The building width in primary build-to (min.) shall be 55%.
 - iv. The building width in side build-to (min.) shall be 20%.
 - v. The ground floor elevation (min) shall be 0'.
 - vi. The blank wall area (max.) standard shall not apply along Bragg Street and Branch Street.
 - H. The following base dimensional standards, also found in Table 1 of Sheet C3.00, shall apply for any General building type:
 - i. The maximum lot area shall not apply.
 - ii. The building/structure setbacks from an alley (min.) shall be 4'.
 - I. The following base dimensional standards, also found in Table 1 of Sheet C3.00, shall apply for any Mixed Use building type:
 - i. The maximum lot area shall not apply.
 - ii. The building/structure setbacks from an alley (min.) shall be 4'.
 - iii. The ground story height, floor to floor (min.) shall be 11'.
 - iv. The ground story (min.) transparency standard shall be 33%.

10. ADDITIONAL DEVELOPMENT STANDARDS

- A. The following principal uses as listed in UDO section 6.1.4 shall be prohibited:
 - i. Vehicle fuel sales
 - ii. Vehicle sales/rental
 - iii. Detention center, jail, prison
 - iv. Vehicle repair (minor)
- B. The maximum building height for any building on Tract A will be 5 stories and fifty-five feet (55'). The maximum building height for any building on Tract B will be 3 stories and fifty feet (50').
- C. The fifth floor of any building containing residential dwelling units shall be limited to the following uses: conditioned space will be limited to vertical circulation, including stairs, landings, and vestibules; the remainder will be open air decks.
- D. Build-To standards for the General and Mixed Use building types shall follow Table 1 of Master Plan sheet C3.00.
- E. The maximum blank wall area for a General building type shall be 20'.

- F. The Project shall comply with NX- sign standards.
- G. Building orientation - A minimum of 40% of units will have a primary pedestrian access on the public streets. The remaining units will have primary pedestrian access from the common open space which is ADA-accessible.
- H. The facades of the building shall be constructed from one or more of the following materials: glass, concrete, clay or brick masonry, stone masonry, stucco, cementitious siding, native and manufactured stone, and pre-cast concrete.
- I. Synthetic stucco (EIFS) and vinyl shall be prohibited as building siding materials. However, vinyl windows, decorative elements and trim are permitted.
- J. Developer shall engage a local artist to create on-site art that recognizes the history and significance of the South Park neighborhood. Said art shall be maintained and preserved by the property owners' association responsible for common area maintenance.
- K. The Open Space shall be publicly accessible. No barriers shall be erected to prevent the Open Space from public accessibility.

CAROLINA COACH

1208 S. BLOUNT ST.
RALEIGH, NC

PLANNED DEVELOPMENT MASTER PLAN

MP-1-19
Z-14-19

TRANSACTION 593222
SUBMITTED ON APRIL 3, 2019
RESUBMITTED ON MARCH 26, 2020
THIRD SUBMITTAL ON SEPTEMBER 13, 2019
FOURTH SUBMITTAL ON MARCH 26, 2020

SHEET INDEX	
SHEET NO.	SHEET NAME
C0.00	COVER
C1.00	EXISTING CONDITION PLAN
C3.00	DEVELOPMENT AND PARKING PLAN
C5.00	PEDESTRIAN CIRCULATION GREENWAY CONNECTION PLAN
C5.01	TYPICAL ROADWAY SECTIONS
C5.02	OVERALL STORMWATER PLAN
C6.00	OVERALL UTILITY PLAN UTILITY SERVICE PLAN
L1.00	TREE CONSERVATION PLAN
A1.00	ARCHITECTUAL ILLUSTRATIVES

- NOTE:
- REFER TO CAROLINA COACH MIXED USE MASTER PLAN NARRATIVE FOR ADDITIONAL DETAIL.
 - NX SIGN STANDARDS WILL APPLY



VICINITY MAP
SCALE: 1" = 200'

LOCATION: 1208 S BLOUNT STREET
PIN:1703738654
EXISTING ZONING: INDUSTRIAL MIXED USE WITH 3 STORIES (IX-3)
PROPOSED ZONING: PLANNED DEVELOPMENT
INSIDE CITY LIMITS: YES
TOTAL SITE AREA: 3.84 ACRES

Rezoning Application

RCP

RALEIGH
DEPARTMENT OF
CITY PLANNING

Department of City Planning | 1 Exchange Plaza, Suite 300 | Raleigh, NC 27601 | 919-996-2682

REZONING REQUEST

☐ General Use

☐ Conditional Use

☒ Master Plan

Existing Zoning Base District IX Height 3 Frontage Overlay(s)

Proposed Zoning Base District PD Height Frontage Overlay(s)

Click [here](#) to view the Zoning Map. Search for the address to be rezoned, then turn on the 'Zoning' and 'Overlay' layers.

If the property has been previously rezoned, provide the rezoning case number: Z-27B-2014

Provide all previous transaction numbers for Coordinated Team Reviews, Due Diligence Sessions, or Pre-Submittal Conferences:

560898

OFFICE
USE ONLY

Transaction #

Rezoning Case #

GENERAL INFORMATION

Date 04/03/2019 Date Amended (1) Date Amended (2)

Property Address 1208 S Blount Street

Property PIN 1703-73-8654 Deed Reference (book/page) 17323 / 2707

Nearest Intersection S Blount Street and Branch Street

Property Size (acres) 3.84 (For PD Applications Only) Total Units 100 Total Square Feet 10,000

Property Owner/Address SM Raleigh, LLC
4020 Westchase Boulevard, Suite 470
Raleigh, NC 27607

Phone

Fax

Email

Project Contact Person/Address Michael Birch, Longleaf Law Partners
2235 Gateway Access Point, Suite 201
Raleigh, NC 27607

Phone 919.645.4317 Fax

Email mbirch@longleaflp.com

Owner/Agent Signature Email

A rezoning application will not be considered complete until all required submittal components listed on the Rezoning Checklist have been received and approved.

PAGE 1 OF 13

WWW.RALEIGHNC.GOV

REVISION 5.15.18

CONDITIONAL USE DISTRICT ZONING CONDITIONS

Zoning Case Number

Date Submitted

Existing Zoning IX-3 Proposed Zoning PD

OFFICE USE ONLY

Transaction #

Rezoning Case #

Narrative of Zoning Conditions Offered

1.

2.

3.

4.

5.

6.

7.

8.

9.

10.

These zoning conditions have been voluntarily offered by the property owner. All property owners must sign each condition page. This page may be photocopied if additional space is needed.

Owner/Agent Signature Print Name

PAGE 2 OF 13

WWW.RALEIGHNC.GOV

REVISION 5.15.18

CLIENT

CIVIL ENGINEERING - LANDSCAPE ARCHITECTURE

ATTORNEY



STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHEM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchemBK@stanleymartin.com



STEWART
223 S. WEST STREET
SUITE 1100
RALEIGH, NC 27603
T 919.380.8750
FIRM LICENSE #: C-1051
www.stewartinc.com
PROJECT #: C18068

STEWART INC. - CIVIL
CONTACT: ROSS MASSEY, PE
CIVIL ENGINEER
919.866.4781 (T)
919.380.8752 (F)
RMASSEY@STEWARTINC.COM

STEWART INC. - LANDSCAPE ARCHITECTURE
CONTACT: CHRISTOPHER MILLER, PLA
LANDSCAPE ARCHITECT
919.866.4796 (T)
919.380.8752 (F)
CMILLER@STEWARTINC.COM

MICHAEL BIRCH

LONGLEAF LAW PARTNERS
2235 GATEWAY ACCESS POINT, STE 201,
RALEIGH, NC 27607
(O) 919.645.4317
(C) 919.208.9427
mbirch@longleaflp.com



Client

Project

CAROLINA
COACH
PLANNED
DEVELOPMENT

Vicinity map



Seal

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for

MASTER PLAN

Title:

EXISTING CONDITION
PLAN

Project number: C18088 Sheet:
Date: 04.03.2019
Drawn by: SP
Approved by: RCM



SURVEY LEGEND:

	SURVEY CONTROL POINT		MANHOLE
	EXISTING IRON PIPE		TRAFFIC SIGNAL BOX
	EXISTING CONCRETE MONUMENT		TRAFFIC SIGNAL POST
	COMPUTED POINT		MONITORING WELL
	STORM DRAIN MANHOLE		BORING LOCATION
	STORM DRAIN CURB INLET		BOLLARD
	SANITARY SEWER MANHOLE		SIGN
	SANITARY SEWER CLEANOUT		FINISHED FLOOR ELEVATION
	SANITARY FORCEMAIN VALVE		DECIDUOUS TREE
	HYDRANT		EVERGREEN TREE
	WATER VALVE		BUSH
	WATER METER		WIRE FENCE
	WATER MANHOLE		CHAIN LINK FENCE
	WATER VAULT		UNDERGROUND TELEPHONE LINE
	WELL		UNDERGROUND FIBER OPTIC LINE
	GAS VALVE		UNDERGROUND GAS LINE
	GAS METER		UNDERGROUND ELECTRIC LINE
	TELEPHONE MANHOLE		UNDERGROUND WATER LINE
	TELEPHONE PEDESTAL		SANITARY SEWER LINE
	TV MANHOLE		STORM DRAIN LINE
	TV PEDESTAL		OVERHEAD WIRES
	FIBER OPTIC WITNESS POST		UNIDENTIFIED LINE
	FIBER OPTIC BOX		UNKNOWN DESTINATION
	ELECTRIC MANHOLE		CONCRETE SURFACE
	ELECTRIC METER		DUCTILE IRON PIPE
	ELECTRIC BOX		POLYVINYL CHLORIDE PIPE
	UTILITY POLE		HIGH-DENSITY POLYETHYLENE PIPE
	GUY POLE		REINFORCED CONCRETE PIPE
	GUY WIRE		CORRUGATED METAL PIPE
	LIGHT POLE		CATCH BASIN

NOTES:

1. INFORMATION IS FROM CITY OF RALEIGH GIS DATABASE



FIRM LICENSE #: C-1051
www.stewartinc.com
PROJECT #: C18088

STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHEM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchemBK@stanleymartin.com

CAROLINA
COACH
PLANNED
DEVELOPMENT

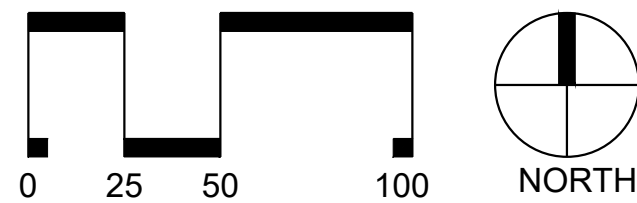


NORTH

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

MASTER PLAN

MASTER PLAN

[illegible]

Title:

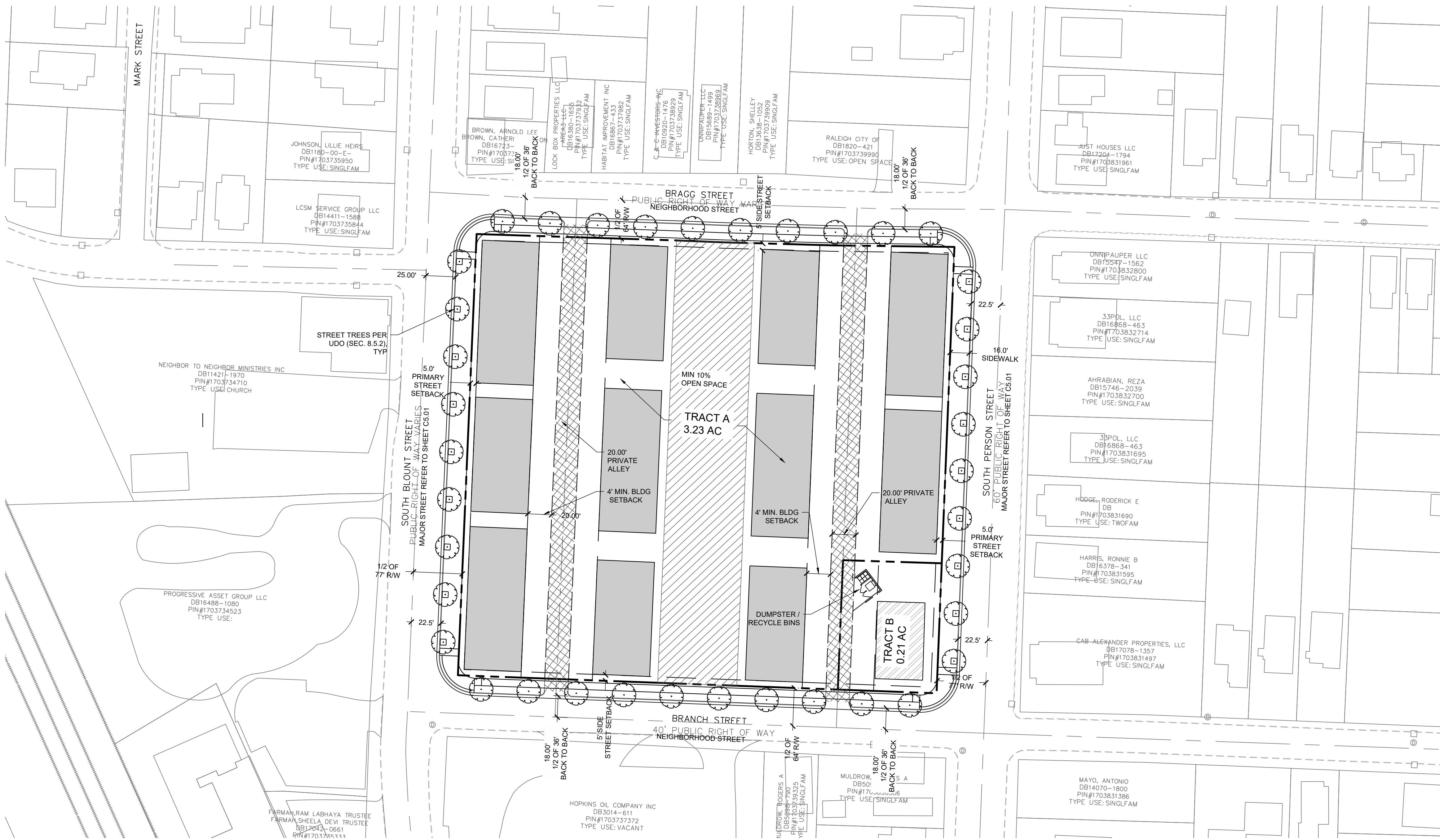
DEVELOPMENT AND PARKING PLAN

Date: 04.03.2019

Drawn by:

Approved by:

C3.00



9. MODIFICATIONS

A. **Parking Standards** - The Planned Development shall comply with the Downtown District (DX-) vehicle parking standards in UDO Section 7.1.3. It is expressly understood that the vehicle parking maximum standard of UDO Section 7.1.3.A.1.a. shall not apply. All parking to be accessed by private alley.

B. **Vehicle Parking Lot Islands** - The vehicle parking lot island requirements in UDO Section 7.1.7 shall not apply to parking spaces located within a building's footprint.

C. **Utility Easement** - There shall be no five-foot (5') utility easement requirement along Person Street and Blount Street.

D. **Street sections** will be those shown on UDOT C.501 Typical Roadway Sections.

E. **0.6% of the gross site area** will be open space, and will be located on Tract A.

F. **Outdoor amenity area** will not be required for Tract A or Tract B.

G. **The following base dimensional standards**, also found in Table 1 of Sheet C3.00, shall apply for any Apartment building type:

- i. The minimum lot area shall not apply.
- ii. The maximum lot area shall not apply.
- iii. The building width in primary build-to (min.) shall be 55%.
- iv. The building width in side build-to (min.) shall be 20%.
- v. The ground floor elevation (min.) shall be 0'.
- vi. The blank wall area (max.) standard shall not apply along Bragg Street and Branch Street.

H. **The following base dimensional standards**, also found in Table 1 of Sheet C3.00, shall apply for any General building type:

- i. The maximum lot area shall not apply.
- ii. The building/structure setbacks from an alley (min.) shall be 4'.

I. **The following base dimensional standards**, also found in Table 1 of Sheet C3.00, shall apply for any Mixed Use building type:

- i. The maximum lot area shall not apply.
- ii. The building/structure setbacks from an alley (min.) shall be 4'.
- iii. The ground story height, floor to floor (min.) shall be 11'.
- iv. The ground story (min.) transparency standard shall be 33%.

A. The following principal uses as listed in UDO section 6.1.4 shall be prohibited:

- i. Vehicle fuel sales
- ii. Vehicle sales/rental
- iii. Detention center, jail, prison
- iv. Vehicle repair (minor)

- B. The maximum building height for any building on Traftt will be 5 stories and fifty-five feet (55'). The maximum building height for any building on Dault & Will be 3 stories and fifty feet (50').
- C. The fifth floor of any building containing residential dwelling units shall be limited to the following uses: conditioned space will be limited to vertical circulation, stairs, restrooms, and restitutes; the remainder will be open air decks.
- D. Built-To standards for the General and Mixed Use building types shall follow Table 1 of a Master Plan sheet C3, D3.
- E. The maximum blank wall area for a General building type shall be 20'.
- F. The Project shall comply with NX - sign standards.
- G. Building orientation - A minimum of 40% of units will have a primary pedestrian access on the public streets. The remaining units will have pedestrian access from the common open space which is ADA-accessible.
- H. The facades of the building shall be constructed from one or more of the following materials: glass, concrete, clay or brick masonry, stone masonry, stucco, cementitious siding, native and manufactured stone, and pre-cast concrete.
- I. Synthetic stone (EIFS) and vinyl shall be prohibited as building siding materials. However, vinyl windows, decorative elements and trim are permitted.
- J. Developer shall engage a local artist to create on-site art that recognizes the history and significance of the South Park neighborhood.

TABLE 2: VEHICLE PARKING REQUIREMENT	
Residential	a. One parking space is required per dwelling unit; b. No vehicle parking is required for the first 16 dwelling units. required for the first 16 dwelling units.
NonResidential	a. No vehicle parking is required for commercial use

	APARTMENT	GENERAL BUILDING	MIXED USE BUILDING
A. LOT DIMENSIONS			
A1 Area (min)	N/A	N/A	N/A
A1 Area (max)	N/A	N/A	N/A
A2 Width (min)	N/A	N/A	N/A
A3 Outdoor amenity area (min)	10% (see notes 4.)	10% (see notes 4.)	10% (see notes 4.)
B. Building/Structure Setbacks			
B1 From primary street (min)	5'	5'	5'
B2 From side street (min)	5'	5'	5'
B3 From side lot line (min)	0' or 6'	0' or 6'	0' or 6'
B4 From rear lot line (min)	0' or 6'	0' or 6'	0' or 6'
B4 From alley (min)	4'	4'	4'
C. Parking Setbacks			
C1 From primary street (min)	10'	10'	10'
C2 From side street (min)	10'	10'	10'
C3 From side lot line (min)	0' OR 3'	0' OR 3'	0' OR 3'
C4 From rear lot line (min)	0' OR 3'	0' OR 3'	0' OR 3'
C4 From alley (min)	4'	4'	4'
D. Build-to			
D1 Blount and Person street build-to	5'/55'	5'/55'	5'/55'
D2 Building width in primary build-to (min)	55%	55%	55%
D3 Bragg and Branch street build-to (min/max)	5'/55'	5'/55'	5'/55'
D4 Building width in side build-to (min)	20%	20%	20%
E. Floor Heights			
E1 Ground floor elevation (min)	0'	0'	0'
E2 Ground story height, floor to floor (min)	N/A	11'	11'
E3 Upper story height, floor to floor (min)	N/A	9'	9'
F. Transparency			
G1 Ground story (min)	N/A	33%	33%
G2 Upper story (min)	N/A	20%	20%
G3 Blank wall area (max) (1)	35'	20'	20'
G. Allowed Building Elements			
	porch, stoop	balcony, gallery, awning	balcony, gallery, awning

(1) Blank wall does not apply to Bragg St. and Branch St.

SITE DATA:

EXISTING ZONING: IX - 3
PROPOSED ZONING: PD

ORIGINAL PARCEL :3.84 AC
PROPOSED PARCEL : 3.44 AC
(0.4 AC RIGHT OF WAY DEDICATION)

TRACT A:

ACREAGE: 3.2 AC
BLDG HEIGHT: 5 STORIES, 55' MAX
MAXIMUM RESIDENTIAL DWELLING UNITS: 100 UNITS
BUILDING TYPE: APARTMENT

TRACT B:

ACREAGE: 0.2 AC
BLDG HEIGHT: 3 STORIES, 50' MAX.
MAXIMUM DEVELOPMENT INTENSITY: 10,000 SF
BUILDING TYPE: GENERAL OR MIXED USE

REQUIRED OPEN SPACE: 10% MIN
PROVIDED OPEN SPACE: 10% MIN

REQUIRED BUILDING TYPES: 2 TYPES
PROPOSED BUILDING TYPES: 2 TYPES

* NOTES:

1. NX SIGN STANDARDS WILL APPLY
2. PARKING STANDARDS - THE PLANNED DEVELOPMENT SHALL COMPLY WITH THE DOWNTOWN DISTRICT (DX) VEHICLE PARKING STANDARDS IN UDO SECTION 7.1.3. IT IS EXPRESSLY UNDERSTOOD THAT THE VEHICLE PARKING MAXIMUM STANDARD OF UDO SECTION 7.1.3A.1A SHALL NOT APPLY. ALL PARKING TO BE ACCESSED VIA PRIVATE ALLEY
3. THE FOLLOWING PRINCIPAL USES AS LISTED IN UDO SECTION 6.1.4 SHALL BE PROHIBITED:
 - i. VEHICLE FUEL SALES
 - ii. VEHICLE SALES/RENTAL
 - iii. DETENTION / DETENTION / JAIL / PRISON
 - iv. VEHICLE REPAIR (MINOR)
4. A MIN. OF 10% OVERALL PLANN DEVELOPMENT WILL BE DEDICATED TO OPEN SPACE AND WILL BE PLACED ON TRAC
5. ANY PRINCIPLE USE THAT IS NOT A DWELLING SPACE COUNTS AS COMMERCIAL SPACE.



Client:

Project

CAROLINA
COACH
PLANNED
DEVELOPMENT

Vicinity map

LEGEND:

..... PEDESTRIAN ACCESS

* NOTES:
NO BARRIERS SHALL BE ERECTED TO PREVENT THE OPEN
SPACE FROM PUBLIC ACCESSIBILITY.

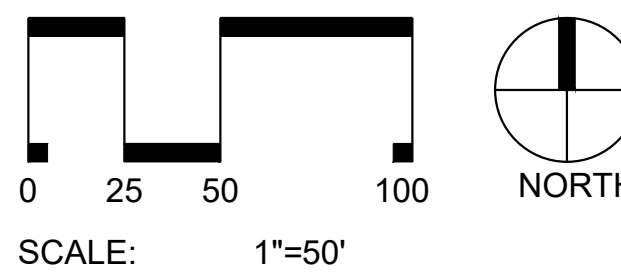


Seal

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for

MASTER PLAN

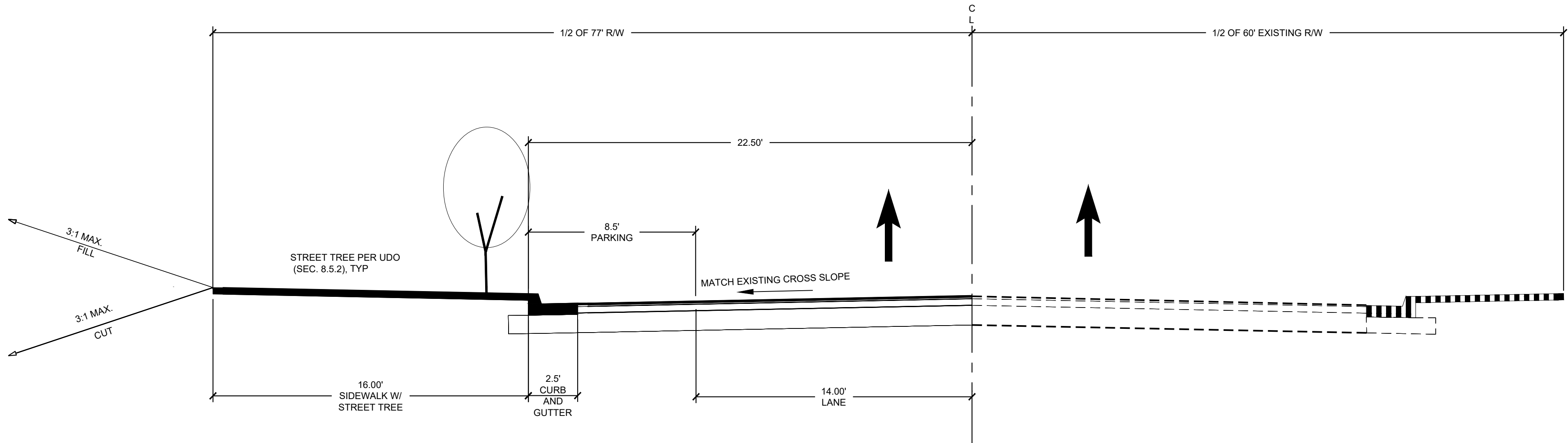
[illegible]

Title:

PEDESTRIAN CIRCULATION GREENWAY CONNECTION PLAN

Project number: C18088 Sheet:
Date: 04.03.2019
Drawn by: SP
Approved by: RCM

L:\Projects\2018\18088 - Carolina Coach Apartments\DWGS11 - Design\Sheets\SD SET\18088-CS-00-Grading & Storm Drainage Plan.dwg Apr 15, 2020 - 3:54pm



TYPICAL MAJOR SECTION FOR S. BLOUNT STREET AND PERSON STREET

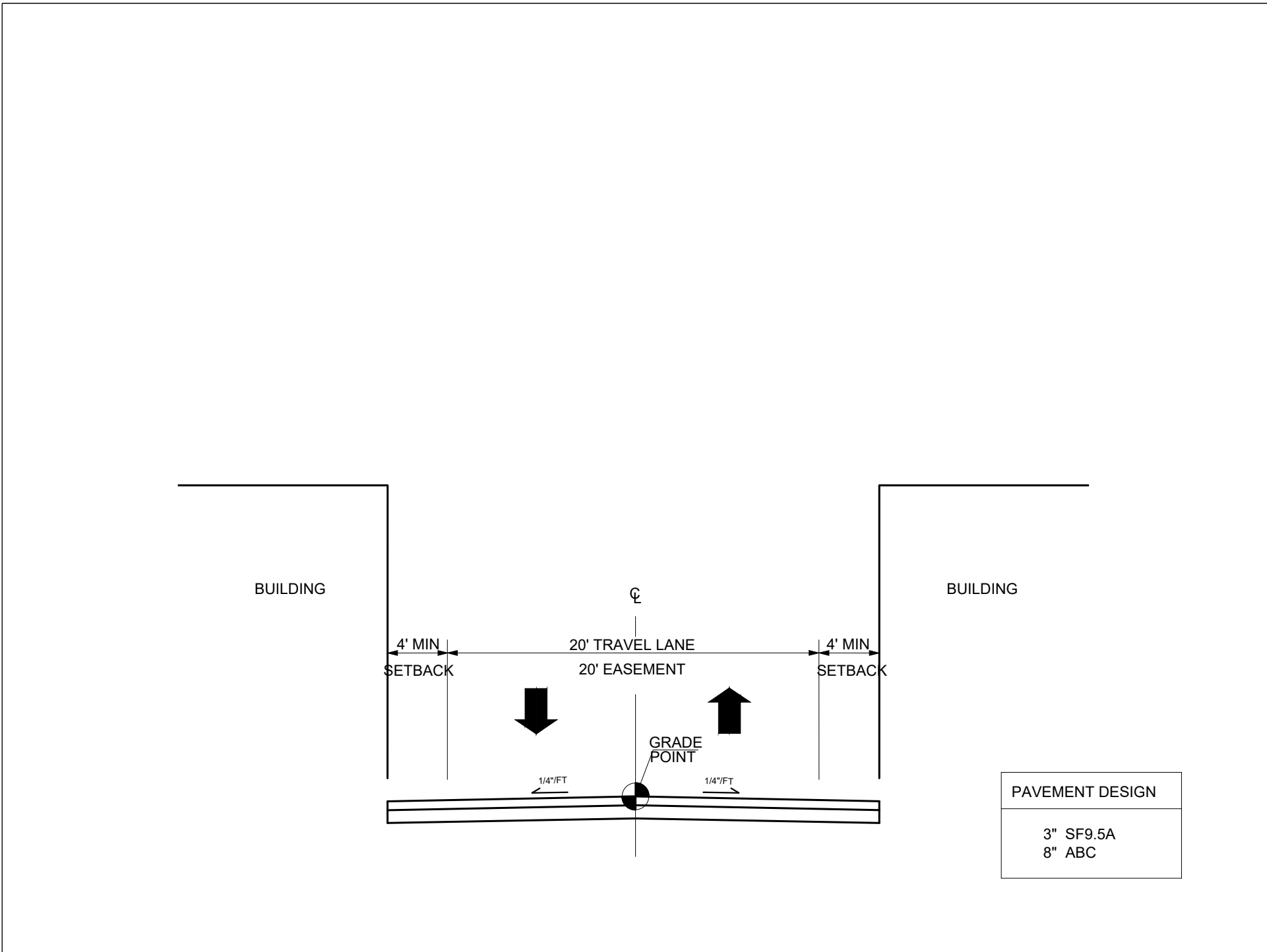
77' RIGHT-OF-WAY 45' B-B

LEGEND

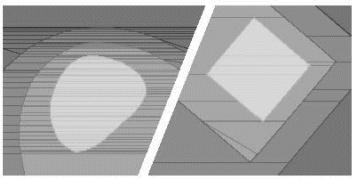
UTIL. = UTILITY
ESMT. = EASEMENT
PARKING = ON STREET PARKING
ROW = RIGHT-OF-WAY
B-B = BACK OF CURB TO BACK OF CURB
E-E = EDGE OF PAVEMENT TO EDGE OF PAVEMENT
SHLDR = SHOULDER

NOTE:

1. BRAGG AND BRANCH STREET WILL BE NEIGHBORHOOD STREET, R.O.W WIDTH IS 64', BACK CURB TO BACK CURB IS 36'.
2. BRAGG AND BRANCH STREET WILL BE PROPOSED AS NEIGHBORHOOD STREET PER SECTION UDO SEC. 8.4.4 LOCAL STREETS



ALLEY SECTION



STEWART

223 S. WEST ST., #1100 FIRM LICENSE # C-1051
RALEIGH, NC 27603 www.stewartinc.com
T 919.380.8750 PROJECT # C18088

Client:

STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHEM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchemBK@stanleymartin.com

Project:

CAROLINA COACH PLANNED DEVELOPMENT

Vicinity map:



Seal:

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for:

MASTER PLAN

No.	Date	Description

Title:

TYPICAL ROADWAY SECTIONS

Project number: C18088 Sheet:

Date:

04.03.2019

Drawn by:

SP

Approved by:

RCM

C5.01



Client:

Project

CAROLINA
COACH
PLANNED
DEVELOPMENT

Vicinity map



Seal

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for

MASTER PLAN

SCALE: 1"=50'

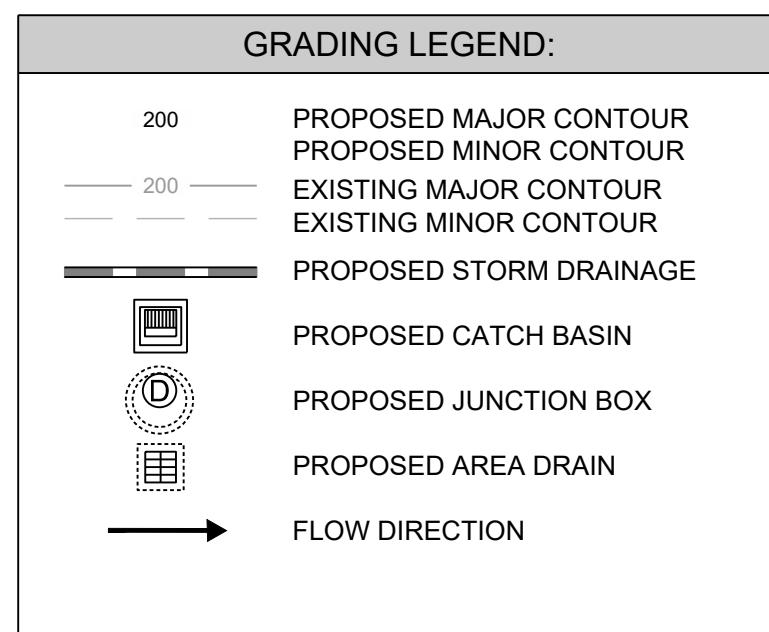
Title:

OVERALL STORMWATER PLAN

Project number: C18088 Sheet

Date: 04.03.2019
 Drawn by: SP
 Approved by: RCM

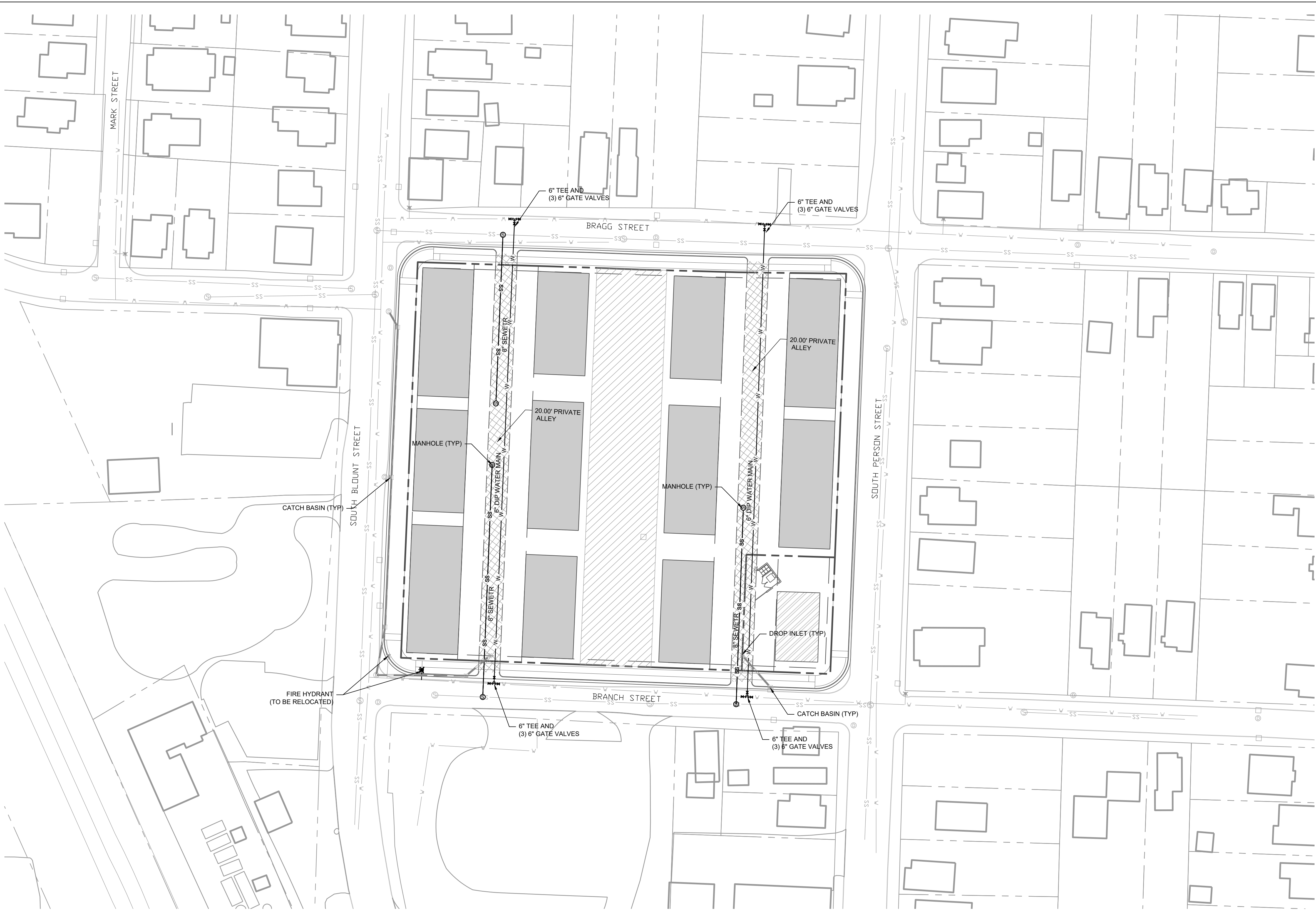
C5.02



GRADING AND STORM DRAINAGE NOTES:

1. THE SITE WILL MEET ALL CITY OF RALEIGH STANDARD REQUIREMENTS FOR STORMWATER OUTLINED IN ARTICLE 9.2 STORMWATER MANAGEMENT IN THE UDO.
2. THE STORM COLLECTION SYSTEM IS SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. SPECIFIC DRAINAGE DETAILS WILL BE ADDRESSED DURING THE SUBDIVISION REVIEW PROCESS.
3. TRIBUTARY: NEUSE RIVER BASIN.
4. SOILS: URBAN LAND
5. THE EFFECT OF LOT WILL MEET ARTICLE 9.4 OF THE UDO DURING CONCURRENT REVIEW.

L:\Projects\2018\18088 - Carolina Coach Apartments\DWGS\1 - Design\Sheets\SD SET\18088-C6.00-Utility Plan.dwg Apr 15, 2020 - 3:55pm

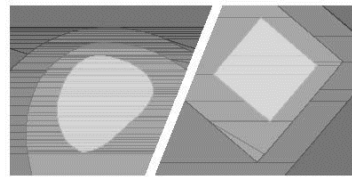


UTILITY LEGEND:

— W —	EXISTING WATER LINE
— W —	PROPOSED WATER LINE
— SS —	EXISTING SANITARY SEWER LINE
— SS —	PROPOSED SANITARY SEWER LINE
⦿	PROPOSED FIRE HYDRANT
⊗	PROPOSED WATER VALVE
⊙	EXISTING SANITARY SEWER MANHOLE
⊙	PROPOSED SANITARY SEWER MANHOLE

UTILITY NOTES:

1. THE WATER AND SEWER UTILITY SYSTEMS ARE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. SPECIFIC DESIGN DETAILS WILL BE ADDRESSED DURING THE SUBDIVISION REVIEW PROCESS.



STEWART

223 S. WEST ST., #1100 FIRM LICENSE # C-1051
RALEIGH, NC 27603 www.stewartinc.com
T 919.380.8750 PROJECT # C18088

Client:

STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHEM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchemBK@stanleymartin.com

Project:

CAROLINA COACH PLANNED DEVELOPMENT

Vicinity map:



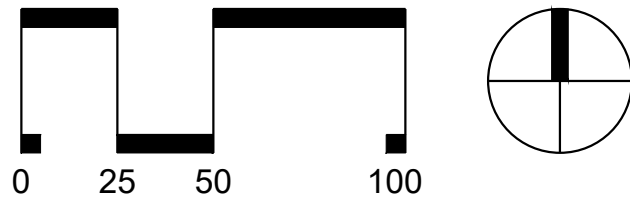
Seal:

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for:

MASTER PLAN

No.	Date	Description



SCALE: 1"=50'

Title:

OVERALL UTILITY PLAN UTILITY SERVICE PLAN

Project number: C18088 Sheet:

Date:

04.03.2019

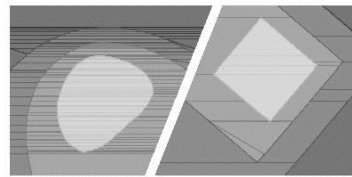
Drawn by:

SP

Approved by:

RCM

C6.00



STEWART

223 S. WEST ST., #1100 FIRM LICENSE #: C-1051
RALEIGH, NC 27603 www.stewartinc.com
T 919.380.8750 PROJECT #: C18088

Client:

STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHEM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchemBK@stanleymartin.com

Project:

CAROLINA
COACH
PLANNED
DEVELOPMENT

Vicinity map:



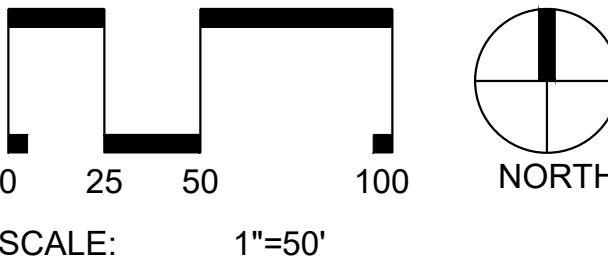
Seal:

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for:

MASTER PLAN

No.	Date	Description



Title:

TREE
CONSERVATION
PLAN

Project number: C18088 Sheet:
Date: 04.03.2019
Drawn by: SP
Approved by: RCM

L1.00



L:\Projects\2018\18088 - Carolina Coach Apartments\DWGS\1 - Design\Sheets\SD SET\18088-A1.00-Site Mass Model.dwg Apr 15, 2020 - 3:55pm

BIRD'S EYE LOOKING SOUTH EAST
Branch Street and South Person Street front



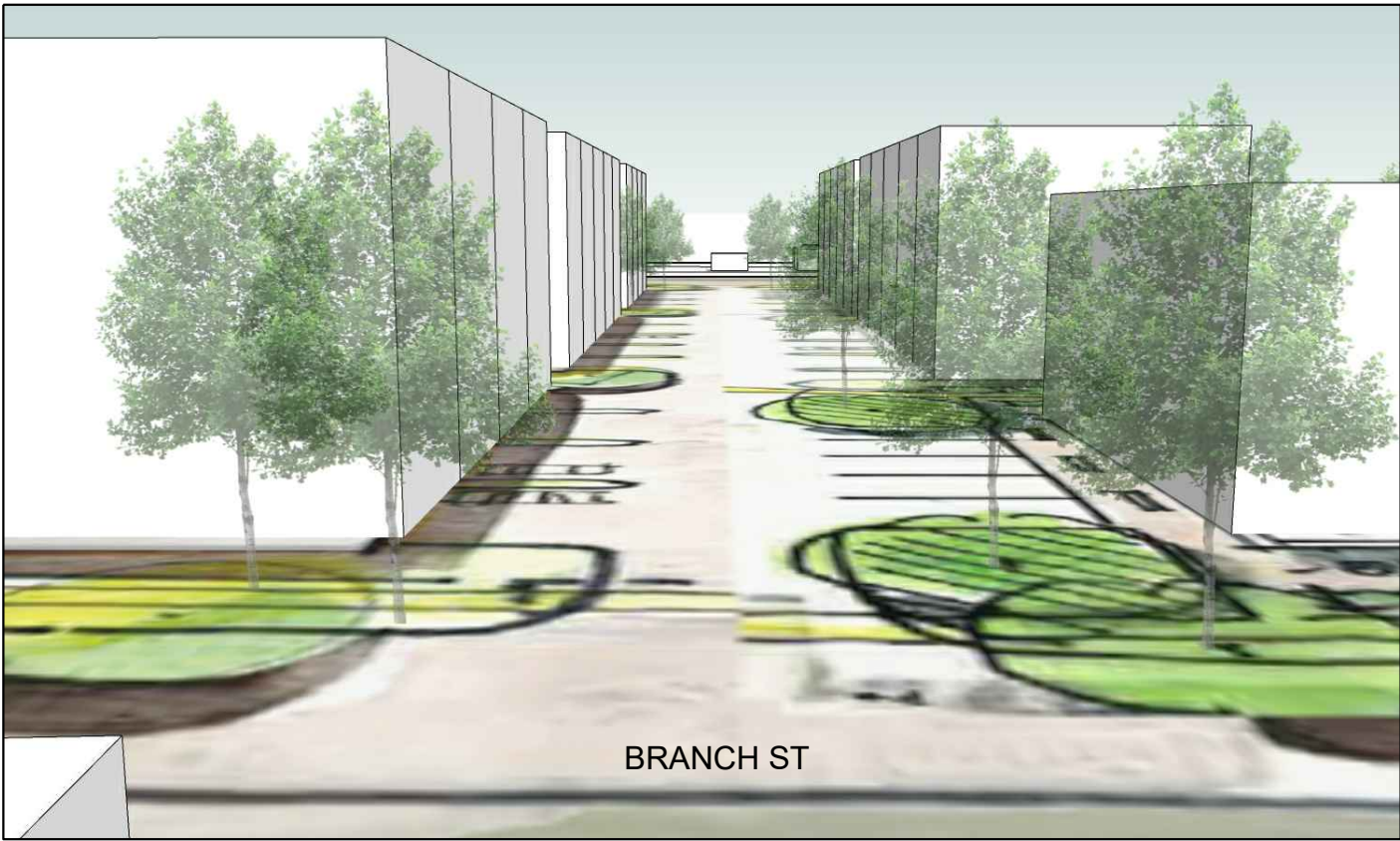
BIRD'S EYE LOOKING NORTHWEST
Branch Street and South Person Street back



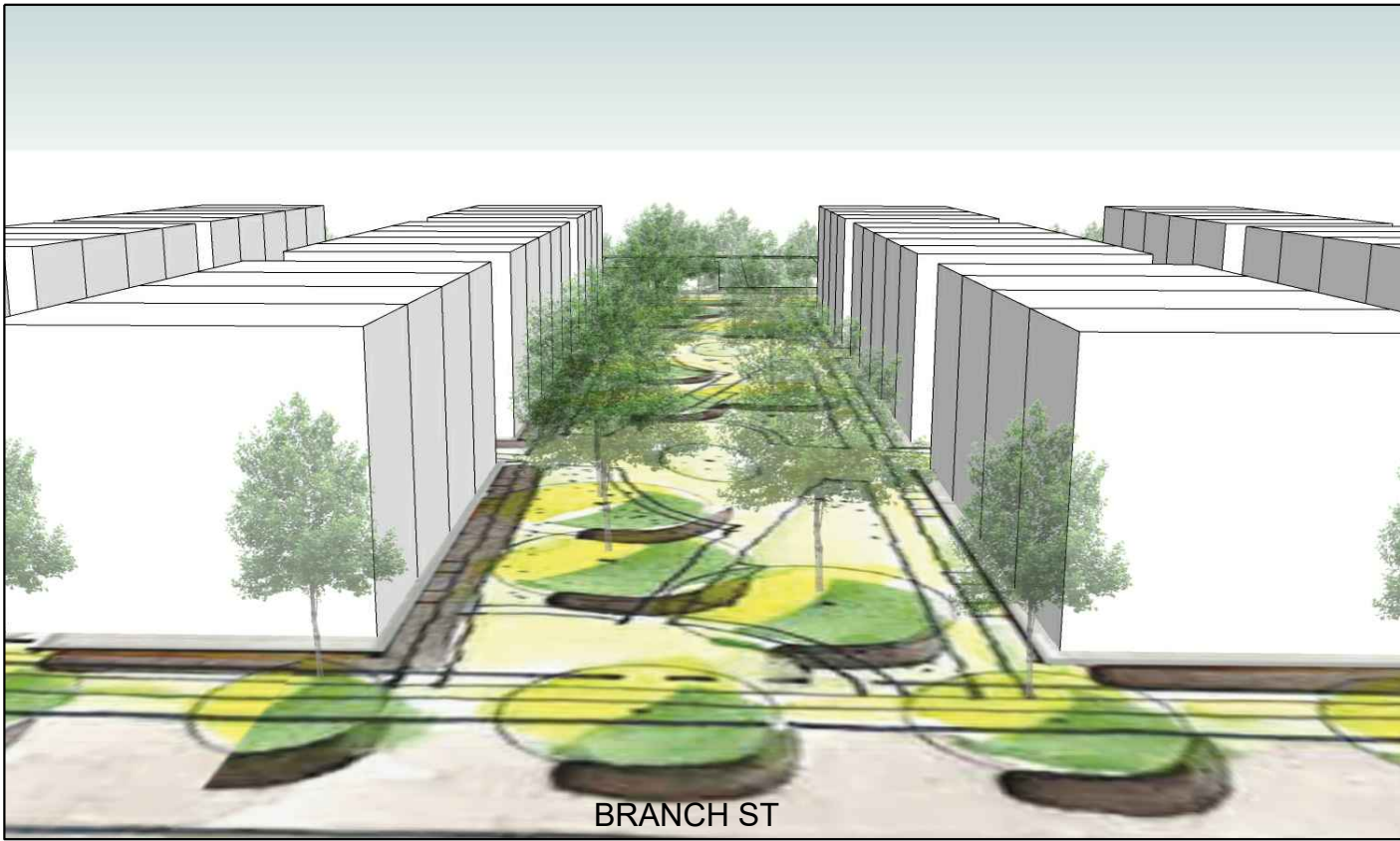
PERSPECTIVE VIEW
Community Two - Story Retail



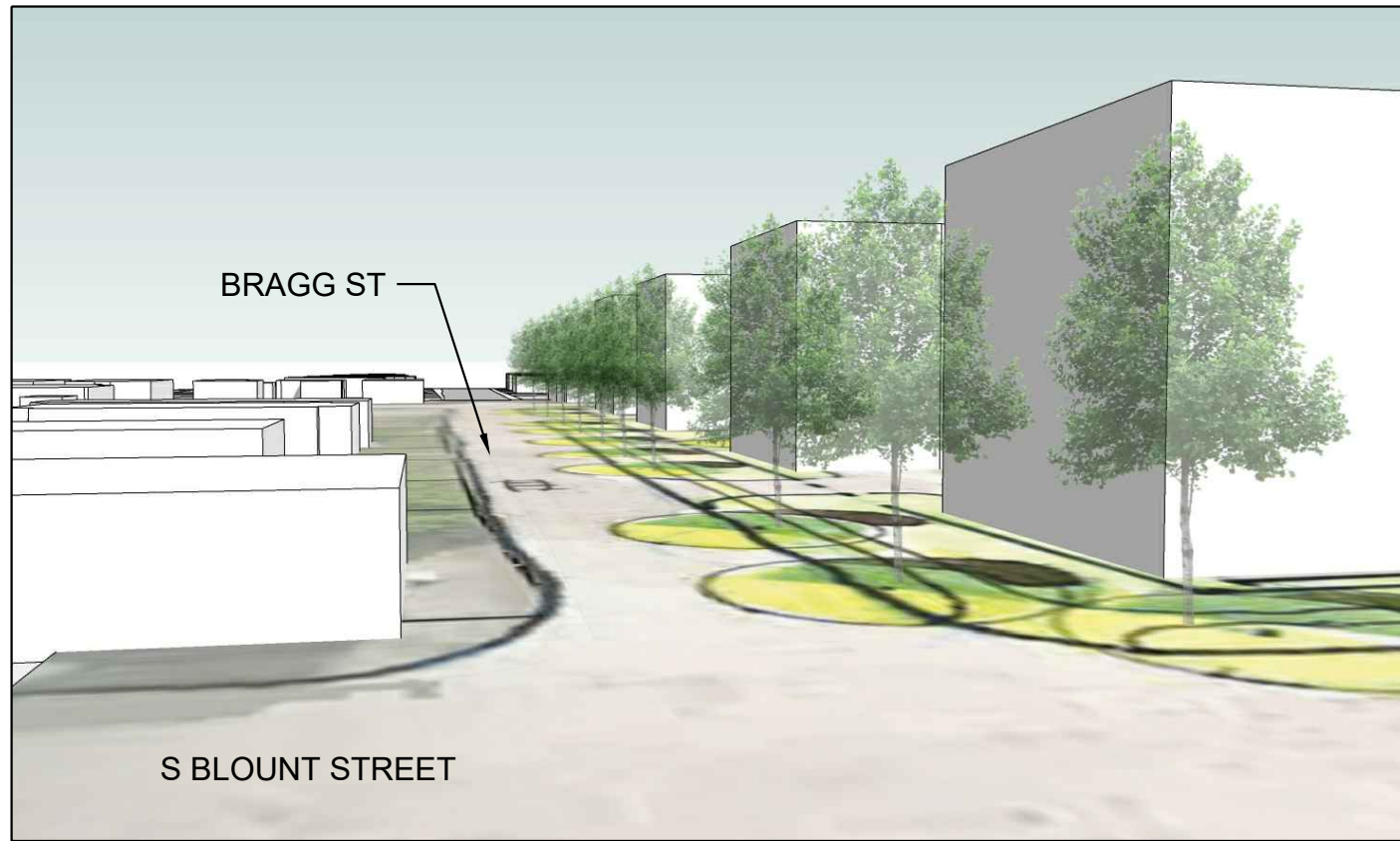
PERSPECTIVE VIEW
Branch Street Looking to Private Alley



PERSPECTIVE VIEW
Branch Street Looking to Open Space



PERSPECTIVE VIEW
South Blount Street and Bragg Street



PERSPECTIVE VIEW
Neighborhood Looking to Retail Store, Residential and Open Space



PRECEDENT IMAGE
Stanley Martin - Riverwalk at Rocketts Landing



PRECEDENT IMAGE
Stanley Martin - Blount Street Commons



Client:

STANLEY MARTIN HOMES
4020 WESTCHASE BOULEVARD,
BUILDING ONE, SUITE 190
RALEIGH, NC 27607
CONTACT: BRIAN KETCHUM, PE, LEED-AP
919.977.8760 (T)
919.724.0624 (C)
KetchumBK@stanleymartin.com

Project:

CAROLINA COACH PLANNED DEVELOPMENT

Vicinity map:



Seal:

PRELIMINARY - DO NOT
USE FOR CONSTRUCTION

Issued for:

MASTER PLAN

No.	Date	Description

SCALE: N.T.S.

Title:

ARCHITECTUAL ILLUSTRATIVES

Project number: C18088 Sheet:

Date: 04.03.2019

Drawn by: ML

Approved by: RCM

A1.00



Raleigh

MEMO

TO: Bynum Walter, AICP, Comprehensive Planning Supervisor

FROM: Eric J. Lamb, PE, Transportation Planning Manager

DATE: June 24, 2020

SUBJECT: TIA Review Amendment Z-14-19 Carolina Coach

This memo updates the previously approved TIA review dated September 19, 2019 for the Carolina Coach Planned Development. The applicant has amended their plan to include an additional 2,000 square feet of retail space. The previously approved review was based on 100 dwelling units of Mid-Rise Multifamily Housing and 8,000 square feet of Retail. The updated condition now includes 100 dwelling units of Mid-Rise Multifamily Housing and 10,000 square feet of Retail. This memo addresses only the pertinent changes from the original assumptions and their projected impacts.

Development Details

<i>Site Location:</i>	Downtown Raleigh, bounded by Bragg Street to the north, Branch Street to the south, Person Street to the east and Blount Street to the west.
<i>Address:</i>	1208 Blount Street
<i>Property Pin:</i>	1703738654
<i>Current Zoning:</i>	IX-3
<i>Proposed Zoning:</i>	PD
<i>Existing Land Use:</i>	Vacant
<i>Existing Allowable Land Use:</i>	820 Shopping Center (102,560 SF)
<i>Proposed Allowable Land Use:</i>	221 Multifamily Housing (Mid-Rise, 100 units) 820 Shopping Center (10,000 SF)
<i>Build-out Year:</i>	2021

Municipal Building
222 West Hargett Street
Raleigh, North Carolina 27601

One Exchange Plaza
1 Exchange Plaza, Suite 1020
Raleigh, North Carolina 27601

City of Raleigh
Post Office Box 590 • Raleigh
North Carolina 27602-0590
(Mailing Address)

Study Area & Analysis Scenarios (*Unchanged from previous memo*)

The following intersections were studied as part of this TIA:

- | | |
|---|----------------------|
| 1. Martin Luther King, Jr. Boulevard at Blount Street | (Signalized) |
| 2. Martin Luther King, Jr. Boulevard at Person Street | (Signalized) |
| 3. Bragg Street at Blount Street | (Unsignalized, TWSC) |
| 4. Bragg Street at Person Street | (Unsignalized, TWSC) |
| 5. Bragg Street at Garner Road | (Signalized) |
| 6. Branch Street at Blount Street | (Unsignalized, TWSC) |
| 7. Branch Street at Person Street | (Unsignalized, TWSC) |
| 8. Hoke Street at Blount Street/Hammond Road | (Signalized) |
| 9. Hoke Street at Person Street/Hammond Road | (Signalized) |
| 10. Bragg Street at Site Driveway 1 | (Unsignalized) |
| 11. Bragg Street at Site Driveway 2 | (Unsignalized) |
| 12. Branch Street at Site Driveway 1 | (Unsignalized) |
| 13. Branch Street at Site Driveway 2 | (Unsignalized) |

TWSC = Two Way Stop Control

Trip Generation

WSP made the following assumptions as agreed to by City Staff:

- 10th Edition ITE Trip Generation Manual – Land Use Codes as listed in the tables below
- A 0.5% growth rate will be applied to the study year 2021
- Internal Capture will be applied to the trip generation potential of the site,
- A 10% multi-modal capture (bike/ped/transit) will be applied to both the current and proposed zoning conditions.
- There are no apparent existing trips for the existing land use.

Table 1: Existing Zoning Maximum Trip Generation

Land Use	ITE Code	Intensity	Daily Traffic (vpd)	AM			PM		
				In	Out	Total	In	Out	Total
Shopping Center	820	102,560 SF	6116	126	77	203	266	288	554
10% Multimodal Reduction:				-13	-8	-21	-27	-29	-56
Pass-by Trip Reduction:				0	0	0	-81	-88	-169
Non-Pass-by Primary Trips:				113	69	182	158	171	329

Table 2: Proposed Zoning Trip Generation

Land Use	ITE Code	Intensity	Daily Traffic (vpd)	AM			PM		
				In	Out	Total	In	Out	Total
Multifamily Housing (Mid-Rise)	221	100 Units	543	9	25	34	27	17	44
Shopping Center	820	10,000 SF	1,256	97	60	157	48	51	99
Total Trips:			1622	106	85	191	75	68	143
Internal Capture:				0	0	0	-17	-17	-34
10% Multimodal Reduction:				-11	-9	-20	-6	-5	-11
Total New Primary Trips:				94	76	171	52	46	98
Proposed Zoning Trips vs. Existing Zoning Trips				-19	7	-11	-106	-125	-231

The Proposed Zoning scenario would generate a decrease in the total peak hours and daily traffic volume as compared to the maximum buildout under the current zoning.

Site Traffic Distribution (*Unchanged from previous memo*)

Trips generated by the proposed development were distributed based on a review of surrounding land uses and existing traffic patterns.

- 35% to/from the north via Blount St./Person St.
- 15% to/from the west via Martin Luther King, Jr. Blvd.
- 5% to/from the east via Martin Luther King, Jr. Blvd.
- 5% to/from the northeast via Garner Rd.
- 40% to/from the south via Blount St./Person St.

Results and Impacts

Level of Service (LOS) results for the studied intersections are summarized in the table below.

Table 3: Level of Service Results

Intersection	2019 Existing				2021 Background				2021 Build-out under Current Zoning				2021 Build-out under Proposed Zoning				2021 Build-out under Proposed Zoning with Improvements			
	LOS		Delay		LOS		Delay		LOS		Delay		LOS		Delay		LOS		Delay	
	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM	AM	PM
Martin Luther King Jr Blvd & S Blount St	B	D	16.6	52.7	B	D	16.8	54.7	B	E	16.9	67.4	B	E	16.9	57.4	B	D	12.7	50.4
Martin Luther King Jr Blvd & S Person St	F	B	81.0	18.1	F	B	82.3	18.3	F	C	85.7	23.2	F	B	86.3	18.8	E	B	68.0	18.2
Bragg St & S Blount St*	-	-	1.6	0.9	-	-	1.6	1.0	-	-	1.8	2.7	-	-	1.9	1.2	-	-	1.9	1.2
Bragg St & S Person St*	-	-	3.4	2.3	-	-	3.5	2.4	-	-	5.5	4.8	-	-	5.5	2.7	-	-	5.5	2.7
Bragg St & Garner Rd	A	A	6.1	8.0	A	A	6.1	8.0	A	A	6.2	8.5	A	A	6.2	8.1	A	A	6.4	7.4
Branch St & S Blount St*	-	-	0.4	0.3	-	-	0.4	0.3	-	-	0.7	2.6	-	-	0.7	0.5	-	-	0.7	0.5
Branch St & S Person St*	-	-	0.7	1.4	-	-	0.7	1.3	-	-	1.0	3.5	-	-	1.1	1.6	-	-	1.1	1.6
Hoke St & S Blount St/Hammond Rd	B	B	17.6	12.3	B	B	17.6	12.4	B	B	17.8	12.7	B	B	17.7	12.4	A	A	8.8	7.3
Hoke St & S Person St/Hammond Rd	B	B	13.2	10.1	B	B	13.3	10.1	B	B	13.5	10.1	B	B	13.5	10.1	B	B	13.5	12.5
Bragg St & Site Driveway 1*	-	-	-	-	-	-	-	-	-	-	1.6	2.3	-	-	1.6	0.8	-	-	1.6	0.8
Bragg St & Site Driveway 2*	-	-	-	-	-	-	-	-	-	-	1.6	2.6	-	-	1.7	1.0	-	-	1.7	1.0
Branch St & Site Driveway 1*	-	-	-	-	-	-	-	-	-	-	2.9	2.3	-	-	3.0	1.3	-	-	3.0	1.3
Branch St & Site Driveway 2*	-	-	-	-	-	-	-	-	-	-	2.8	5.1	-	-	2.9	2.2	-	-	2.9	2.2

When Table 3 is compared to the previously approved summary of results, no significant changes are indicated.

Conclusions

City Staff agrees with the updated analysis performed as a result of the project adding 2,000 square feet of retail to the original submittal. Based on these findings, there are no significant changes to the projected levels of service. Therefore, city staff makes no further recommendation at this time.

EJL / bc

REZONING OF PROPERTY CONSISTING OF +/- 3.86 ACRES
LOCATED BETWEEN SOUTH BLOUNT STREET, SOUTH PERSON STREET, BRANCH
STREET AND BRAGG STREET, IN THE CITY OF RALEIGH

REPORT OF MEETING WITH ADJACENT PROPERTY OWNERS ON
MARCH 28, 2019

Pursuant to applicable provisions of the Unified Development Ordinance, a meeting was held with respect to a potential rezoning with adjacent property owners on Thursday, March 28, 2019 at 6:30 p.m. The property considered for this potential rezoning totals approximately 3.86 acres, and is located between South Blount Street, South Person Street, Branch Street and Bragg Street, in the City of Raleigh, having Wake County Parcel Identification Number 1703-73-8654. This meeting was held at the Chavis Community Center, located at 505 Martin Luther King Jr. Boulevard, Raleigh, NC 27601. All owners of property within 500 feet of the subject property were invited to attend the meeting. Attached hereto as **Exhibit A** is a copy of the neighborhood meeting notice. A copy of the required mailing list for the meeting invitations is attached hereto as **Exhibit B**. A summary of the items discussed at the meeting is attached hereto as **Exhibit C**. Attached hereto as **Exhibit D** is a list of individuals who attended the meeting.

EXHIBIT A – NEIGHBORHOOD MEETING NOTICE



Michael Birch
Email: mbirch@longleaflp.com
Direct: (919) 645-4317

2235 Gateway Access Point, Ste 201
Raleigh, NC 27607
Tel: (919) 645-4300
Fax: (919) 510-6802
longleaflp.com

To: Neighboring Property Owner

From: Michael Birch

Date: March 18, 2019

Re: Neighborhood Meeting for Potential Rezoning of 1208 S Blount Street

We are counsel for a developer that is considering rezoning the 3.86-acre tract of land located between S Blount Street, S Person Street, Branch Street and Bragg Street, with an address of 1208 S Blount Street and Parcel Identification Number 1703-73-8654 (the "Property"). The Property is currently zoned Industrial Mixed Use with a maximum building height of three stories (IX-3), and the developer is considering rezoning the Property to the Planned Development (PD) district in order to allow a five-story residential development with roof-top terraces.

You are invited to attend a meeting to discuss the potential rezoning. We have scheduled a meeting with surrounding property owners on Thursday, March 28, 2018 at 6:30 p.m. This meeting will be held in the media center at the Chavis Community Center located at 505 Martin Luther King Jr. Boulevard, Raleigh, NC 27601.

We previously held a neighborhood meeting for this rezoning on Tuesday, September 11, 2018. We will present much of the same information that was presented at the prior neighborhood meeting.

The City of Raleigh requires a neighborhood meeting involving the owners of property within 500 feet of the Property prior to filing a rezoning application. After the meeting, we will prepare a report for the Planning Department regarding the items discussed at the meeting.

Please do not hesitate to contact me directly if you have any questions or wish to discuss any issues. I can be reached at 919.645.4317 and mbirch@longleaflp.com. Also, for more information about rezoning, you may visit www.raleighnc.gov or contact the Raleigh City Planning Department at 919.996.2180 or rezoning@raleighnc.gov.

EXHIBIT C – ITEMS DISCUSSED

1. Architectural styles: Modern versus Traditional
2. The amount of commercial space permitted under the PD narrative
3. The potential effects to neighbors' property values
4. The type of commercial uses contemplated once development is complete, and where the commercial space would be located on site
5. Other Stanley Martin Homes products in Raleigh and other cities
6. Common amenity areas for condominium residents
7. Parking for residents and guests
8. How the project can incorporate the park north of the site
9. The possible construction timeline after rezoning approval
10. The total number of residential units
11. Affordable housing
12. Remediation of the site from prior uses, and the Brownfield program
13. Stormwater runoff and possible remediation
14. The height of the condominiums
15. The possibility for educational and training space within the designated commercial space

EXHIBIT D – MEETING ATTENDEES

1. Andrew Clark
2. Lonnette Williams
3. Josephus Richburg
4. Carolyn Winters
5. Joseph Winters