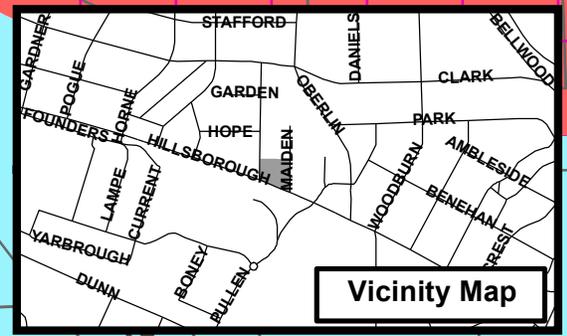
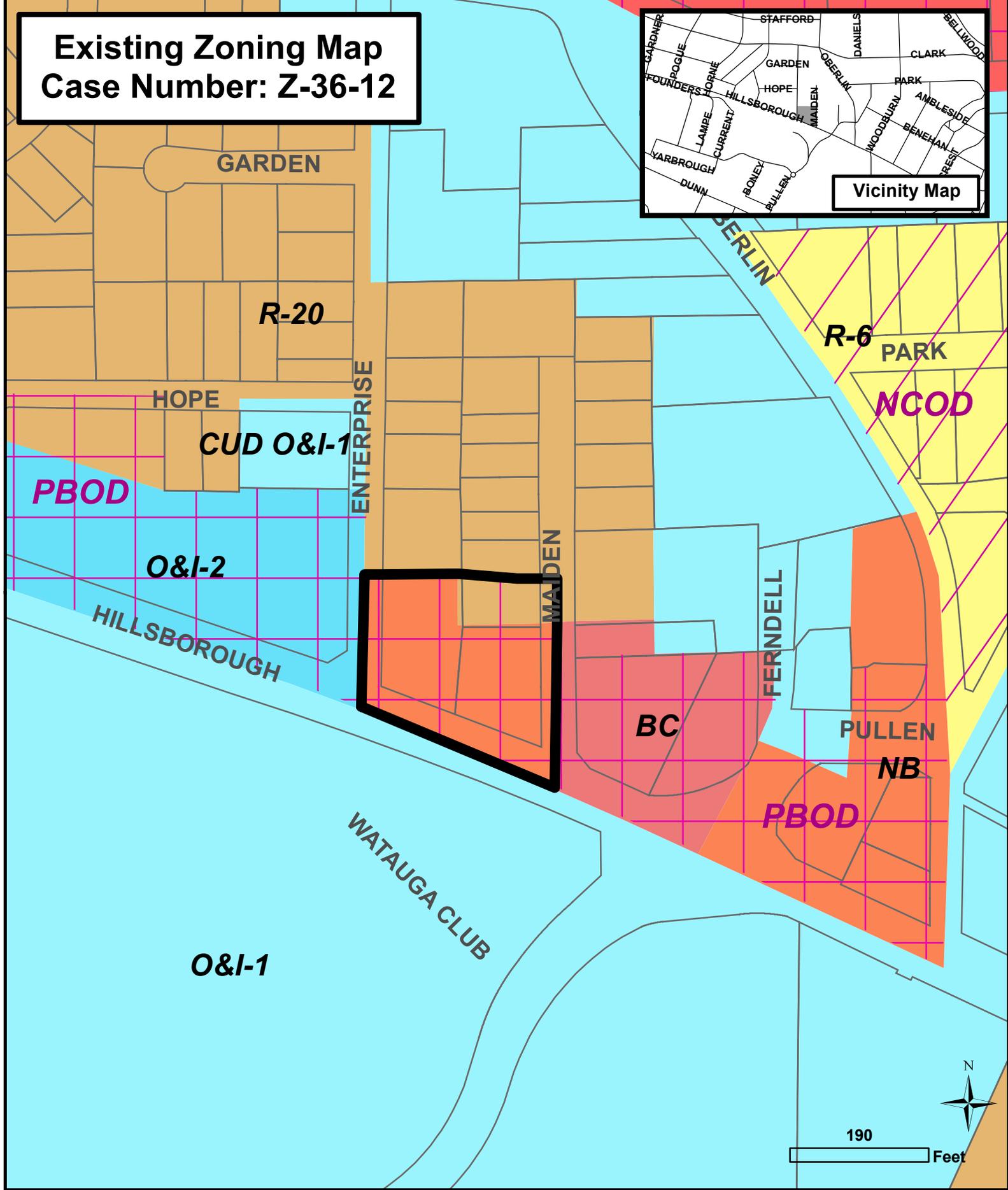


**Existing Zoning Map
Case Number: Z-36-12**



Vicinity Map



190 Feet

Request:
0.95 acre from R-20 & NB w/PBOD
to NB CUD w/PBOD

City of Raleigh Public Hearing
October 16, 2012
(January 14, 2013)

Summary of Conditions

<i>Submitted Conditions</i>	<ul style="list-style-type: none"> • Streetscape Plan • Prohibited Uses • Lighting restrictions • Parking deck provisions
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Issues and Impacts

<i>Outstanding Issues</i>	<ol style="list-style-type: none"> 1. Inconsistent with Future Land Use Map. 2. Inconsistent with Comprehensive Plan policy. 	<i>Suggested Conditions</i>	<ol style="list-style-type: none"> 1. Consider condition for building step back off Hillsborough Street above 3rd story.
<i>Impacts Identified</i>	<ol style="list-style-type: none"> 1. None identified. 	<i>Proposed Mitigation</i>	<ol style="list-style-type: none"> 1. N/A

Public Meetings

<i>Neighborhood Meeting</i>	<i>Public Hearing</i>	<i>Committee</i>	<i>Planning Commission</i>
6/14/12	10/16/12	Date: Action	Date: 10/23/12

Valid Statutory Protest Petition

Attachments

1. Staff report
2. Existing Zoning/Location Map
3. Future Land Use

Planning Commission Recommendation

<i>Recommendation</i>	The Planning Commission finds that this case is inconsistent with the Comprehensive Plan, however based on the finding and reasons below, should be approved in accordance with conditions dated October 24 th , 2012.
<i>Findings & Reasons</i>	<ol style="list-style-type: none"> 1. That while the proposed rezoning request is inconsistent with the Future Land Use Map, it is consistent with all applicable Comprehensive Plan Policies as well as the Urban Design Guidelines. 2. That the amended Streetscape Plan is compatible and consistent with the surrounding area. Applicant has offered several conditions and provisions to help ensure adequate buffering and transitions to surrounding lower intensity uses. 3. That the proposed amendment will provide more flexibility for redevelopment on the subject property, and will still be developed in harmony with the overall University Village Streetscape Plan.
<i>Motion and Vote</i>	Motion: Fleming Second: Mattox

	In favor: Buxton, Fleming, Fluhner, Harris Edmisten, Haq, Mattox, Sterling Lewis, Terando Excused: Schuster
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This document is a true and accurate statement of the findings and recommendations of the Planning Commission. Approval of this document incorporates all of the findings of the attached Staff Report.

_____	_____	_____	<u>10/23/12</u>
Planning Director	Date	Planning Commission Chairperson	Date

Staff Coordinator: Stan Wingo stan.wingo@raleighnc.gov



Zoning Staff Report – Case Z-36-12

Conditional Use District

Request

<i>Location</i>	North side of Hillsborough Street, east of its intersection with Enterprise Street.
<i>Request</i>	Rezoning property from Residential-20 and Neighborhood Business with Pedestrian Business Overlay to Neighborhood Business Conditional Use with Pedestrian Business Overlay
<i>Area of Request</i>	0.95 acre
<i>Property Owner</i>	Bell Tower Holdings, LLC and NCSU Alumni of Delta Upsilon, Inc
<i>PC Recommendation Deadline</i>	January 14, 2013

Subject Property

	<i>Current</i>	<i>Proposed</i>
<i>Zoning</i>	R-20 and NB	NB CUD
<i>Additional Overlay</i>	PBOD	PBOD
<i>Land Use</i>	Retail, Residential	Mixed Use (Retail, Office and/or Residential)
<i>Residential Density</i>	320 units per acre (304 units)	320 units per acre (304 units)

Surrounding Area

	<i>North</i>	<i>South</i>	<i>East</i>	<i>West</i>
<i>Zoning</i>	R-20	O&I-1	BC w/PBOD	O&I-2 w/PBOD
<i>Future Land Use</i>	Moderate Density Residential	Institutional	Neighborhood Mixed Use, Moderate Density Residential	Neighborhood Mixed Use
<i>Current Land Use</i>	Residential	University	Retail	University Housing

Comprehensive Plan Guidance

<i>Future Land Use</i>	Neighborhood Mixed Use and Moderate Density Residential
<i>Area Plan</i>	N/A
<i>Applicable Policies</i>	Policy LU 1.3 - Conditional Use District Consistency Policy LU 2.2 - Compact Development Policy LU 2.6 - Zoning and Infrastructure Impacts Policy LU 4.5 - Connectivity Policy LU 5.1 - Reinforcing the Urban Pattern

	Policy LU 5.2 - Managing Commercial Development Impacts Policy LU 5.4 - Density Transitions Policy LU 5.6 - Buffering Requirements Policy LU 7.4 - Scale and Design of New Commercial Uses Policy LU 7.6 - Pedestrian Friendly Development Policy T 1.6 - Transportation Impacts Policy T 2.9 - Curb Cuts Policy T 5.1 - Enhancing Bike/Pedestrian Circulation Policy T 5.2 - Incorporating Bicycle and Ped. Improvements Policy T 6.1 - Surface Parking Alternatives Policy EP 8.4 - Noise and Light Impacts Policy UD 2.1 - Building Orientation Policy UD 2.3 - Activating the Street Policy UD 2.4 - Transitions in Building Intensity Policy UD 2.6 - Parking Location and Design Policy UD 4.2 - Streets as Public Spaces Policy UD 4.3 - Improving Streetscape Design Policy UD 4.4 - Management of Sidewalk Space Policy UD 4.5 - Improving the Street Environment Policy UD 6.1 - Encouraging Pedestrian-Oriented Uses Policy UD 6.2 - Ensuring Pedestrian Comfort and Convenience Policy UD 6.3 - Pedestrian Scale Lighting Policy UD 6.4 - Appropriate Street Tree Selection Policy UD 7.3 - Design Guidelines
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Contact Information

<i>Staff</i>	Stan Wingo, (919)516-2663
<i>Applicant</i>	Mack Paul, (919)590-0377, mpaul@morningstarlawgroup.com
<i>Citizens Advisory Council</i>	Wade CAC

Case Overview

The subject property is located on Hillsborough Street between Enterprise Court and Maiden Lane. Surrounding zoning includes Residential-20 to the north, Office and Institution-2 with Pedestrian Business Overlay to the west, Buffer Commercial with Pedestrian Business Overlay to the east and Office & Institution-1 to the south. Surrounding land uses include single and multifamily residential as well as a religious institutional use to the north, student housing to the west, retail to the east and North Carolina State University to the south. The University bell tower is located directly across Hillsborough Street from the subject property.

The rezoning request proposes amending the current Pedestrian Business Overlay District to increase allowable building heights fronting on Hillsborough Street, and to rezone a residential property on Maiden Lane to Neighborhood Business Conditional Use to remain in the Pedestrian Business Overlay.

Exhibit C & D Analysis

1. Consistency of the proposed rezoning with the Comprehensive Plan and any applicable City-adopted plan(s)

1.1 Future Land Use

The rezoning request comprises two parcels, located within two different Future Land Use classifications. The larger parcel, fronting on Hillsborough, is designated as being appropriate for Neighborhood Mixed Use development. The request is consistent with this designation. The smaller parcel fronting on Maiden is designated as Moderate Density Residential. The request as currently proposed would be inconsistent with this Future Land Use designation.

1.2 Policy Guidance

The following policy guidance is applicable with this request:

Policy LU 1.3 - Conditional Use District Consistency
All conditions proposed as part of a conditional use district (CUD) should be consistent with the Comprehensive Plan

Proposal is consistent with this policy. Conditions offered by the applicant are consistent with the Comprehensive Plan.

Policy LU 2.2 - Compact Development
New development and redevelopment should use a more compact land use pattern to support the efficient provision of public services, improve the performance of transportation networks, preserve open space, and reduce the negative impacts of low intensity and non-contiguous development.

Proposal is consistent with this policy. The request to amend the Pedestrian Business Overlay and rezone the subject property will provide the applicant more opportunity for redevelopment, and in doing so will promote a compact pattern of development.

Policy LU 2.6 - Zoning and Infrastructure Impacts
Carefully evaluate all amendments to the zoning map that significantly increase permitted density or floor area to ensure that impacts to infrastructure capacity resulting from the projected intensification of development are adequately mitigated or addressed.

Proposal is consistent with this policy. There were no additional impacts identified.

Policy LU 4.5 - Connectivity
New development and redevelopment should provide pedestrian and vehicular connectivity between individual development sites to provide alternative means of access along corridors.

Proposal is consistent with this policy. The subject property encompasses an entire block face on Hillsborough Street, with residential land uses located behind. Cross access is not recommended.

Policy LU 5.1 - Reinforcing the Urban Pattern

New development should be visually integrated with adjacent buildings, and more generally with the surrounding area. Quality design and site planning is required so that new development opportunities within the existing urban fabric of Raleigh are implemented without adverse impacts on local character and appearance.

Proposal is consistent with this policy. The subject property is located within a larger overall Pedestrian Business Overlay District. The regulations associated with this Overlay help to ensure a consistent pattern of development along Hillsborough Street.

Policy LU 5.2 - Managing Commercial Development Impacts

Manage new commercial development using zoning regulations and through the conditional use zoning and development review processes so that it does not result in unreasonable and unexpected traffic, parking, litter, shadow, view obstruction, odor, noise, and vibration impacts on surrounding residential uses.

Proposal is consistent with this policy. Applicant has included several conditions and Streetscape provisions to help transition to adjacent residential and lower intensity uses.

Policy LU 5.4 – Density Transitions

Low to medium density residential development and/or low-impact office uses should serve as transitional densities between lower-density neighborhoods and more intensive commercial and residential uses. Where two areas designated for significantly different development intensity abut on the Future Land Use Map, the implementing zoning should ensure that the appropriate transition occurs on the site with the higher intensity.

Proposal is consistent with this policy. Applicant has provided zoning conditions to limit adverse impacts to adjacent residential uses. Conditions provide transitions to lower intensity uses.

Policy LU 5.6 – Buffering Requirements

New development adjacent to areas of lower intensity should provide effective physical buffers to avoid adverse effects. Buffers may include larger setbacks, landscaped or forested strips, transition zones, fencing, screening, height and/or density step downs, and other architectural and site planning measures that void potential conflicts.

Proposal is currently consistent with this policy. Applicant has included additional buffering to shield parking decks located on site from adjacent residential.

Policy LU 7.4 - Scale and Design of New Commercial Uses

New uses within commercial districts should be developed at a height, mass, scale and design that is appropriate and compatible with surrounding areas.

Proposal is inconsistent with this policy. Applicant should consider building step backs from Hillsborough Street to ensure compatible building scale and massing.

Policy LU 7.6 – Pedestrian-Friendly Development

New commercial developments and redeveloped commercial areas should be pedestrian-friendly.

Proposal is currently consistent with this policy. Current Streetscape Plan ensures pedestrian-friendly development.

Policy T 1.6 – Transportation Impacts
Identify and address transportation impacts before a development is implemented.

Proposal is consistent with this policy. No additional transportation impacts were identified.

Policy T 5.1 – Enhancing Bicycle/Pedestrian Circulation
Enhance pedestrian and bicycle circulation, access, and safety along corridors, downtown, in activity and employment centers, at densely developed areas and transit stations, and near schools, libraries, and parks.

Proposal is consistent with this policy. Current Streetscape Plan ensures sufficient bicycle and pedestrian circulation.

Policy T 5.2 – Incorporating Bicycle and Pedestrian Improvements
All new developments, roadway construction projects, and roadway resurfacing projects in the City of Raleigh’s jurisdiction should include appropriate bicycle facilities as indicated in the Recommended Bicycle Network of the 2008 City of Raleigh Bicycle Transportation Plan.

Proposal is consistent with this policy. Current Streetscape Plan includes several pedestrian and bicycle improvements.

Policy T 6.1 – Surface Parking Alternatives
Reduce the amount of land devoted to parking through measures such as development of parking structures and underground parking, the application of shared parking from mixed-use developments, flexible ordinance requirements, maximum parking standards and the implementation of Transportation Demand Management plans to reduce parking needs.

Proposal is consistent with this policy. Current proposal will include structured parking.

Policy EP 8.4 – Noise and Light Impacts
Mitigate potential noise and light pollution impacts from new development on adjoining residential properties.

Proposal is consistent with this policy. Applicant has included a zoning condition that limits lighting to full cut-off shielded design.

Policy UD 2.1 – Building Orientation
Buildings in mixed-use developments should be oriented along streets, plazas and pedestrian ways. Their facades should create an active and engaging public realm.

Proposal is consistent with this policy. Buildings are oriented along streets.

Policy UD 2.3 – Activating the Street

New retail and mixed-use centers should activate the pedestrian environment of the street frontage in addition to internal pedestrian networks and connections.

Proposal is consistent with this policy. Current Streetscape Plan will help to provide a pedestrian environment as well as active ground floor uses.

Policy UD 2.4 – Transitions in Building Intensity

Establish gradual transitions between large-scale and small-scale development. The relationship between taller, more visually prominent buildings and lower, smaller buildings (such as single family or row houses) can be made more pleasing when the transition is gradual rather than abrupt. The relationship can be further improved by designing larger buildings to reduce their apparent size and recessing the upper floors of the building to relate to the lower scale of the adjacent properties planned for lower density.

Proposal is consistent with this policy. Building height will be tapered down from Hillsborough Street to the residential uses adjacent.

Policy UD 2.6 – Parking Location and Design

New surface parking lots should be avoided within mixed-use centers. Instead, shared parking garages with active ground floor uses and architectural treatments for all facades visible from a public right-of-way should be used.

Proposal is consistent with this policy. Redevelopment of the subject property will include structured parking located at the rear of the site.

Policy UD 4.3 – Improving Streetscape Design

Improve the appearance and identity of Raleigh's streets through the design of street lights, paved surfaces, landscaped areas, bus shelters, street "furniture", and adjacent building facades.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan. The University Village Streetscape Plan includes provisions for all of these items.

Policy UD 4.4 – Management of Sidewalk Space

Manage Raleigh's sidewalk space in a way that promotes pedestrian safety, efficiency, and comfort and provides adequate space for tree boxes. Sidewalks should enhance the visual character of streets, with landscaping and buffer planting used to reduce the impacts of vehicle traffic.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan. The University Village Streetscape Plan provides several enhancements to the pedestrian space, while also promoting an appropriate visual character along Hillsborough Street.

Policy UD 4.5 – Improving the Street Environment

Create attractive and interesting commercial streetscapes by promoting ground level retail and desirable street activities, making walking more comfortable and convenient, ensuring that sidewalks are wide enough to accommodate pedestrian traffic, minimizing curb cuts and driveways, and avoiding windowless facades and gaps in the street wall.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan. The University Village Streetscape Plan provides several

enhancements to the pedestrian space, while also promoting an appropriate visual character along Hillsborough Street.

Policy UD 6.1 – Encouraging Pedestrian-Oriented Uses
New development, streetscape, and building improvements in Downtown and mixed-use corridors and centers should promote high intensity, pedestrian-oriented use and discourage automobile-oriented uses and drive-through uses.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan which promotes and encourages these types of uses.

Policy UD 6.2 – Ensuring Pedestrian Comfort and Convenience
Promote a comfortable and convenient pedestrian environment by requiring that buildings face the sidewalk and street area. On-street parking should be provided along the pedestrian streets and surface parking should be in the rear. This should be applied in new development, wherever feasible, especially on transit and urban corridors and in mixed-use centers.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan which provides several pedestrian related amenities and improvements.

Policy UD 6.3 – Pedestrian Scale Lighting
Ensure that pedestrian-scale lighting is provided as a means of providing a safe and visible pedestrian realm as well as establishing a theme or character for different streets and neighborhoods.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan which provides provisions that ensure pedestrian scale lighting.

Policy UD 6.4 – Appropriate Street Tree Selection
Street tree planting should be appropriate to the function of the street. For example:

Trees on commercial streets should complement the face of the buildings;
Trees on residential streets should shade both the street and the sidewalk, and serve as a visual buffer between the street and the home; and
In high traffic areas and Downtown, trees should be planted in tree wells with grates over top to protect the roots.

Proposal is consistent with this policy. Proposal is included within a larger overall Streetscape Plan.

Policy UD 7.3 – Design Guidelines
Proposal would be subject to the Urban Design Guidelines as referenced in Table UD-1

Proposal is consistent with the Urban Design Guidelines.

Elements of Mixed-Use Areas

1. *All Mixed-Use Areas should generally provide retail (such as eating establishments, food stores, and banks), office, and residential uses within walking distance of each other.*

Applicant Response: The Neighborhood Business district permits retail, office and residential uses, and the Pedestrian Business Overlay District permits increased residential density. The proposed zoning districts facilitate mixed-use development, and the properties are located within an existing pedestrian-oriented mixed use development.

Mixed-Use Areas /Transition to Surrounding Neighborhoods

2. *Within all Mixed-Use Areas buildings that are adjacent to lower density neighborhoods should transition (height, design, distance and/or landscaping) to the lower heights or be comparable in height and massing.*

Applicant Response: The properties are not adjacent to low density neighborhoods. The surrounding area is zoned Residential-20 and many of the properties are used as multi-family dwellings. However, the solar access angle and the proposed height map in the University Village Streetscape Plan ensure an appropriate height transition to adjacent properties.

Mixed-Use Areas / The Block, The Street and The Corridor

3. *A mixed use area's road network should connect directly into the neighborhood road network of the surrounding community, providing multiple paths for movement to and through the mixed use area. In this way, trips made from the surrounding residential neighborhood(s) to the mixed use area should be possible without requiring travel along a major thoroughfare or arterial.*

Applicant Response: The properties front along a major thoroughfare and are bordered on each side by streets that connect to and through the surrounding community, and provide paths for movement to and through the mixed use area without requiring travel on a major thoroughfare or arterial.

4. *Streets should interconnect within a development and with adjoining development. Cul-de-sacs or dead-end streets are generally discouraged except where topographic conditions and/or exterior lot line configurations offer no practical alternatives for connection or through traffic. Street stubs should be provided with development adjacent to open land to provide for future connections. Streets should be planned with due regard to the designated corridors shown on the Thoroughfare Plan.*

Applicant Response: The existing roadway system complies with current City of Raleigh standards, and no cul-de-sacs or dead-end streets are proposed as part of this development.

5. *Block faces should have a length generally not exceeding 660 feet.*

Applicant Response: the existing block face length along Hillsborough Street is approximately 235 feet.

Site Design/Building Placement

6. *A primary task of all urban architecture and landscape design is the physical definition of streets and public spaces as places of shared use. Streets should be lined by buildings rather than parking lots and should provide interest especially for pedestrians. Garage entrances and/or loading areas should be located at the side or rear of a property.*

Applicant Response: The properties are subject to the University Village Streetscape Plan, which requires that buildings be placed along the street and sidewalks and parking be located behind the building. Entrances to the proposed parking structure are proposed to be located off the side streets adjacent to the properties.

7. *Buildings should be located close to the pedestrian street (within 25 feet of the curb), with off-street parking behind and/or beside the buildings.*

Applicant Response: As required by the University Village Streetscape Plan, the new building is proposed to be located along Hillsborough Street with no intervening parking, with the off-street parking located behind the building. The new development will provide the wider sidewalks required by the Pedestrian Business Overlay District.

8. *If the building is located at a street intersection, the main building or part of the building placed should be placed at the corner. Parking, loading or service should not be located at an intersection.*

Applicant Response: The properties are situated along Hillsborough Street, between the intersections with Enterprise Street and Maiden Lane. The new building will likely span the length of the site along Hillsborough Street so that the main building will be placed at both corners.

Site Design/Urban Open Space

9. *To ensure that urban open space is well-used, it is essential to locate and design it carefully. The space should be located where it is visible and easily accessible from public areas (building entrances, sidewalks). Take views and sun exposure into account as well.*

Applicant Response: The new building will be located immediately adjacent to the wide sidewalks along Hillsborough Street that serve the student body of NC State University and the surrounding community. The developer intends to locate a plaza at the corner of Maiden Lane and Hillsborough Street, directly across from the NC State bell tower, which will serve future active uses located on the ground floor of the new building.

10. *New urban spaces should contain direct access from the adjacent streets. They should be open along the adjacent sidewalks and allow for multiple points of entry. They should also be visually permeable from the sidewalk, allowing passersby to see directly into the space.*

Applicant Response: The plaza noted above will likely be located immediately adjacent to the public sidewalk system, which will provide points of entry to the public.

11. *The perimeter of urban open spaces should consist of active uses that provide pedestrian traffic for the space including retail, cafés, and restaurants and higher-density residential.*

Applicant Response: The new building will likely contain accessory retail uses on the ground floor, adjacent to the street and the pedestrian areas organized

around the streetscape. Access to these retail uses may be made from outside storefront entrances and form the interior of the new building.

12. *A properly defined urban open space is visually enclosed by the fronting of buildings to create an outdoor "room" that is comfortable to users.*

Applicant Response: The plaza noted above will be defined by its placement at the corner of Hillsborough Street and Maiden Lane, directly across Hillsborough Street from the NC State bell tower. The space will be defined by the new building, which will be located adjacent to the wide sidewalk.

Site Design/Public Seating

13. *New public spaces should provide seating opportunities.*

Applicant Response: The developer intends to provide seating to accommodate the proposed ground-floor retail space via benches, chairs and seat walls.

Site Design/Automobile Parking and Parking Structures

14. *Parking lots should not dominate the frontage of pedestrian-oriented streets, interrupt pedestrian routes, or negatively impact surrounding developments.*

Applicant Response: As required by the University Village Streetscape Plan, the planned parking will be located behind the new building so that it does not impact the pedestrian-oriented Hillsborough Street. The parking is planned to be nestled into the grades of the site to maximize its obscurity.

15. *Parking lots should be located behind or in the interior of a block whenever possible. Parking lots should not occupy more than 1/3 of the frontage of the adjacent building or not more than 64 feet, whichever is less.*

Applicant Response: As required by the University Village Streetscape Plan, the parking will be located behind the new building so that it will not be visible from the frontage of the properties.

16. *Parking structures are clearly an important and necessary element of the overall urban infrastructure but, given their utilitarian elements, can give serious negative visual effects. New structures should merit the same level of materials and finishes as that a principal building would, care in the use of basic design elements can make a significant improvement.*

Applicant Response: If a parking structure is located on the properties, the developer intends to comply with this guideline by incorporating architectural precast concrete and landscaping and construct it in such a way as to nestle the structure into the grades to minimize visibility of the structure and the headlights of the automobiles.

Site Design/Transit Stops

17. *Higher building densities and more intensive land uses should be within walking distance of transit stops, permitting public transit to become a viable alternative to the automobile.*

Applicant Response: There are multiple bus stops located within walking distance of the properties that serve NC State University, downtown Raleigh and Cameron Village. There is a bus stop directly across the street for both

directions. Also, the properties are within one-half mile of a planned rail transit station on Pullen Road.

18. *Convenient, comfortable pedestrian access between the transit stop and the building entrance should be planned as part of the overall pedestrian network.*

Applicant Response: The properties front along the recently redeveloped Hillsborough Street streetscape with wider sidewalks that provide pedestrian access between transit stops and the building entrance.

Site Design/Environmental Protection

19. *All development should respect natural resources as an essential component of the human environment. The most sensitive landscape areas, both environmentally and visually, are steep slopes greater than 15 percent, watercourses, and floodplains. Any development in these areas should minimize intervention and maintain the natural condition except under extreme circumstances. Where practical, these features should be conserved as open space amenities and incorporated in the overall site design.*

Applicant Response: There are no environmentally sensitive features on the site, so this guideline does not apply.

Street Design/General Street Design Principles

20. *It is the intent of these guidelines to build streets that are integral components of community design. Streets should be designed as the main public spaces of the City and should be scaled for pedestrians.*

Applicant Response: Hillsborough Street was recently reconstructed implementing wider walks and trees to enhance pedestrian use and comfort. Also, the developer will widen sidewalks as required by the Pedestrian Business Overlay District.

21. *Sidewalks should be 5-8 feet wide in residential areas and located on both sides of the street. Sidewalks in commercial areas and Pedestrian Business Overlays should be a minimum of 14-18 feet wide to accommodate sidewalk uses such as vendors, merchandising and outdoor seating.*

Applicant Response: The recently completed Hillsborough Street project incorporates wider sidewalks to promote vendor use, and the developer will provide wider sidewalks where required by the Pedestrian Business Overlay District.

22. *Streets should be designed with street trees planted in a manner appropriate to their function. Commercial streets should have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets should provide for an appropriate canopy, which shadows both the street and sidewalk, and serves as a visual buffer between the street and the home. The typical width of the street landscape strip is 6-8 feet. This width ensures healthy street trees, precludes tree roots from breaking the sidewalk, and provides adequate pedestrian buffering. Street trees should be at least 6 1/4" caliper and should be consistent with the City's landscaping, lighting and street sight distance requirements.*

Applicant Response: The recently completed Hillsborough Street project incorporates wider sidewalks and street trees, and any additional improvements will complement the existing sidewalks and plantings.

Street Design/Spatial Definition

23. *Buildings should define the streets spatially. Proper spatial definition should be achieved with buildings or other architectural elements (including certain tree plantings) that make up the street edges aligned in a disciplined manner with an appropriate ratio of height to width.*

Applicant Response: The developer intends to make use of the ground floor of the new building to enhance pedestrian scale of the space along the street edge. Also, the proposed plaza along Hillsborough Street will provide additional human scale spatial definition by the use of seating and helping to activate the pedestrian and user interface along Hillsborough Street.

Building Design/Facade Treatment

24. *The primary entrance should be both architecturally and functionally on the front facade of any building facing the primary public street. Such entrances shall be designed to convey their prominence on the fronting facade.*

Applicant Response: The developer intends to locate a primary entrance to the new building along Hillsborough Street for the primary use as well as other potential entrances for the retail uses, which will provide a strong pedestrian link to wide sidewalks. The developer intends to incorporate architectural features to convey the prominence of the entrance by using methods as suggested in the University Village Streetscape Plan.

25. *The ground level of the building should offer pedestrian interest along sidewalks. This includes windows entrances, and architectural details. Signage, awnings, and ornamentation are encouraged.*

Applicant Response: The developer intends to recess the new building at the ground-floor level and provide canopies and other accents at entrances where appropriate to provide pedestrian interest and as suggested in the University Village Streetscape Plan.

Building Design/Street Level Activity

26. *The sidewalks should be the principal place of pedestrian movement and casual social interaction. Designs and uses should be complementary to that function.*

Applicant Response: The developer will provide wide sidewalks as required by the Pedestrian Business Overlay District. These and other publicly accessible sidewalks surrounding the properties are one of the primary design elements to link the new building with the surrounding neighborhoods, Hillsborough Street business and NC State University. The courtyard area will be a place to enhance the casual and active interaction between the project and the pedestrian movement.

2. Compatibility of the proposed rezoning with the property and surrounding area

The subject property is located on Hillsborough Street directly across from North Carolina State University. The site encompasses the entire block face between Enterprise Street and Maiden Lane. Surrounding land uses include single and multifamily residential as well as a religious institutional use to the north, multifamily student housing to the west, the University located across Hillsborough to the south, and retail uses to the east. Property to the north is zoned residential, with office and institutional zoning to the west and south and commercial to the east. The subject property is also within the University Village Pedestrian Business Overlay District which encompasses a large area fronting along the north side of Hillsborough Street. The Future Land Use surrounding the property includes Moderate Residential to the north, Institutional to the south and Neighborhood Mixed Use to the East and West along Hillsborough.

Zoning conditions offered by the applicant provide a list of prohibited uses, buffering to adjacent residential as well as restricted lighting on the property. Amendments to the Streetscape Plan include increased heights along Hillsborough Street for this site, as well as provisions for screening the proposed parking deck.

3. Public benefits of the proposed rezoning

While the proposed rezoning request is inconsistent with the Future Land Use Map, it should be noted that it is consistent with the majority of applicable Comprehensive Plan policies as well as the Urban Design Guidelines. The proposal would increase redevelopment opportunities on the subject property while maintaining the current regulations of the University Village Streetscape Plan, with the only amendment being increased height along Hillsborough.

4. Detriments of the proposed rezoning

There are no known detriments associated with this request.

5. The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, etc.

5.1 Transportation

<u>Primary Streets</u>	<u>Classification</u>	<u>2011 NCDOT Traffic Volume (ADT)</u>	<u>2035 Traffic Volume Forecast (CAMPO)</u>			
Hillsborough Street	Major Thoroughfare	15,000	16,000			
Enterprise Street	Commercial Street	N/A	N/A			
Maiden Lane	Commercial Street	N/A	N/A			
<u>Street Conditions</u>						
<u>Hillsborough Street</u>	<u>Lanes</u>	<u>Street Width</u>	<u>Curb and Gutter</u>	<u>Right-of-Way</u>	<u>Sidewalks</u>	<u>Bicycle Accommodations</u>
Existing	2	48'	Back-to-back curb and gutter section	75'	5' sidewalks on both sides	Striped bicycle lanes on both sides
City Standard	4	65'	Back-to-back curb and gutter section	90'	minimum 5' sidewalks on both sides	Striped bicycle lanes on both sides
Meets City Standard?	NO	NO	YES	NO	YES	YES
<u>Enterprise Street</u>	<u>Lanes</u>	<u>Street Width</u>	<u>Curb and Gutter</u>	<u>Right-of-Way</u>	<u>Sidewalks</u>	<u>Bicycle Accommodations</u>
Existing	2	31'	Back-to-back curb and gutter section	50'	5' sidewalks on both sides	None
City Standard	2	41'	Back-to-back curb and gutter section	60'	minimum 5' sidewalks on one side	N/A
Meets City Standard?	YES	NO	YES	NO	YES	N/A
<u>Maiden Lane</u>	<u>Lanes</u>	<u>Street Width</u>	<u>Curb and Gutter</u>	<u>Right-of-Way</u>	<u>Sidewalks</u>	<u>Bicycle Accommodations</u>
Existing	2	20'	Back-to-back curb and gutter section	50'	5' sidewalks on both sides	None
City Standard	2	41'	Back-to-back curb and gutter section	60'	minimum 5' sidewalks on one side	N/A
Meets City Standard?	YES	NO	YES	NO	YES	N/A
<u>Expected Traffic Generation [vph]</u>	<u>Current Zoning</u>	<u>Proposed Zoning</u>	<u>Differential</u>			
AM PEAK	59	59	0			
PM PEAK	213	80	-133			
Suggested Conditions/ Impact Mitigation:		Traffic Study Determination: Staff has reviewed a trip generation differential report for this case and a traffic impact analysis study is not recommended for Z-36-12.				
Additional Information:	Neither NCDOT nor the City of Raleigh have any roadway construction projects scheduled in the vicinity of this case.					

Impact Identified: None

5.2 Transit

No comments.

Impact Identified: None

5.3 Hydrology

<i>Floodplain</i>	None
<i>Drainage Basin</i>	Rocky Branch
<i>Stormwater Management</i>	Subject to Part 10, Chapter 9

<i>Overlay District</i>	None
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Impact Identified: None.

5.4 Public Utilities

	<i>Maximum Demand (current)</i>	<i>Maximum Demand (proposed)</i>
<i>Water</i>	6,660 gpd	UNKNOWN
<i>Waste Water</i>	6,660 gpd	UNKNOWN

There are presently existing six (6") inch sanitary sewer mains in Maiden Lane, Enterprise Street, and Hillsborough Street rights-of-ways, existing four (4") inch water mains in both Maiden Lane and Enterprise Street rights-of-way, and existing eight (8") inch and twelve (12") inch water mains in the Hillsborough Street right-of-way.

Impact Identified: The proposed rezoning impact cannot be determined at this time with the application information supplied. The subsequent development of the properties would use these mains for connection to the City's wastewater collection and water distribution systems. Down stream sanitary sewer improvements may be required, by the City, of the developer depending upon the actual use.

5.5 Parks and Recreation

The subject tract is not located adjacent to a greenway corridor. The proposed rezoning will not impact the recreation level of service

Impact Identified: None.

5.6 Urban Forestry

Combined acreage of the parcels is less than two acres. It is therefore not subject to tree conservation laws.

No tree conservation impacts will occur by this rezoning.

Impact Identified: None

5.7 Designated Historic Resources

There are no historic districts or designated landmarks on this property.

Impact Identified: None

5.8 Community Development

The subject property is not within a designated redevelopment area.

Impact Identified: None

5.9 Impacts Summary

- No additional impacts on infrastructure were identified.

5.10 Mitigation of Impacts

- N/A

6. Appearance Commission

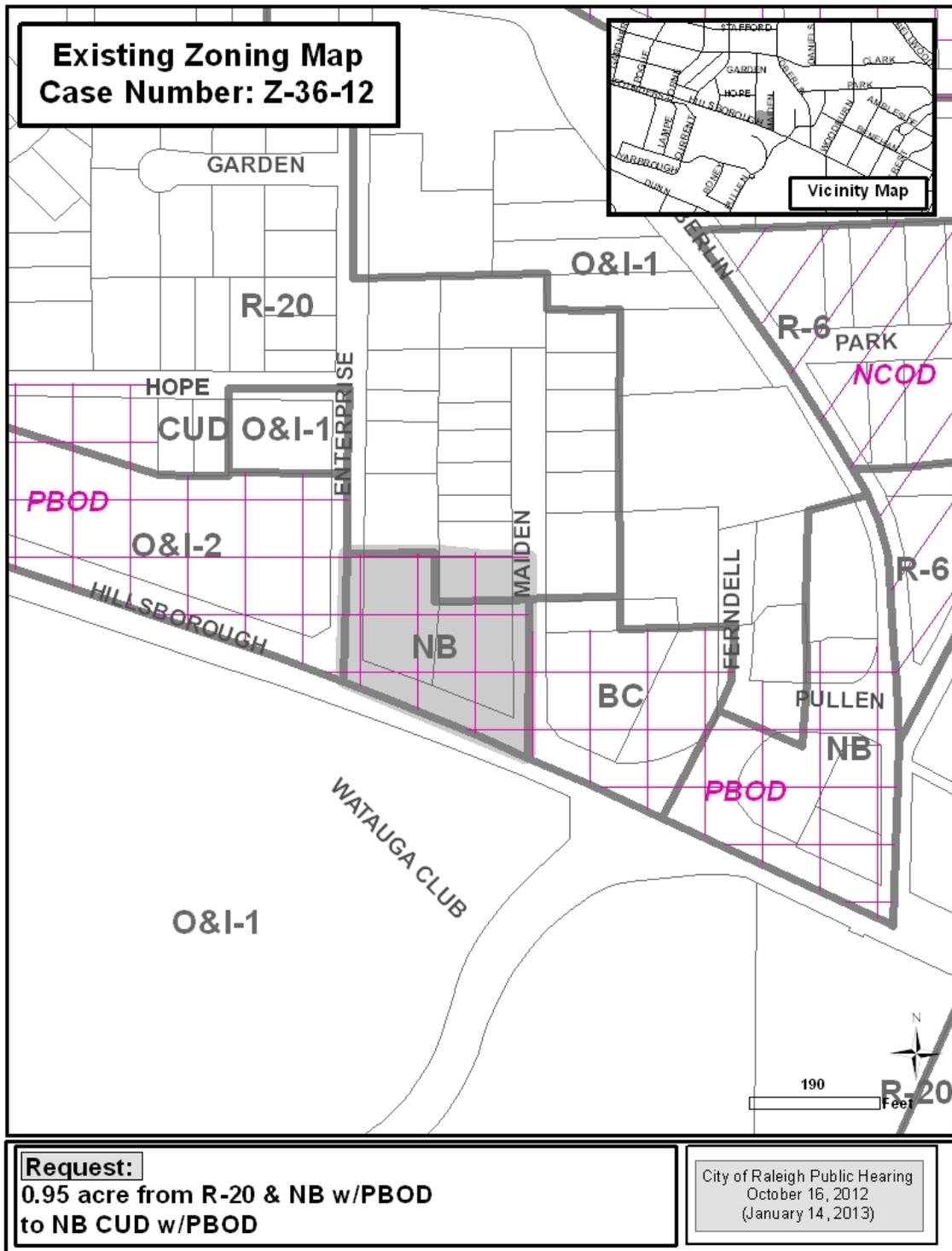
This request is subject to review by the Appearance Commission and was discussed at the DRC of the Appearance Commission on September 13, 2012. The Commission had no comments on this rezoning proposal.

7. Conclusions

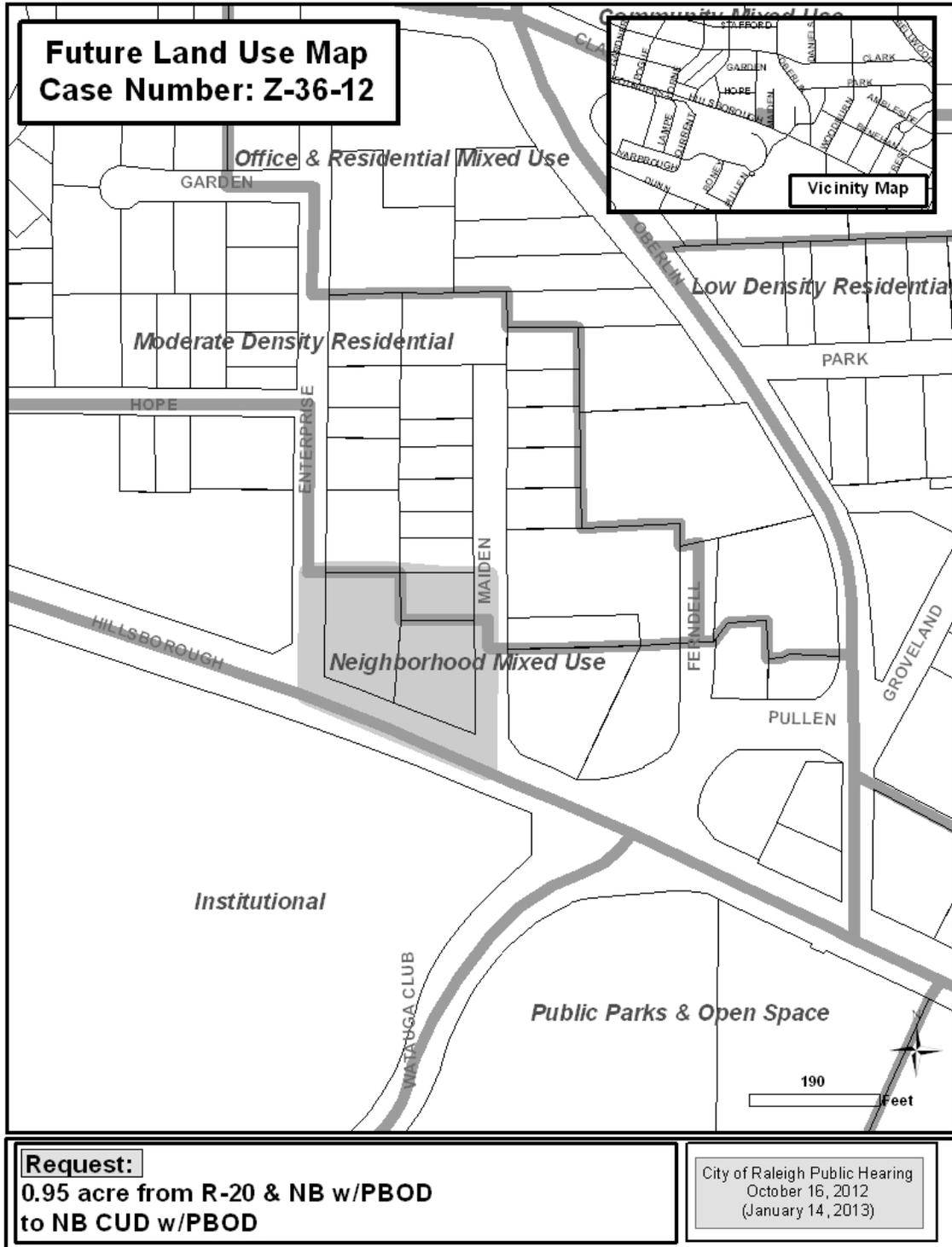
The proposed rezoning request is inconsistent with the Future Land Use Map. A portion of the subject area is located within the Moderate Density Residential designation on the map. The request would be consistent with the Neighborhood Mixed Use designation located on the remainder of the site. While the overall rezoning request is inconsistent with the Future Land Use Map, the proposal is consistent with the majority of applicable Comprehensive Plan policies and is consistent with the Urban Design Guidelines.

The majority of surrounding land uses in this area are commercial and institutional in nature, however, residential land uses are located directly adjacent to the north of the subject property. The request is compatible with the surrounding area. The applicant has offered several zoning conditions that include prohibited uses, restricted lighting, and provided provisions for the proposed parking deck to help minimize impacts. Through the proposed conditions, as well as amendments to the streetscape plan, the applicant has provided appropriate transitions and buffering to surrounding lower intensity uses.

Existing Zoning Map



Future Land Use Map





Petition to Amend the Official Zoning Map

Before the City Council of the City of Raleigh, North Carolina

Z-36-12
SSP-8-12

pd 1081.00 by eHA
24677

The following items are required with the submittal of rezoning petition. For additional information on these submittal requirements, see the *Filing Instructions* addendum.

Rezoning Application Submittal Package Checklist

Completed Rezoning Application which includes the following sections:

- Signatory Page
- Exhibit B
- Exhibit C (only for Conditional Use filing)
- Exhibit D
- Map showing adjacent property owner names with PIN's

Application Fee

- \$540 for General Use Cases
- \$1081 for Conditional Use Cases
- \$2702 for PDD Master Plans

Neighborhood Meeting Report (only for Conditional Use filing)

Receipt/ Verification for Meeting Notification Mail out

Traffic Impact Generation Report OR written waiver of trip generation from Raleigh Transportation Services Division

N/A (General Use ONLY) if applicant is not the owner must provide proof of notification to the adjacent property owners per G.S. 160A-384

- and provide proof of notification to the property owner before submitting application

2012 JUN 15 AM 11:08
CITY OF RALEIGH
CITY PLANNING DEPT

Z-36.12
SSP.8.12



Petition to Amend the Official Zoning Map

Before the City Council of the City of Raleigh, North Carolina

The petitioner seeks to show the following:

1. That, for the purposes of promoting health, morals, or the general welfare, the zoning classification of the property described herein must be changed.
2. That the following circumstance(s) exist(s):
 - City Council has erred in establishing the current zoning classification of the property by disregarding one or a combination of the fundamental principles of zoning as set forth in the enabling legislation, North Carolina General Statutes Section 160A-381 and 160A-383.
 - Circumstances have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.
 - The property has not heretofore been subject to the zoning regulations of the City of Raleigh.
3. That the requested zoning change is or will be consistent with the Raleigh Comprehensive Plan.
4. That the fundamental purposes of zoning as set forth in the N.C. enabling legislation would be best served by changing the zoning classification of the property. Among the fundamental purposes of zoning are:
 - a. to lessen congestion in the streets;
 - b. to provide adequate light and air;
 - c. to prevent the overcrowding of land;
 - d. to facilitate the adequate provision of transportation, water, sewerage, schools, parks, and other public requirements;
 - e. to regulate in accordance with a comprehensive plan;
 - f. to avoid spot zoning; and
 - g. to regulate with reasonable consideration to the character of the district, the suitability of the land for particular uses, the conservation of the value of buildings within the district and the encouragement of the most appropriate use of the land throughout the City.

THEREFORE, petitioner requests that the Official Zoning map be amended to change the zoning classification of the property as proposed in this submittal, and for such other action as may be deemed appropriate. All property owners must sign below for conditional use requests.

ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS

<u>Signature(s)</u>	<u>Print Name</u>	<u>Date</u>
	Charles D. Leffler, President	6/5/12
<u>NC State University Partnership Corp., its Member</u>		
<u>for Bell Tower Holdings, LLC</u>		
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Petition to Amend the Official Zoning Map

Before the City Council of the City of Raleigh, North Carolina

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 - d. to facilitate the adequate provision of transportation, water, sewerage, schools, parks, and other public requirements;
 - e. to regulate in accordance with a comprehensive plan;
 - f. to avoid spot zoning; and
 - g. to regulate with reasonable consideration to the character of the district, the suitability of the land for particular uses, the conservation of the value of buildings within the district and the encouragement of the most appropriate use of the land throughout the City.

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ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS

Signature(s) 	Print Name David Cox	Date 5-30-12
<hr/>		

2-36-12
551-8-12

EXHIBIT B. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

Contact Information

	Name(s)	Address	Telephone/Email
Petitioner(s) (for conditional use requests, petitioners must own petitioned property)	Bell Tower Holdings LLC	Primrose Hall Campus Box 7230 Raleigh, NC 27695	919.515.2018 ralph_recchie@ncsu.edu
	North Carolina State University Alumni of Delta Upsilon, Inc.	3 Maiden Lane Raleigh, NC 27607	704.602.3070 davidc@refinerproducts.com
Property Owner(s)	Same as above		
Contact Person(s)	Joe Whitehouse Bell View Partners LLC	7101 Creedmoor Rd. Suite 142 Raleigh, NC 27613	919.802.2233 joe@cueinc.net
	Ralph Recchie NCSU	B Holladay Hall Campus Box 7010 Raleigh, NC 27695	919.515.2018 ralph_recchie@ncsu.edu
	David Cox NCSU Alumni of Delta Upsilon, Inc.	3 Maiden Lane Raleigh, NC 27607	704.602.3070 davidc@refinerproducts.com
	Mack Paul Morningstar Law Group	630 Davis Drive Suite 200 Morrisville, NC 27560	919.590.0377 mpaul@morningstarlawgroup.com

Property information

Property Description (Wake County PIN)	0794-91-7521, 0794-91-8424, 0794-91-8535
Nearest Major Intersection	Hillsborough Street, between Enterprise Street and Maiden Lane
Area of Subject Property (in acres)	+/- .95 acres
Current Zoning Districts (include all overlay districts)	Residential-20 and Neighborhood Business, all with Pedestrian Business Overlay District
Requested Zoning Districts (include all overlay districts)	Neighborhood Business Conditional Use with Pedestrian Business Overlay District, with amendment to Streetscape Plan

Z-36-12

EXHIBIT C. Request for Zoning Change

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pg 102

Conditional Use District requested: Neighborhood Business Conditional Use

Narrative of conditions being requested:

1. The following principal uses shall be prohibited on the property:

- riding stable – all
- outdoor stadium/theater/amphitheater/racetrack – all
- rifle range – all
- cemetery
- correctional/penal facility – all
- crematory
- funeral home
- hospital
- orphanage
- adult establishment
- car wash facility
- exterminating service
- kennel/cattery
- heliport – all
- airfield/landing strip
- mini warehouse storage facility
- parking facility – principal use
- automotive service and repair facility
- landfill - all



2. Development of the property shall conform to the provisions of the Streetscape Plan for University Village on Hillsborough Street, adopted May 23, 1986 and as amended thereafter, in effect as of the date of this rezoning ordinance and including any amendments to the Streetscape Plan adopted by this rezoning ordinance (the "University Village Streetscape Plan").

3. All outdoor pole-mounted light fixtures shall be full cut-off design and shall be directed away from those properties with Wake County Parcel Identification Numbers 0794-91-7624 (DB7492, PG1) and 0794-91-8631 (DB14354, PG2693).

4. The above-ground portion of any principal or accessory building shall not be located within 5 feet of the common property line with those properties with Wake County Parcel Identification Numbers 0794-91-7624 (DB7492, PG1) and 0794-91-8631 (DB14354, PG2693).

I acknowledge that these restrictions and conditions are offered voluntarily and with knowledge of the guidelines stated in the *Filing Addendum*. If additional space is needed, this form may be copied. Each page must be signed by all property owners.

ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS

Signature(s)	Print Name	Date
	Charles D. Leffler, President	9/7/12
<hr/>		
NC State University Partnership Corp., its Member		
<hr/>		
for Bell Tower Holdings, LLC		
<hr/>		

EXHIBIT C. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

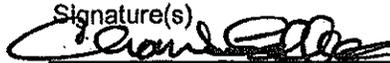
pg 2 of 2

5. For that side of a parking deck facing the properties with Wake County Parcel Identification Numbers 0794-91-7624 (DB7492, PG1) and 0794-91-8631 (DB14354, PG2693), the parking deck shall include a solid wall at least 42 inches in height as measured from the top of each parking surface.

6. In the event a parking deck is located within 20 feet of the common property line with that property with Wake County Parcel Identification Number 0794-91-8631 (DB14354, PG2693), and such property contains a detached residential unit, then those trees required as part of a transitional protective yard located between the parking deck and the common property line shall be at least 3 inches in diameter at the time of installation and shall be evergreen.

I acknowledge that these restrictions and conditions are offered voluntarily and with knowledge of the guidelines stated in the *Filing Addendum*. If additional space is needed, this form may be copied. Each page must be signed by all property owners.

ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS

Signature(s)	Print Name	Date
	Charles D. Leffler, President	9/7/12
	NC State University Partnership Corp., Its Member	
	for Bell Tower Holdings, LLC	

Z-36.12
SSP.8.12

EXHIBIT D. Request for Zoning Change

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This section is reserved for the applicant to state factual information in support of the rezoning request.

Required items of discussion:

The Planning Department is instructed not to accept any application for amending the official zoning map without a statement prepared by the applicant analyzing the reasonableness of the rezoning request. This statement *shall* address the consistency of the proposed rezoning with the Comprehensive Plan and any other applicable *City*-adopted plan(s), the compatibility of the proposed rezoning with the *property* and surrounding area, and the benefits and detriments of the proposed rezoning for the landowner, the immediate neighbors and the surrounding community.

Recommended items of discussion (where applicable):

1. An error by the City Council in establishing the current zoning classification of the property.
2. How circumstances (land use and future development plans) have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.
3. The public need for additional land to be zoned to the classification requested.
4. The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, topography, access to light and air, etc.

PETITIONER'S STATEMENT:

I. Consistency of the proposed map amendment with the Comprehensive Plan (www.raleighnc.gov).

A. Please state the recommended land use(s) for this property as shown on the Future Land Use Map and discuss the consistency of the proposed land uses:

The two parcels fronting along Hillsborough Street are designated Neighborhood Mixed Use, which recommends pedestrian-oriented commercial uses and mixed-use projects with upper story housing at a moderate density (up to 28 units per acre). The parcel fronting along Maiden Lane is designated Moderate Density Residential, which recommends residential density up to 14 units per acre and a variety of housing types.

The land uses permitted by the proposed rezoning are mostly consistent with the recommended land uses for the properties as shown on the Future Land Use Map. The Neighborhood Business District and Pedestrian Business Overlay district permit pedestrian-oriented mixed-use development. Also, the Land Use Element of the 2030 Comprehensive Plan notes that many areas designated Neighborhood Mixed Use are zoned Neighborhood Business. Therefore, the rezoning of the properties classified Neighborhood Mixed Use is consistent with the Future Land Use Map. However, the Neighborhood Business district permits non-residential land uses that are not recommended by the Moderate Density Residential category. Therefore, the proposed rezoning of the parcel classified Moderate Density Residential is inconsistent with the Future Land Use Map.

Based on the above, the majority of the rezoning is consistent with the Future Land Use Map.

B. Please state whether the subject property is located within any Area Plan or other City Council-adopted plans and policies and discuss the policies applicable to future development within the plan(s) area.

The properties are not located within any Area Plan.

EXHIBIT D. Request for Zoning Change

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- C. Is the proposed map amendment consistent or inconsistent with the Comprehensive Plan and other City Council-adopted plans and policies? All references to Comprehensive Plan policies should include both the policy number (e.g. LU 4.5) and short title (e.g. “Connectivity”).**

The proposed map amendment is consistent with the applicable prescriptive Comprehensive Plan policies as set forth below:

LU 1.2 “Future Land Use Map and Zoning Consistency.” The majority of proposed rezoning is consistent with the Future Land Use Map because .77 acres of the .95 acres subject to the rezoning are designated as Neighborhood Mixed Use. Although the proposed Neighborhood Business district is inconsistent with the Future Land Use Map designation of Moderate Density Residential for the .18 acre parcel on Maiden Lane, the request is reasonable and in the public interest given the surrounding nonresidential land uses, the property’s inclusion in the Pedestrian Business Overlay District, and the compatibility of uses permitted in the Neighborhood Business district with the surrounding multi-family residential and nonresidential uses. Based on the above, the majority of the rezoning is consistent with the Future Land Use Map and the rezoning is consistent with this policy.

LU 1.3 “Conditional Use District Consistency.” The proposed zoning condition prohibit uses that are incompatible with surrounding land uses. Prohibiting those uses listed in the condition is consistent with the Comprehensive Plan.

LU 2.6 “Zoning and Infrastructure Impacts.” Based on the Trip Generation Letter submitted with this rezoning petition, the proposed rezoning will not adversely impact the transportation infrastructure capacity in the area.

LU 4.7 “Capitalizing on Transit Access.” As shown on the Growth Framework Map, the properties are located about a quarter-mile from a planned transit station. The proposed Neighborhood Business district permits retail uses and the existing Pedestrian Business Overlay District permits high density residential uses, all of which can support and take advantage of transit infrastructure.

LU 5.4 “Density Transitions.” The properties subject to the rezoning are not near low density residential neighborhoods. Instead, the properties to the north are zoned Residential-20, classified Moderate Density Residential on the Future Land Use Map, and are used as multi-family dwellings. The Moderate Density Residential category is an appropriate category adjacent to properties classified Neighborhood Mixed Use, and the multi-family dwellings are appropriate land use transitions from commercial uses. The other surrounding properties are classified Neighborhood Mixed Use or Institutional, which are compatible with the Future Land Use Map recommendation for the properties subject to the rezoning. The proposed height map and the existing solar access angle in the University Village Streetscape Plan ensure appropriate height transitions to surrounding residential properties.

UD 2.4 “Transitions in Building Intensity.” The proposed height map and the existing solar access angle in the University Village Streetscape Plan ensure gradual height transitions to surrounding lower, smaller institutional and residential structures.

UD 7.3 “Design Guidelines.” Based on the responses to the Urban Design Guidelines submitted with this rezoning petition, the proposed rezoning is consistent with this policy.

EXHIBIT D. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

II. Compatibility of the proposed map amendment with the property and the surrounding area.

A. Description of land uses within the surrounding area (residential housing types, parks, institutional uses, commercial uses, large parking lots, thoroughfares and collector streets, transit facilities):

North: A church-related facility with a large surface parking lot and a single-family structure converted to multi-unit dwellings are located to the north of the property.

East: Single-story retail buildings with a surface parking lot along Hillsborough Street. The buildings are located to the east of the properties across Maiden Lane. Also, multi-unit dwellings with large surface parking lots are located to the east of the properties.

South: The main campus of North Carolina State University is located to the south of the properties, across Hillsborough Street, which is a major thoroughfare. In particular, the Bell Tower, academic buildings and surface parking lots are located closest to Hillsborough Street.

West: The North Hall dormitory is located to the west of the properties across Enterprise Street, and surrounded by large surface parking lots.

B. Description of existing Zoning patterns (zoning districts including overlay districts) and existing built environment (densities, building heights, setbacks, tree cover, buffer yards):

North: The parcels located to the north are zoned Residential-20 and are developed for residential structures used for institutional and single- and multi-family dwellings. The properties have high building lot coverage ratios and minimal building setbacks.

East: The properties to the east along Hillsborough Street are zoned Buffer Commercial with Pedestrian Business Overlay District. Other properties to the east are zoned Buffer Commercial, Office & Institution-1 and Residential-20. The commercial properties are developed for small single-tenant buildings with surface parking. The other properties are developed for single-family to multi-family conversions and a fraternity house.

South: The large University-owned parcel to the south is zoned Office & Institution-1 and is developed for academic and administrative office buildings and open space.

West: The property to the west is owned by the University, zoned Office & Institution-2 with Pedestrian Business Overlay District, and developed for a six-story dormitory with a large surface parking lot surrounding the building.

C. Explanation of how the proposed zoning map amendment is compatible with the suitability of the property for particular uses and the character of the surrounding area:

The majority of the property subject to the proposed rezoning is already zoned and developed for commercial uses. The proposed zoning map amendment also seeks to rezone a .18 acre parcel to allow commercial uses. The proposed rezoning would also amend the University Village Streetscape Plan to regulate height on the three properties outside of the Neighborhood Business height standards. The property is located along a major thoroughfare and in a pedestrian-oriented commercial district. The proposed rezoning permits commercial land uses and the applicable streetscape plan requires certain pedestrian-oriented infrastructure and design elements upon redevelopment. The surrounding area is non-residential in nature, particularly along Hillsborough Street. The proposed height map and existing solar

EXHIBIT D. Request for Zoning Change

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access angle height standard will ensure any redevelopment is consistent with the structure heights in the surrounding area, particularly the North Hall dormitory and the Bell Tower. Based on the current commercial development on the property and the location of the properties along a major thoroughfare in a pedestrian-oriented commercial district, the proposed rezoning is compatible with the suitability of the property for particular uses and the character of the surrounding area.

III. Benefits and detriments of the proposed map amendment.

A. For the landowner(s):

The proposed rezoning benefits the landowner of the .18 acre parcel fronting Maiden Lane by expanding the types of permitted land uses. The proposed rezoning benefits the owners of all three parcels by providing an alternative means of regulating height and by enabling the coordinated redevelopment of the three parcels.

B. For the immediate neighbors:

The proposed rezoning benefits the immediate neighbors because the rezoning restricts the types of land uses permitted on the properties currently zoned Neighborhood Business. The proposed rezoning also benefits the immediate neighbors by enabling the coordinated redevelopment of the properties in accordance with the University Village Streetscape and Parking Plan.

C. For the surrounding community:

The proposed rezoning benefits the surrounding community by enabling the redevelopment of the properties in accordance with the University Village Streetscape Plan and for the type and intensity of land uses that compliment the recently improved pedestrian experience along Hillsborough Street.

IV. Does the rezoning of this property provide a significant benefit which is not available to the surrounding properties? Explain:

No, the rezoning of the properties does not provide a significant benefit that is not available to the surrounding properties. The surrounding properties adjacent to the north side of Hillsborough Street are zoned for nonresidential land uses and are within the Pedestrian Business Overlay District.

Explain why the characteristics of the subject property support the proposed map amendment as reasonable and in the public interest.

The subject properties are located immediately adjacent to a major thoroughfare, in a pedestrian-oriented mixed use area, and immediately across the street from the main campus of North Carolina State University. The type and intensity of land uses permitted by the proposed rezoning are compatible and consistent with the built environment along Hillsborough Street. Also, the height permitted by the proposed rezoning is consistent with surrounding structures and provides an appropriate transition to nearby residential uses. The subject properties are bounded on three sides by public streets, which supports redevelopment of the three parcels in a coordinated way. Finally, the proposed rezoning is in the public interest because it promotes the type and form of commercial development envisioned by the Future Land Use Map and supported by the City's recent pedestrian infrastructure investments along Hillsborough Street.

FILING ADDENDUM: Instructions for filing a petition to amend the official Zoning Map of the City of Raleigh, North Carolina

V. Recommended items of discussion (where applicable).

- a. **An error by the City Council in establishing the current zoning classification of the property.**

Not applicable.

- b. **How circumstances (land use and future development plans) have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.**

Not applicable.

- c. **The public need for additional land to be zoned to the classification requested.**

Not applicable.

- d. **The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, topography, access to light and air, etc.**

Not applicable.

- e. **How the rezoning advances the fundamental purposes of zoning as set forth in the N.C. enabling legislation.**

The proposed rezoning advances the fundamental purposes of zoning by lessening congestion in the streets, ensuring the adequate provision of light and air to adjoining properties, regulating land use in accordance with the comprehensive plan, and regulating with reasonable consideration to the character of the district and the suitability of the land for particular uses.

As indicated by the attached Trip Generation letter, the proposed rezoning could result in minimal additional new trips in the AM and PM peak hours if developed for retail uses, and a great reduction in the daily and peak hour trips if developed for a hotel use. The proposed height map and existing solar access angle in the University Village Streetscape Plan ensures any development on the properties will allow for adequate access to light and air for nearby properties. The uses permitted by the proposed rezoning and regulations in the University Village Streetscape plan ensure development consistent with the land use and policy recommendations of the comprehensive plan. Finally, development permitted by the proposed zoning district and existing overlay district ensures compatibility with surrounding uses.

VI. Other arguments on behalf of the map amendment requested.

The Petitioner has no other arguments on behalf of the map amendment at this time.

Design Guidelines for Mixed Use Areas (Responses for Z-__-12)

RALEIGH COMPREHENSIVE PLAN

Elements of Mixed-Use Areas

1. *All Mixed-Use Areas should generally provide retail (such as eating establishments, food stores, and banks), office, and residential uses within walking distance of each other.*

The Neighborhood Business district permits retail, office and residential uses, and the Pedestrian Business Overlay District permits increased residential density. The proposed zoning districts facilitate mixed-use development, and the properties are located within an existing pedestrian-oriented mixed use development.

Mixed-Use Areas /Transition to Surrounding Neighborhoods

2. *Within all Mixed-Use Areas buildings that are adjacent to lower density neighborhoods should transition (height, design, distance and/or landscaping) to the lower heights or be comparable in height and massing.*

The properties are not adjacent to low density neighborhoods. The surrounding area is zoned Residential-20 and many of the properties are used as multi-family dwellings. However, the solar access angle and the proposed height map in the University Village Streetscape Plan ensure an appropriate height transition to adjacent properties.

Mixed-Use Areas /The Block, The Street and The Corridor

3. *A mixed use area's road network should connect directly into the neighborhood road network of the surrounding community, providing multiple paths for movement to and through the mixed use area. In this way, trips made from the surrounding residential neighborhood(s) to the mixed use area should be possible without requiring travel along a major thoroughfare or arterial.*

The properties front along a major thoroughfare and are bordered on each side by streets that connect to and through the surrounding community, and provide paths for movement to and through the mixed use area without requiring travel on a major thoroughfare or arterial.

4. *Streets should interconnect within a development and with adjoining development. Cul-de-sacs or dead-end streets are generally discouraged except where topographic conditions and/or exterior lot line configurations offer no practical alternatives for connection or through traffic. Street stubs should be provided with development adjacent to open land to provide for future connections. Streets should be planned with due regard to the designated corridors shown on the Thoroughfare Plan.*

The existing roadway system complies with current City of Raleigh standards, and no cul-de-sacs or dead-end streets are proposed as part of this development.

5. *Block faces should have a length generally not exceeding 660 feet.*

The existing block face length along Hillsborough Street is approximately 235 feet.

Site Design/Building Placement

6. *A primary task of all urban architecture and landscape design is the physical definition of streets and public spaces as places of shared use. Streets should be lined by buildings rather than parking lots and should provide interest especially for pedestrians. Garage entrances and/or loading areas should be located at the side or rear of a property.*

The properties are subject to the University Village Streetscape Plan, which requires that buildings be placed along the street and sidewalks and parking be located behind the building. Entrances to the proposed parking structure are proposed to be located off the side streets adjacent to the properties.

7. *Buildings should be located close to the pedestrian street (within 25 feet of the curb), with off-street parking behind and/or beside the buildings.*

As required by the University Village Streetscape Plan, the new building is proposed to be located along Hillsborough Street with no intervening parking, with the off-street parking located behind the building. The new development will provide the wider sidewalks required by the Pedestrian Business Overlay District.

8. *If the building is located at a street intersection, the main building or part of the building placed should be placed at the corner. Parking, loading or service should not be located at an intersection.*

The properties are situated along Hillsborough Street, between the intersections with Enterprise Street and Maiden Lane. The new building will likely span the length of the site along Hillsborough Street so that the main building will be placed at both corners.

Site Design/Urban Open Space

9. *To ensure that urban open space is well-used, it is essential to locate and design it carefully. The space should be located where it is visible and easily accessible from public areas (building entrances, sidewalks). Take views and sun exposure into account as well.*

The new building will be located immediately adjacent to the wide sidewalks along Hillsborough Street that serve the student body of NC State University and the surrounding community. The developer intends to locate a plaza at the corner of Maiden Lane and Hillsborough Street, directly across from the NC State Bell Tower, which will serve future active uses located on the ground floor of the new building.

10. *New urban spaces should contain direct access from the adjacent streets. They should be open along the adjacent sidewalks and allow for multiple points of entry. They should also be visually permeable from the sidewalk, allowing passersby to see directly into the space.*

The plaza noted above will likely be located immediately adjacent to the public sidewalk system, which will provide points of entry to the public.

11. *The perimeter of urban open spaces should consist of active uses that provide pedestrian traffic for the space including retail, cafés, and restaurants and higher-density residential.*

The new building will likely contain accessory retail uses on the ground floor, adjacent to the street and the pedestrian areas organized around the streetscape. Access to these retail uses may be made from outside storefront entrances and from the interior of the new building.

12. *A properly defined urban open space is visually enclosed by the fronting of buildings to create an outdoor "room" that is comfortable to users.*

The plaza noted above will be defined by its placement at the corner of Hillsborough Street and Maiden Lane, directly across Hillsborough Street from the NC State Bell Tower. The space will be defined by the new building, which will be located adjacent to the wide sidewalk.

Site Design/Public Seating

- 13. New public spaces should provide seating opportunities.*

The developer intends to provide seating to accommodate the proposed ground-floor retail space via benches, chairs and seat walls.

Site Design/Automobile Parking and Parking Structures

- 14. Parking lots should not dominate the frontage of pedestrian-oriented streets, interrupt pedestrian routes, or negatively impact surrounding developments.*

As required by the University Village Streetscape Plan, the planned parking will be located behind the new building so that it does not impact the pedestrian-oriented Hillsborough Street. The parking is planned to be nestled into the grades of the site to maximize its obscurity.

- 15. Parking lots should be located behind or in the interior of a block whenever possible. Parking lots should not occupy more than 1/3 of the frontage of the adjacent building or not more than 64 feet, whichever is less.*

As required by the University Village Streetscape Plan, the parking will be located behind the new building so that it will not be visible from the frontage of the properties.

- 16. Parking structures are clearly an important and necessary element of the overall urban infrastructure but, given their utilitarian elements, can give serious negative visual effects. New structures should merit the same level of materials and finishes as that a principal building would, care in the use of basic design elements can make a significant improvement.*

If a parking structure is located on the properties, the developer intends to comply with this guideline by incorporating architectural precast concrete and landscaping and construct it in such a way as to nestle the structure into the grades to minimize visibility of the structure and the headlights of the automobiles.

Site Design/Transit Stops

- 17. Higher building densities and more intensive land uses should be within walking distance of transit stops, permitting public transit to become a viable alternative to the automobile.*

There are multiple bus stops located within walking distance of the properties that serve NC State University, downtown Raleigh and Cameron Village. There is a bus stop directly across the street for both directions. Also, the properties are within one-half mile of a planned rail transit station on Pullen Road.

- 18. Convenient, comfortable pedestrian access between the transit stop and the building entrance should be planned as part of the overall pedestrian network.*

The properties front along the recently redeveloped Hillsborough Street streetscape with wider sidewalks that provide pedestrian access between transit stops and the building entrance.

Site Design/Environmental Protection

- 19. All development should respect natural resources as an essential component of the human environment. The most sensitive landscape areas, both environmentally and visually, are steep slopes greater than 15 percent, watercourses, and floodplains. Any development in these areas should minimize intervention and maintain the natural condition except under extreme circumstances. Where practical, these features should be conserved as open space amenities and incorporated in the overall site design.*

There are no environmentally sensitive features on the site, so this guideline does not apply.

Street Design/General Street Design Principles

20. *It is the intent of these guidelines to build streets that are integral components of community design. Streets should be designed as the main public spaces of the City and should be scaled for pedestrians.*

Hillsborough Street was recently reconstructed implementing wider walks and trees to enhance pedestrian use and comfort. Also, the developer will widen sidewalks as required by the Pedestrian Business Overlay District.

21. *Sidewalks should be 5-8 feet wide in residential areas and located on both sides of the street. Sidewalks in commercial areas and Pedestrian Business Overlays should be a minimum of 14-18 feet wide to accommodate sidewalk uses such as vendors, merchandising and outdoor seating.*

The recently completed Hillsborough Street project incorporates wider sidewalks to promote vendor use, and the developer will provide wider sidewalks where required by the Pedestrian Business Overlay District.

22. *Streets should be designed with street trees planted in a manner appropriate to their function. Commercial streets should have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets should provide for an appropriate canopy, which shadows both the street and sidewalk, and serves as a visual buffer between the street and the home. The typical width of the street landscape strip is 6-8 feet. This width ensures healthy street trees, precludes tree roots from breaking the sidewalk, and provides adequate pedestrian buffering. Street trees should be at least 6 1/4" caliper and should be consistent with the City's landscaping, lighting and street sight distance requirements.*

The recently completed Hillsborough Street project incorporates wide sidewalks and street trees, and any additional improvements will complement the existing sidewalks and plantings.

Street Design/Spatial Definition

23. *Buildings should define the streets spatially. Proper spatial definition should be achieved with buildings or other architectural elements (including certain tree plantings) that make up the street edges aligned in a disciplined manner with an appropriate ratio of height to width.*

The developer intends to make use of the ground floor of the new building to enhance pedestrian scale of the space along the street edge. Also, the proposed plaza along Hillsborough Street will provide additional human scale spatial definition by the use of seating and helping to activate the pedestrian and user interface along Hillsborough Street.

Building Design/Facade Treatment

24. *The primary entrance should be both architecturally and functionally on the front facade of any building facing the primary public street. Such entrances shall be designed to convey their prominence on the fronting facade.*

The developer intends to locate a primary entrance to the new building along Hillsborough Street for the primary use as well as other potential entrances for the retail uses, which will provide a strong pedestrian link to wide sidewalks. The developer intends to incorporate architectural features to convey the prominence of the entrance by using methods as suggested in the University Village Streetscape Plan.

25. *The ground level of the building should offer pedestrian interest along sidewalks. This includes windows entrances, and architectural details. Signage, awnings, and ornamentation are encouraged.*

The developer intends to recess the new building at the ground-floor level and provide canopies and other accents at entrances where appropriate to provide pedestrian interest and as suggested in the University Village Streetscape Plan.

Building Design/Street Level Activity

26. *The sidewalks should be the principal place of pedestrian movement and casual social interaction. Designs and uses should be complementary to that function.*

The developer will provide wide sidewalks as required by the Pedestrian Business Overlay District. These and other publicly accessible sidewalks surrounding the properties are one of the primary design elements to link the new building with the surrounding neighborhoods, Hillsborough Street business and NC State University. The courtyard area will be a place to enhance the casual and active interaction between the project and the pedestrian movement.

Streetscape Plan

for University Village on Hillsborough Street
Raleigh, North Carolina
May 23, 1986

Amended May 19, 1992
Public Hearing
(See Map A)

Amended March 1, 2004
See Exhibit "A"

Amended September 15, 2009



Streetscape Design

It is the intention of this plan to provide design guidelines for the public "face" of the University Village area of Hillsborough Street. The plan addresses the following elements of this public area:

BUILDING FACADES	p.7
STREET FURNITURE	p. 9
SIDEWALK DESIGN AND CONSTRUCTION	p. 11
STREET TREES	p.13
MAINTENANCE	p. 15
UTILITY LINES	p.17
SIGNS	p. 20
CROSSWALKS	p. 22
PARKING	p. 23
AWNINGS	p. 25
SOLAR ACCESSI BUILDINGH EIGHT	p. 26
SIDEWALK CAFE SEATING	p.27

The design guidelines contained in this plan are intended to coordinate change to present a common image for University Village, provide a more comfortable and safe street environment for pedestrians, improve the maintenance and general appearance of the University Village district, and coordinate the provision of necessary services to the local community.

The guidelines and locations of streetscape amenities noted in this plan may be adjusted by the Planning Director where unforeseen conflicts with underground utility lines may occur.

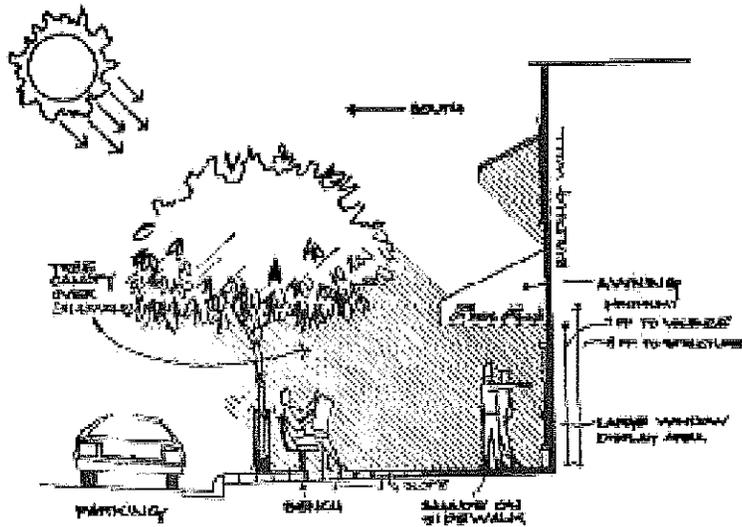


FIGURE: 1. SAMPLE STREETSCAPE SECTION ACROSS SIDEWALK. Note that a window-shopping space is created under the awning, out of the traffic way along the sidewalk. A sitting space is also created under the tree.

Sample Storefront Treatment

The following are overall concepts which represent the general character of the streetscape for any individual property.

STREET TREES IN TREE GRATES ALONG SIDEWALK EDGE using details on p. 10, 13-14

BRICK SIDEWALKS WITH CONTRASTING BRICK STRIPS BETWEEN STOREFRONTS using detail on p. 11

AN OVERHEAD OF SOME SORT, EITHER AN AWNING, A BALCONY, OR A RECESSED ENTRYWAY using guidelines p. 7,8,25

LARGE WINDOWS ON THE GROUND FLOOR using guidelines p. 7, 8

MANY PLACES TO SIT using bench p. 9

SIGNS WHICH ARE INTEGRATED WITH THE BUILDING AND ORIENTED TO THE PEDESTRIAN using guidelines p. 20, 21

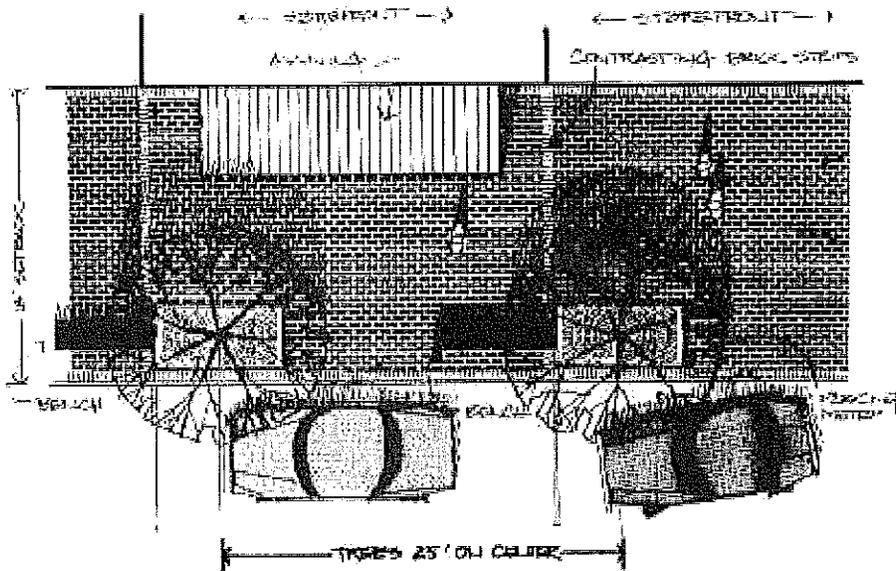


FIGURE. 2. SAMPLE STREETSCAPE IN PLAN VIEW. Note location of trees to avoid interfering with opening passenger doors on parked cars.

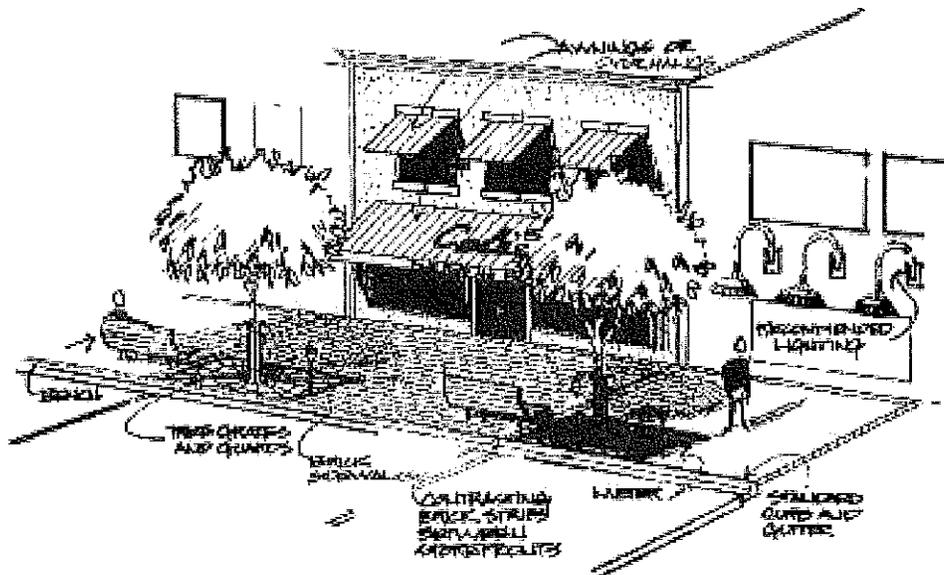


FIGURE. 3. STREETSCAPE FOR A TYPICAL STOREFRONT

Scale Structure:

Streetscape elements are to be used to establish a structure for the parts of the sidewalk that serve different purposes. The tree canopy not only provides shade, an environmental concern, but also establishes a space underneath which encourages sitting, and at the same time defines the overhead plane of the walking path.

The overhang or awning provides the same overhead plane, which defines the window-shopping area beneath, where the pedestrian can feel more comfortable slowing to look at window displays or stop for a conversation with someone else, without interfering with the pedestrian traffic flow.

An added dimension to the streetscape, an added "room", can be created using the tree, the awning, and a recessed entryway, effectively creating three spaces across the sidewalk.

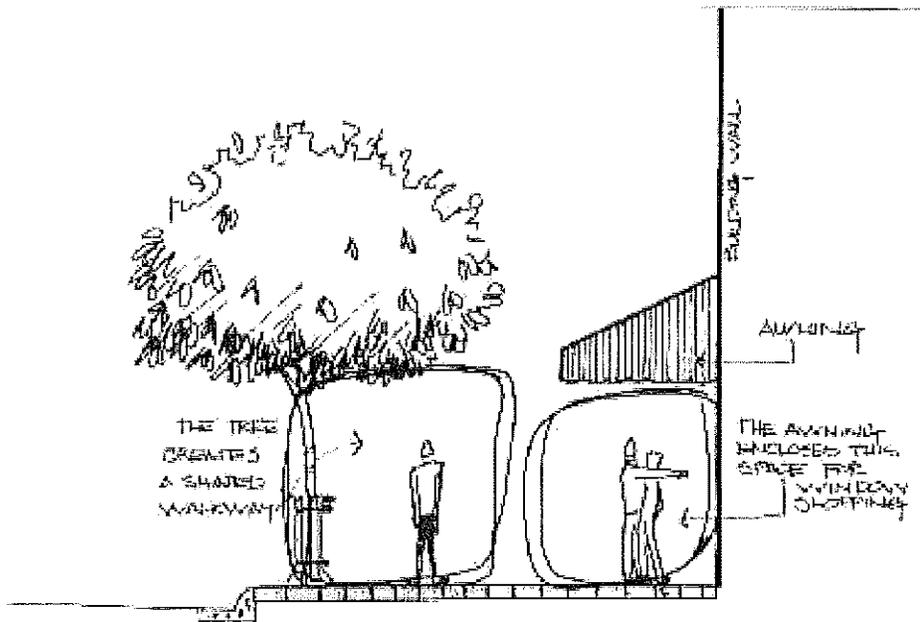


FIGURE 4. TWO-SPACE STRUCTURE USING AWNING AND TREE

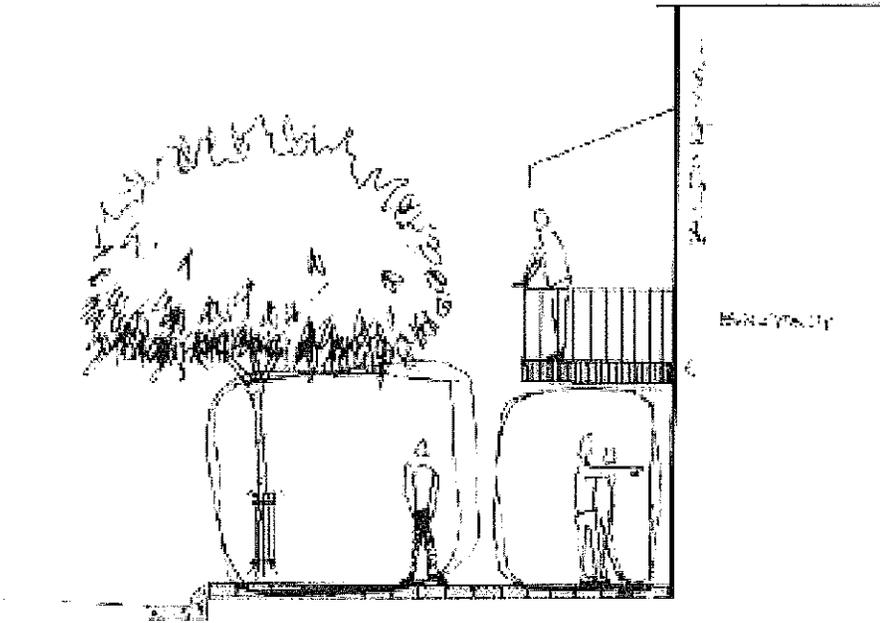


FIGURE 5. TWO-SPACE STRUCTURE USING DIFFERENT TYPES OF AWNINGS

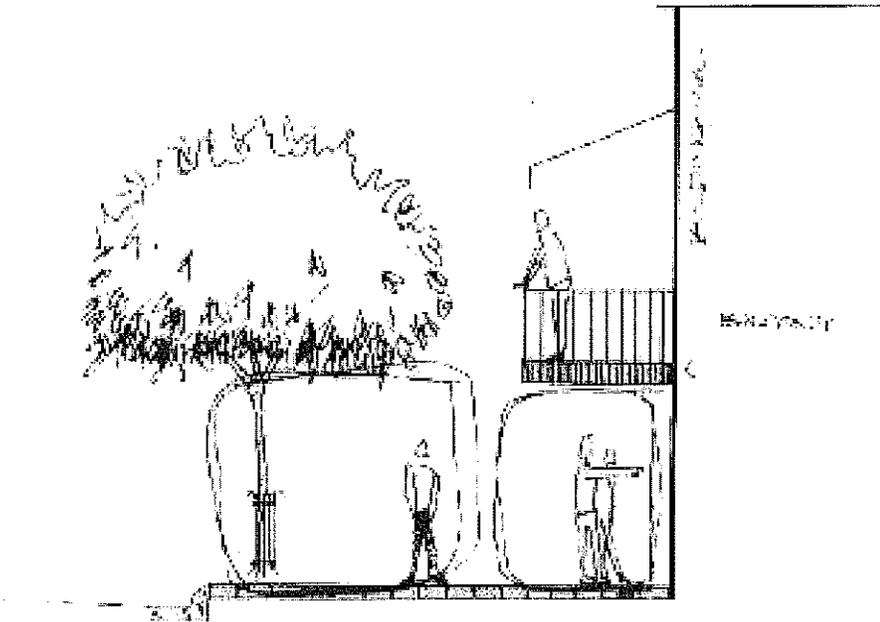


FIGURE 6. TWO-SPACE STRUCTURE WITH BALCONY INSTEAD OF AWNING

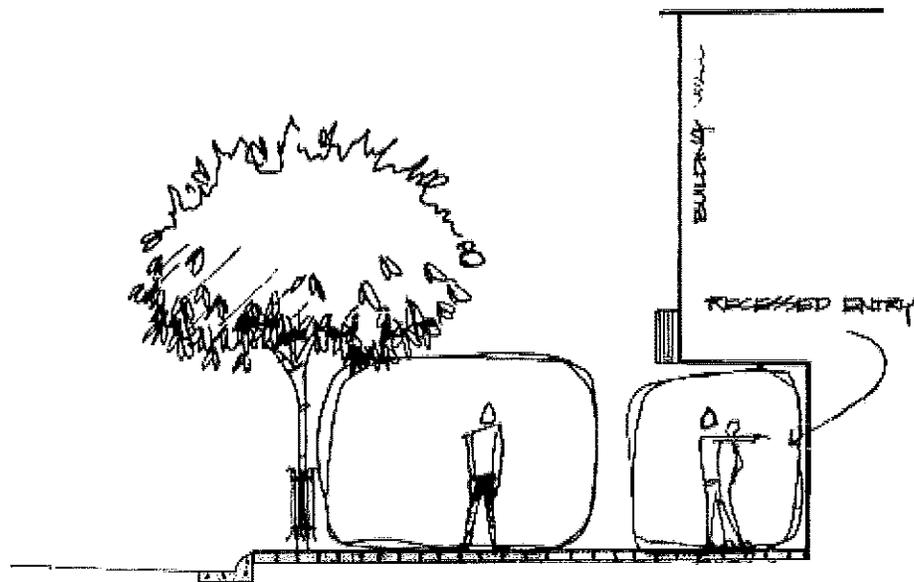


FIGURE 7. TWO-SPACE STRUCTURE WITH RECESSED ENTRY

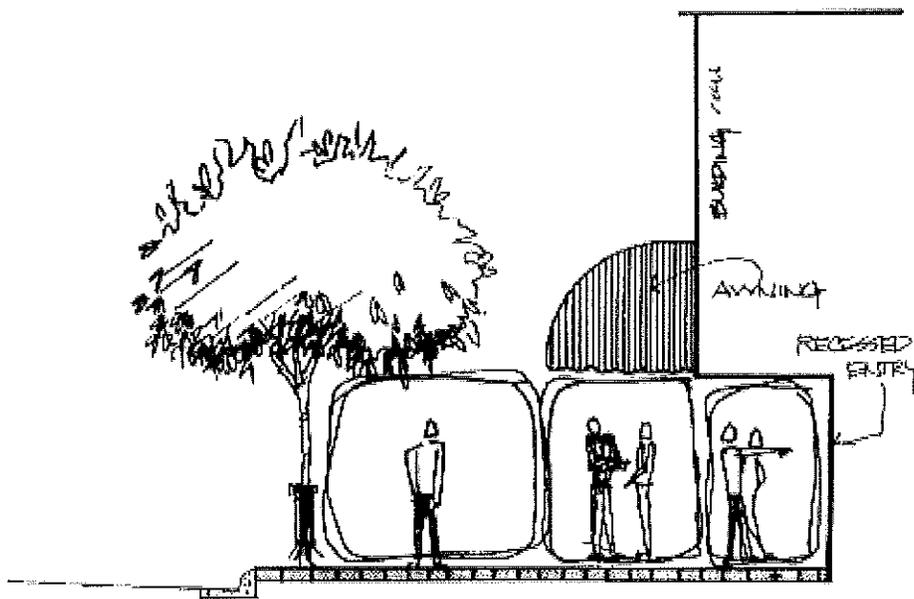


FIGURE 8. THREE-SPACE STRUCTURE WITH AWNING AND RECESSED ENTRY

Building Facades

The front of each building on the street contributes to the overall streetscape. It can either add to or subtract from the character of the street. Efforts must be made to make sure each building has the same SCALE and ORIENTATION, not necessarily the same architectural style or building material.

The diversity of character and architecture in University Village is one of its greatest assets. The intention of the streetscape plan is to encourage such diversity, while maintaining a connecting theme of scale. Please refer to the section in this document which describes a typical storefront for more design information on building facades.

The following key elements should be included in each building facade:

-OVERHEAD OR AWNING. This may be any structure which overhangs the sidewalk, recessed entryway, an awning, or a balcony, which reduces the scale of the sidewalk area to "people-size" and establishes a space to stop and window-shop outside the main traffic area of the sidewalk.

-LARGE WINDOWS FACING THE SIDEWALK. A clear view of activity inside a store is the best and one of the least expensive types of advertising available. It also brings the activity of the stores and restaurants out onto the sidewalk, which attracts more pedestrians to the area.

-SECOND STORY WINDOWS. For businesses located on the second floor, the view of the street from the interior is attractive to the patron, and focuses attention on the sidewalk/ pedestrian environment. The advertising benefits of a view from the sidewalk are still applicable for second-story uses.

-SUBDUED COLORS. Building colors should be relatively neutral, or at least subdued, and brighter colors used for accent, either in an awning or sign, or to accentuate details of the building facade.

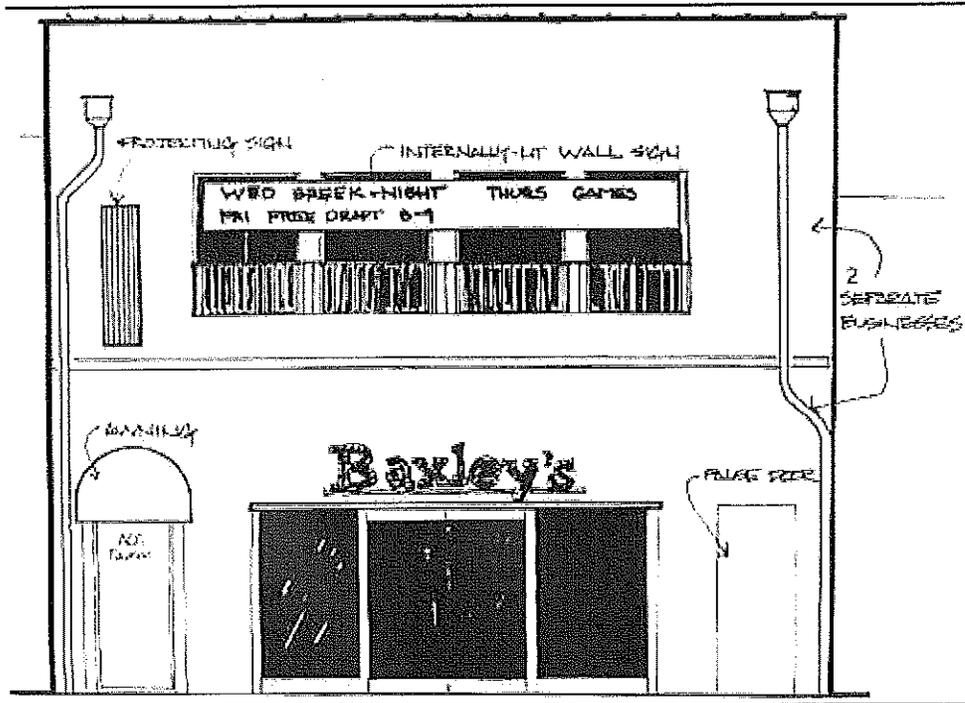


FIGURE 1. EXISTING UNIVERSITY FAÇADE

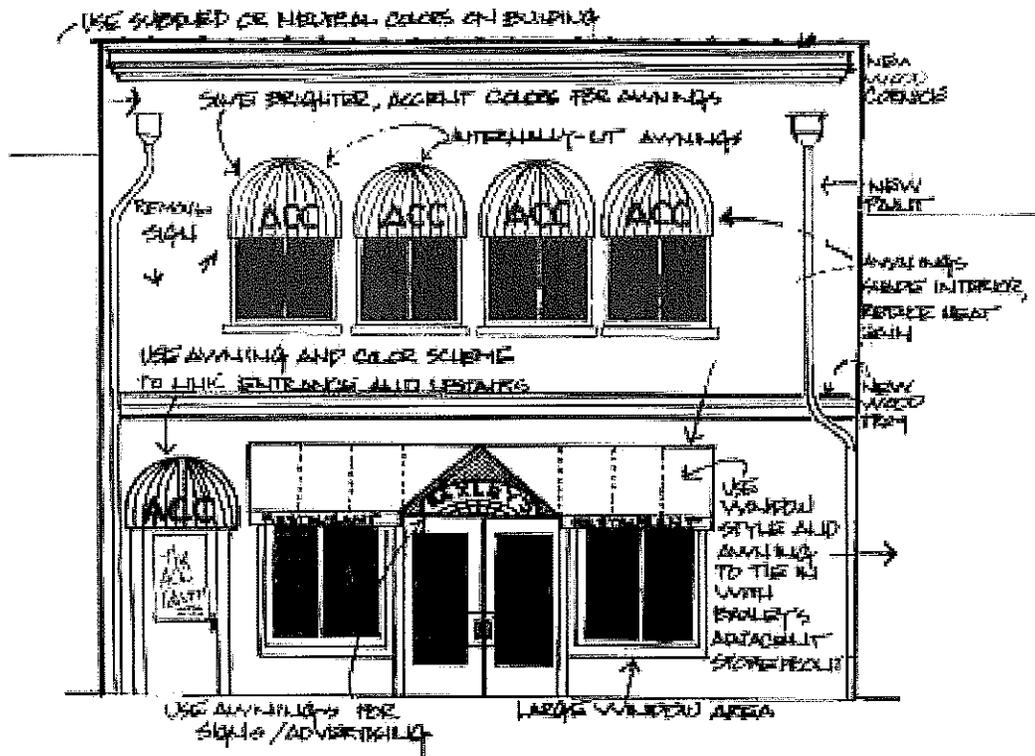
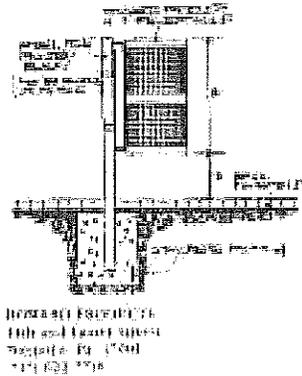


FIGURE 2. EXAMPLE OF RENOVATED FAÇADE

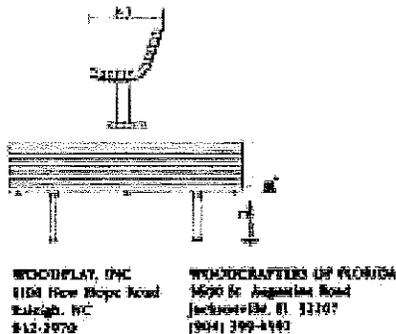
Street Furniture

It is important to the consistency of the streetscape that a pattern for the design and use of street furniture be established. For all improvements in the public right-of-way, the following furnishings, or a similar substitute, should be used. The manufacturers listed are not the only manufacturers from whom similar products are available. They are suggested suppliers.



Trash Receptacle

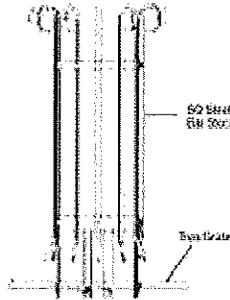
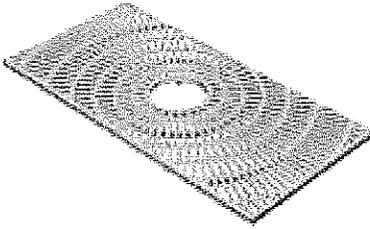
This design is simple, easy to empty, and similar to the already-common receptacles on the University Campus.



Benches

This type of bench was selected because it is simple. Yet attractive, durable, and commonly available from a variety of manufactures in this style.

Rectangular grate design featuring two light openings per grate with bolted down light access grates. Tree opening is expandable. Weight per a set – 645 pounds

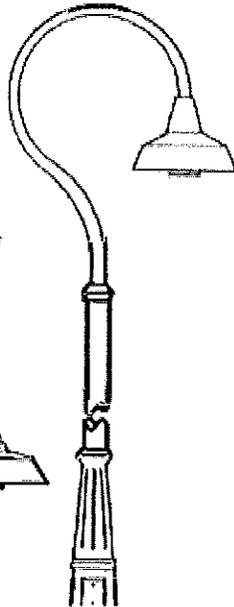


NEENAH FOUNDRY
 BOX 729 NEENAH, WI 54956
 (414) 725-7000

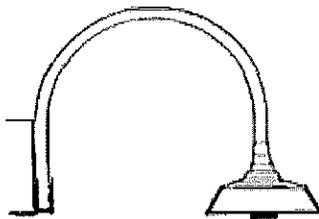
Tree Grates and Guards

Tree grates are necessary to give the young tree access to oxygen while still allowing the space to be available or pedestrian traffic on a congested sidewalk. Tree guards are necessary to protect the tree trunk from vandalism and from damage caused by bicycle locks.

Luminaire Size- 20 Dia.
 Diffuser - Holograph Lens
 Overall Height - 14'-0"
 Max. Wallage - 175 Watt Mercury Vapor



Luminaire Size 20" Dia
 Diffuser - Holograph Lens
 Overall Height - 36" x 64" Projection
 Max. Wallage - 175 Watt Mercury Vapor



WESTERN LIGHTING STANDARDS
 18060 Mt. Washington
 Fountain Valley, Ca. 92706
 (714) 549-0650

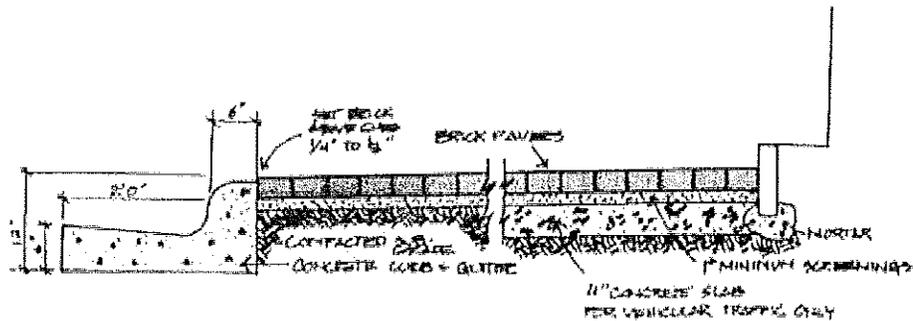
Pedestrian and Storefront Lighting

If used consistently throughout the district, this light standard can become a unifying design element along the street. The lights were chosen because they did not recall any particular historic style and were adapted both to contemporary architecture and traditional building styles.

Sidewalks

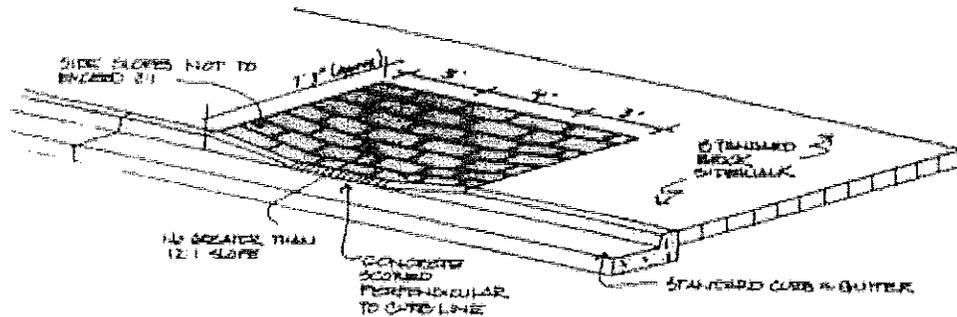
Sidewalk materials and design provide the connecting link between the diverse buildings and personalities of each business in University Village. If the Village is to develop its streetscape incrementally the standards used in each case must be the same.

The following construction details outline the standards for constructing sidewalks in University Village. More design information is available in the section of this document, which describes a typical storefront.



Sidewalk Construction

Brick specification:
WIRE-CUT CHEROKEE FLASH PAVERS or equivalent



Handicap Access Ramps

Important design considerations for handicap access ramps include the following:

SCORING AT CURB EDGE TO AID VISSUALLY HANDICAPPED IDENTIFY EDGE OF STREET

ADEQUATE DRAINAGE TO AVOID PONDING AT BOTTOM OF RAMP, AND SUBSEQUENT ICE HAZARD

AVOID STEEP SLOPES. SUDDEN "LUMPS" OR OTHER POSSIBLE HAZARDS TO THE NON-HANDICAPPED AS Well AS THE HANDICAPPED

Street Trees

This area of Hillsborough Street is a difficult place for trees to grow. The soil is compacted by heavy foot traffic. It is hot and dry, and the trees are subject to other abuses such as vandalism and bicycles being locked to tree trunks.

Because of these difficult conditions, it is necessary to provide trees with a good head start, and use species which are well adapted to these conditions.

Street trees reduce the impact of the southern exposure of the sidewalk area, providing shade, and thus a place to browse with less discomfort, promoting more pedestrian traffic and encouraging those pedestrians to stay longer.

Tree canopies also serve the same function as awnings and overhangs. They reduce the scale of buildings and the street to a "people-size" pathway along the sidewalk.

Recommended Species

In order to create a consistent visual image from Hillsborough Street, a particular species of tree is to be used ALONG HILLSBOURGH STREET:

CARPINUS BETULUS European Hornbeam

This tree is well adapted to urban conditions, and is commonly used in Europe. It has a very regular shape, which makes it particularly attractive along thoroughfares. European Hornbeam is available from these nurseries:

Princeton Nurseries

P. O. Box 191

Princeton, NJ, 08540

(609) 924-1776

Pleasant Cove Nursery

Rt.3

Rock Island, TN 38581

(615) 686-2215

Other species are available to be used in special circumstances. These trees are recommended for SIDE STREETS AND COURTYARDS:

LAGERSTROEMIA INDICA Crape Myrtle (deciduous)

ILEX OPACA "Savannah" Savannah Holly (evergreen)

BETULA NIGRA River Birch (deciduous)

All of these are available in local nurseries

Maintenance

Perhaps the most critical part of any streetscape plan is the design of a maintenance plan for the improvements. If allowed to deteriorate, all the investment in such a major change in the University Village area will be discounted. Traditionally, in Raleigh, the adjacent landowner is responsible for any improvements which are required in the sidewalk area. Since the maintenance of street trees and other elements is more complex in such a densely-developed street and sidewalk area, the outline for the maintenance responsibility of each streetscape element is defined here.

Streetscape Elements to be Maintained by the CITY

STREET TREES
TREE GRATES
TREE GUARDS
CURB AND GUTTER
STREET PAVING
CROSSWAIK STRIPING AND SIGNALS
BUS STOPS
ON-STREET PARKING SPACE STRIPING
PARKING METERS
TRASH RECEPTACLE

Streetscape Elements to be Maintained by the LANDOWNER

SIDEWALKS
ANY BUILDING IMPROVEMENTS
AWNINGS
BENCHES
BUSINESS SIGNS
LIGHTING

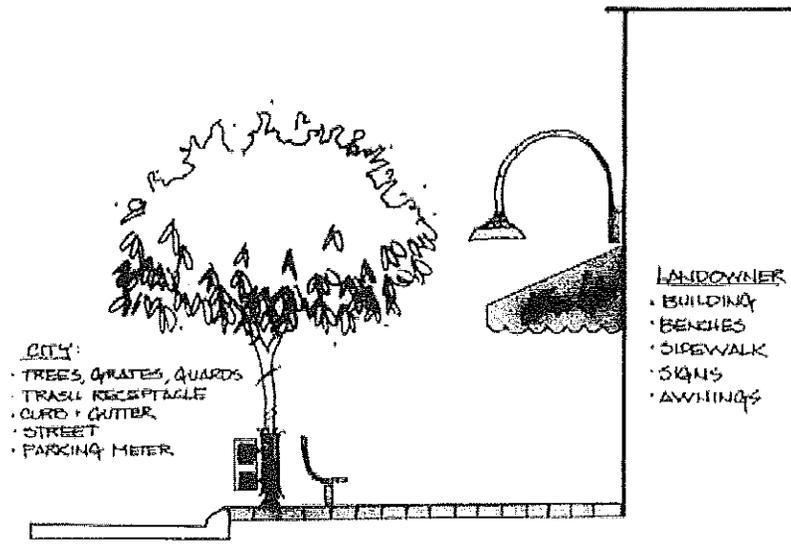


FIGURE 1. MAINTENANCE RESPONSIBILITY

Utility Lines

At present utility lines and poles play a significant role in defining the character of the streetscape in University Village. Because there are few street trees, they dominate the edge of the street and are quite visible, both from the automobile, and from the pedestrian vantage point. The number of poles and proliferation of wires detracts from the streetscape by appearing haphazard and works against the existing structure of spaces on the sidewalk and the street.

The intent of this plan is to reduce the impact of utility lines on the character of University Village and encourage the establishment of other defining elements such as street trees and overhead awnings or balconies along the sidewalk.

The solution to this problem will require the cooperation of the various utility companies, and City departments, such as the Transportation Department, which require poles and wiring in the street area.

Short-term Plan

Over the next two years, the City and the various utility companies should cooperate to reduce the number of poles, straighten them, consolidate wires whenever possible, and raise the level of the lowest wires to 20 feet. Zig-zag wiring across the street should be minimized or eliminated, if possible.

Long-term Plan

The University Village area should be designated a priority area, and a proposal should be developed to remove the overhead lines from the street.



FIGURE 1. EXISTING STREETScape WITH UTILITY LINES AND POLES



FIGURE 2. STREETScape WITH CONSOLIDATING AND STRAIGHTENING OF LINES AND POLES



FIGURE 3. STREETScape WITH REMOVAL OF UTILITY LINES AND POLES

Signs

The public face of the district, its personality, is often a characteristic of its signs. They tell the story of the products available, the atmosphere of the shops and stores, the quality and attractiveness of the area as a whole.

In University Village, the key characteristics to be presented to the patron are: High Quality, Lively Pedestrian Atmosphere, Wide Variety of Products.

Large, garish signs with many contrasting colors are characteristic of "strip" commercial areas along highways, and usually create a "discount" image, and bring to mind large chain stores where a little bit of quality is sacrificed for a low price. As more and more are erected, the competition from so many signs causes more and larger ones to be built, at greater expense.

In University Village, pedestrians make up at least 50% of most businesses' patrons. The district has a densely developed character which makes many large signs inappropriate. The emphasis of the plan should be to encourage smaller high-quality signs which are primarily oriented to the pedestrian, and are integrated with the building and its elements to attract customers, and present a quality image.

The following signs are ENCOURAGED in University Village:

BACK-LIT AWNINGS using translucent materials
WINDOW STENCILS
NEON LETTERING on the inside of windows
EXTERNALLY-LIT PROJECTING SIGNS
INTERNALLY LIT PARKING LOCATION SIGNS no greater than 10 sq. ft.
EXPOSED NEON no greater than 10 sq. ft.
EXTERNALLY -LIT WALL SIGNS

The following signs are PROHIBITED in University Village:

INTERNALLY-LIT WALL SIGNS (except back-lit awnings) greater than 10sq.ft.
GROUND SIGNS
FREE-STANDING SIGNS
OUTDOOR ADVERTISING SIGNS

Use of Neon

Neon can be either wonderfully attractive, or obnoxious, especially if it is overused. Any use of neon on the exterior of the building should be very subdued, and used only to accent a sign or provide external lighting to other types of copy. Neon in windows is encouraged (again in moderation) and may be used for text.

Back-Lit Awnings

Back-lit awnings, made of translucent materials can be a successful streetscape element, as well as a versatile advertising, and shading device for individual businesses. This type of awning is readable in the day, is available in a variety of colors, and at night becomes not only a pedestrian amenity, but provides the business the opportunity to have an advertising element which is visible from the street, without erecting less-attractive and expensive internally-lit free-standing or projecting signs, which detract from the overall appearance of the area.

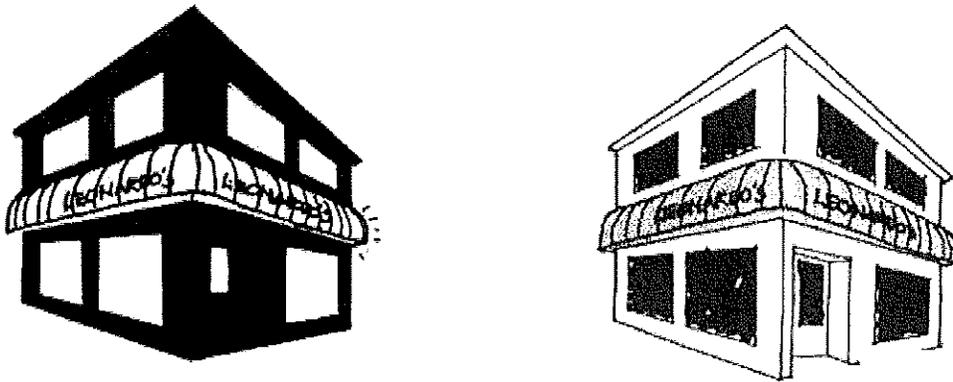


FIGURE 1. BACK LIT AWNINGS. These can serve a double purpose as visible, attractive amenities both day and night.

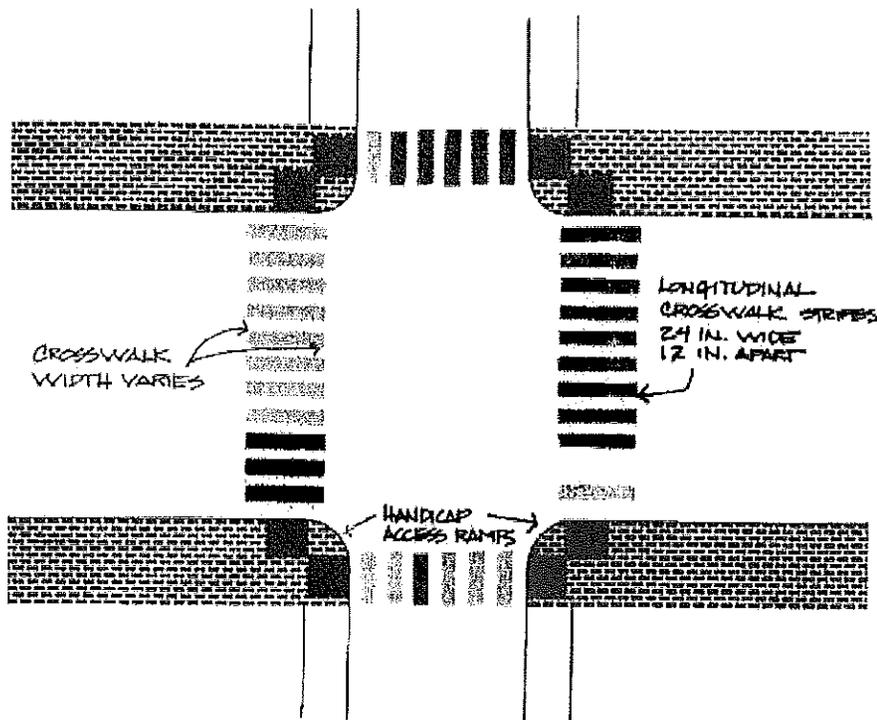
Crosswalks

Pedestrian Crosswalks in this high-traffic area must be more visible than other crosswalks in less-well-traveled districts. Since Hillsborough Street is a thoroughfare, the volume of both pedestrian and vehicular traffic is very high.

Bold striping patterns are necessary to help clearly define the pedestrian pathways. The preferred pattern for this district is the boldest striping pattern recommended by the Department of Transportation for high-visibility.

Crosswalks should exist connecting all four corners of the intersection, and each corner should be handicap-accessible.

FIGURE 1. TYPICAL CROSSWALK LAYOUT AND STRIPING



Parking

Parking has traditionally been a difficult issue in this area. Since the district has grown primarily as a pedestrian area, provision of off-street parking has often been waived. Increasing demand for student commuter parking and patron parking for those who do not walk to University Village businesses has conflicted with the lack of available land on which to build such parking.

Cooperation with the University is necessary in order to solve this problem. Since the results of a Planning Department survey show approximately 50% of the business patrons walk to the stores, a reduction in off-street parking requirements seems logical. This should be combined with the provision of public parking in the district to make up for the lack of available land to serve the present deficit, and to remove some of the pressure on the University Park neighborhood to provide overflow parking.

At the same time, effort by the University to increase the availability of student commuter parking in the district is essential. Since the classroom areas are primarily on the North Campus side, demand for convenient parking nearby is heavy.

Parking Plan Recommendations:

- Reduce off-street parking requirements to 55% of the current level, for commercial uses only.
- NCSU should build a student commuter deck at Friendly Drive on the west end of North Campus
- Public parking lots should be provided, in cooperation with the University, on a by-the-hour basis. Possible locations of such lots are:
 - Hillsborough Square at Ferndell and Hillsborough
 - Brooks Lot at Brooks and Hillsborough
 - North Hall Lot at Logan and Hillsborough
- A public parking structure should be considered at North Hall. Such a lot should include retail store frontage on Hillsborough Street.

Parking Design

-For new buildings, all parking lots should be located to the rear of the property, off of Hillsborough Street. Access points should be limited in both number and size to minimize conflicts between pedestrian and vehicles. Handicap and compact spaces shall be provided as per current city requirements.

-Any freestanding lots shall be landscaped to screen both the street and the neighboring properties.

-Benches and street trees shall be provided along the sidewalk for freestanding lots.

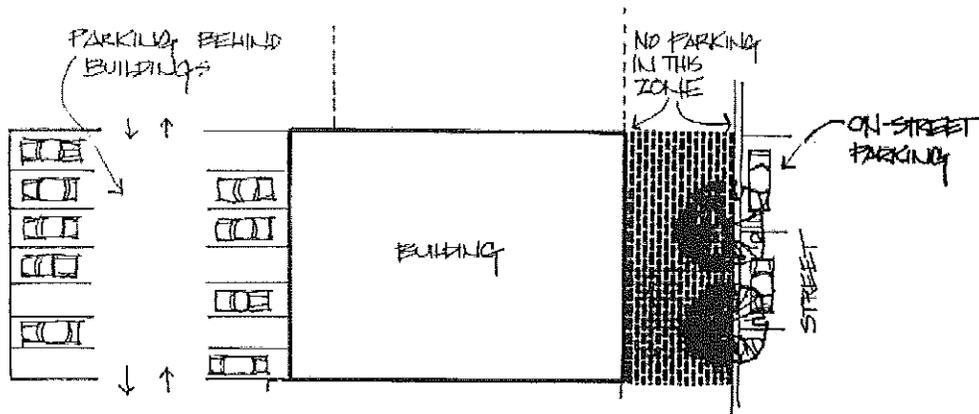


FIGURE 1. RECOMMENDED ON-SITE PARKING LOCATION

Awnings

Awnings serve many purposes. They can act as a sign, or provide shade to reduce heat gain in a building. Awnings also provide an overhead cover to reduce the scale of the sidewalk area to "people-size". All of these characteristics contribute to the utility of the awning as a part of the streetscape in an urban area.

In University Village, awnings or overhead balconies are encouraged to provide a window-shopping space on the sidewalk. These awnings must be made of a durable material, and attached with a permanent structure to the building.

Acceptable Awning Materials

CANVAS
VINYL
TRANSLUCENT MATERIALS FOR BACK-LIT AWNINGS

Prohibited Materials

METAL unless covered by any other acceptable material

Height Above Sidewalk

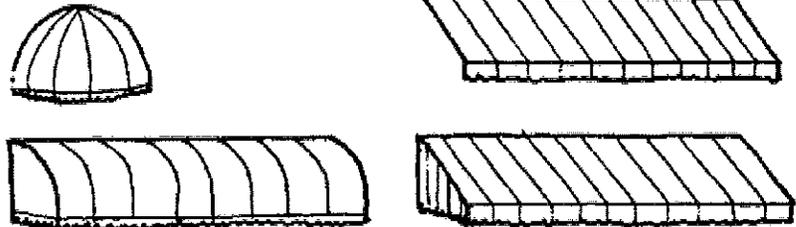
7 FEET to valence (minimum)
8 FEET to structure (minimum)
(no poles)

Colors

ALL COLORS are allowed
BRIGHT COLORS tend to fade quickly, and should be used assuming greater maintenance and earlier replacement

FIGURE 1. RECCOMENDED AWNING TYPES

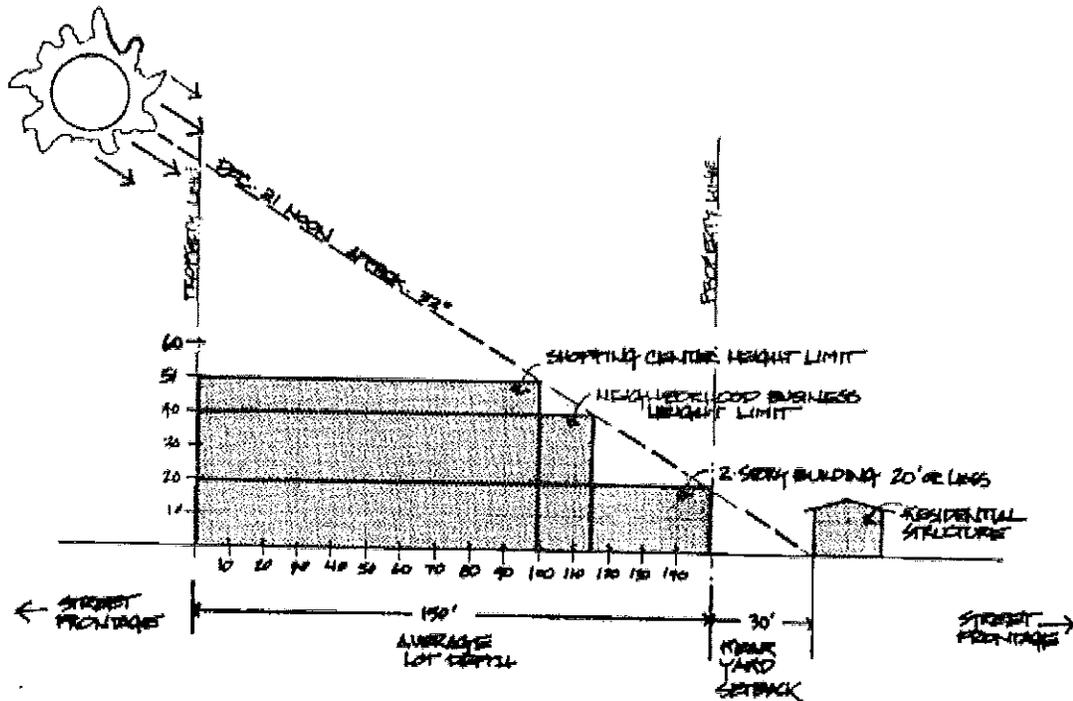
Note. May be back-lit or retractable, in addition to standard fixed awning



Solar Access / Building Height

Commercial buildings fronting on Hillsborough Street should not prevent adjacent residential properties from gaining access to sunshine.

No part of any building should be built higher than a line drawn at a 32 degree angle (the approximate sun angle at noon on the winter solstice), starting at the rear yard setback line for adjacent residential properties (usually 30 feet) and crossing the entire commercial property, as illustrated below:

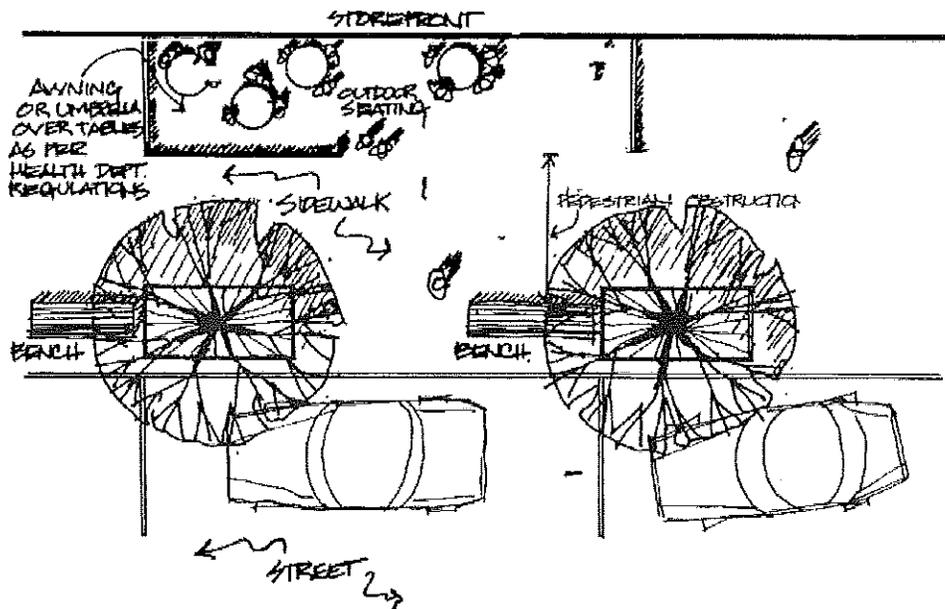


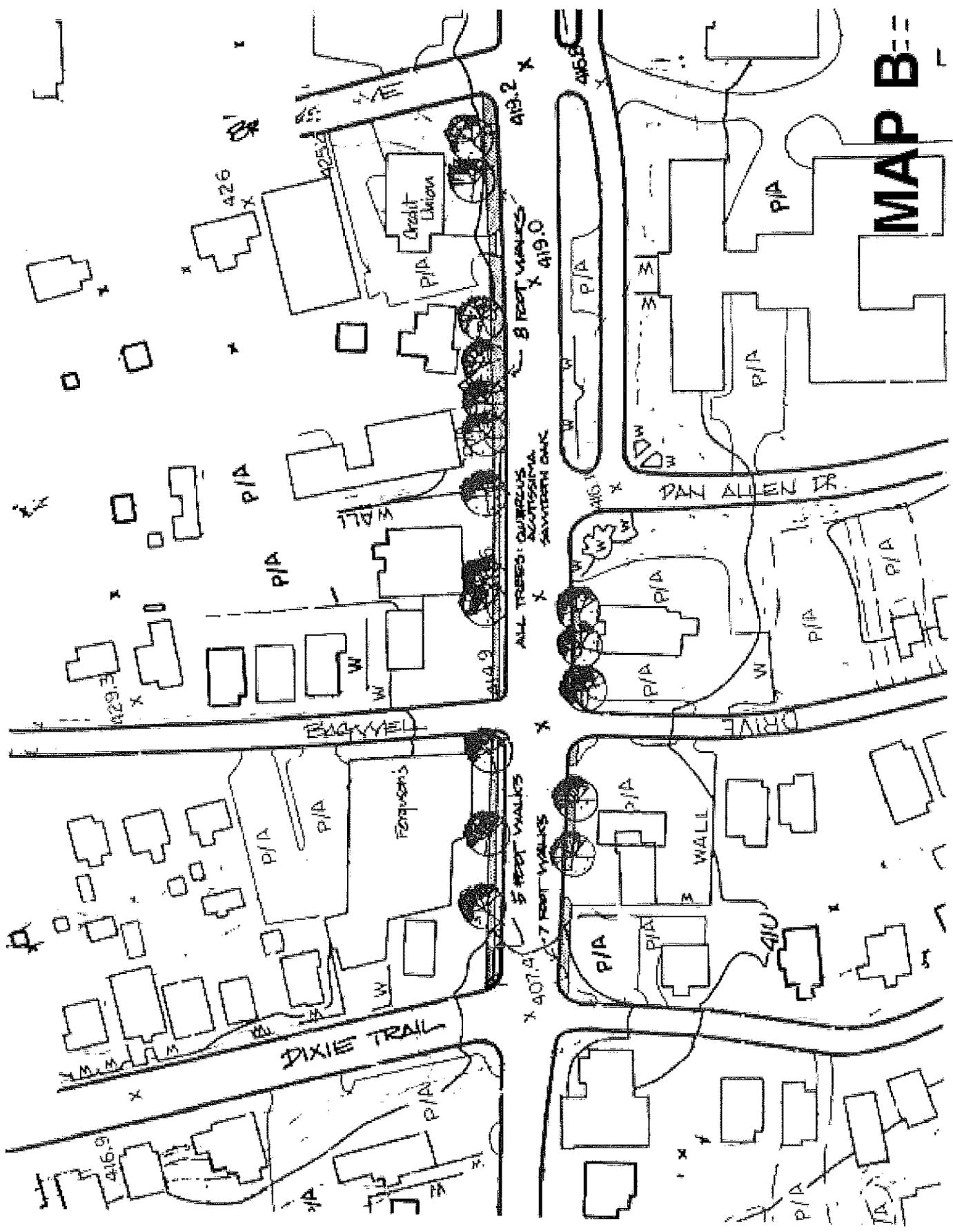
Sidewalk Café Seating

Encroachments on the public right-of-way for restaurant seating are allowed (after approval by the City Council), provided all Health Department regulations are met, and a clear passage of 9 1/2 feet is maintained between the required barrier or wall of the seating area, and the nearest vertical pedestrian obstruction.

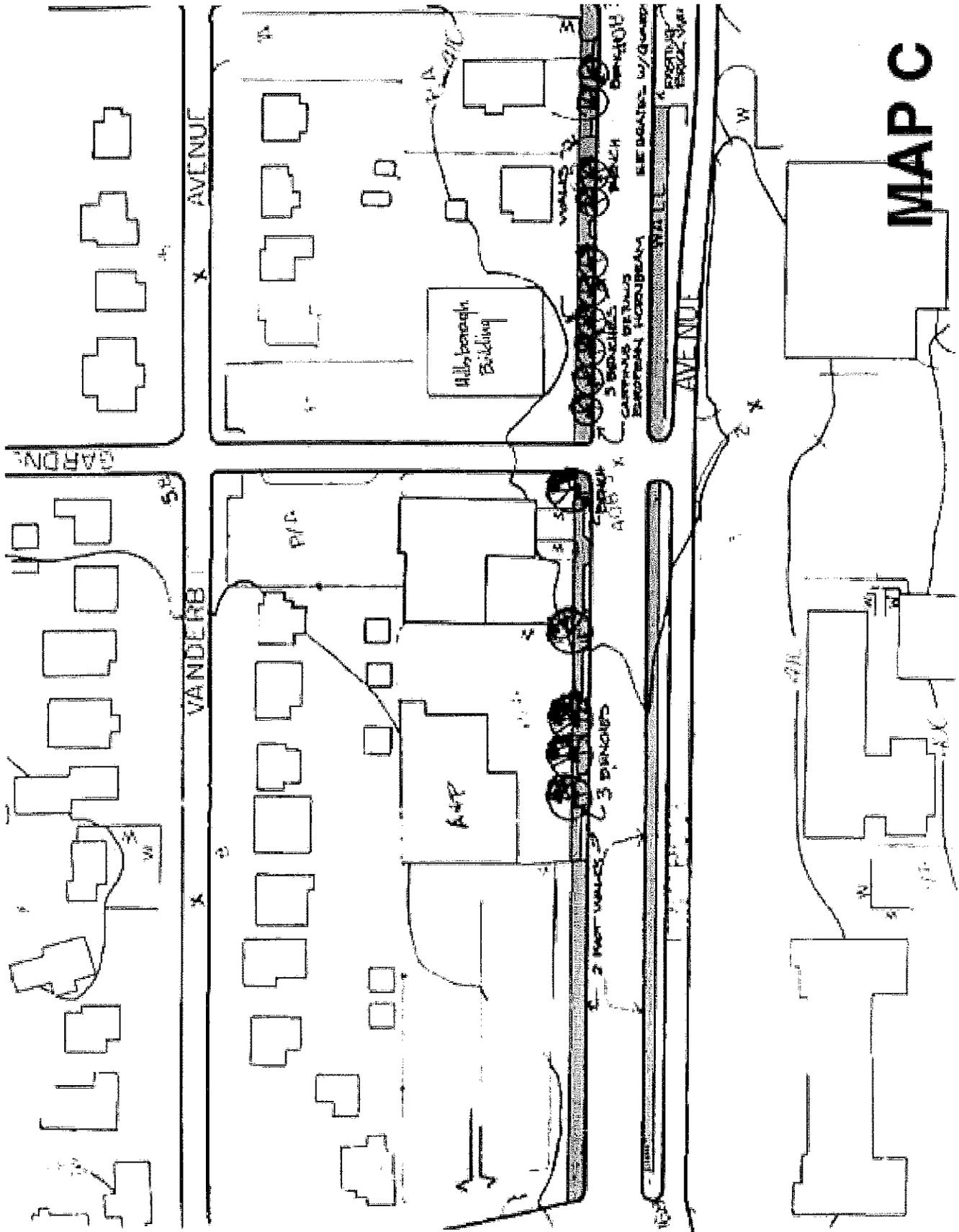
Pedestrian obstructions can be a bench, a sign post, a parking meter, a tree trunk or any other such item that would impede the flow of pedestrian traffic. Tree grates should not be considered pedestrian obstructions, but unprotected planting areas beneath trees or shrubs should be considered an obstruction.

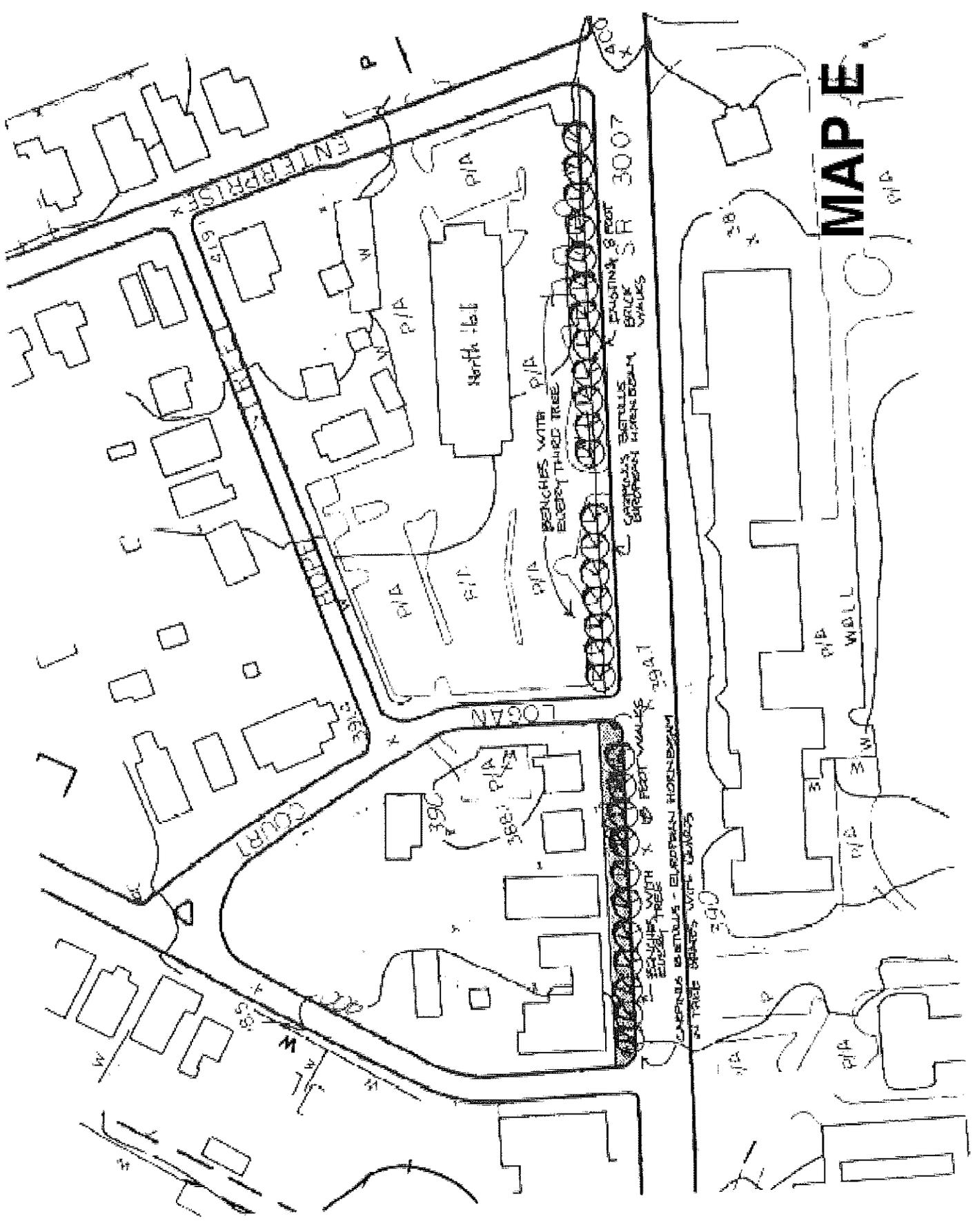
No barrier or wall should be permanently attached to the sidewalk.





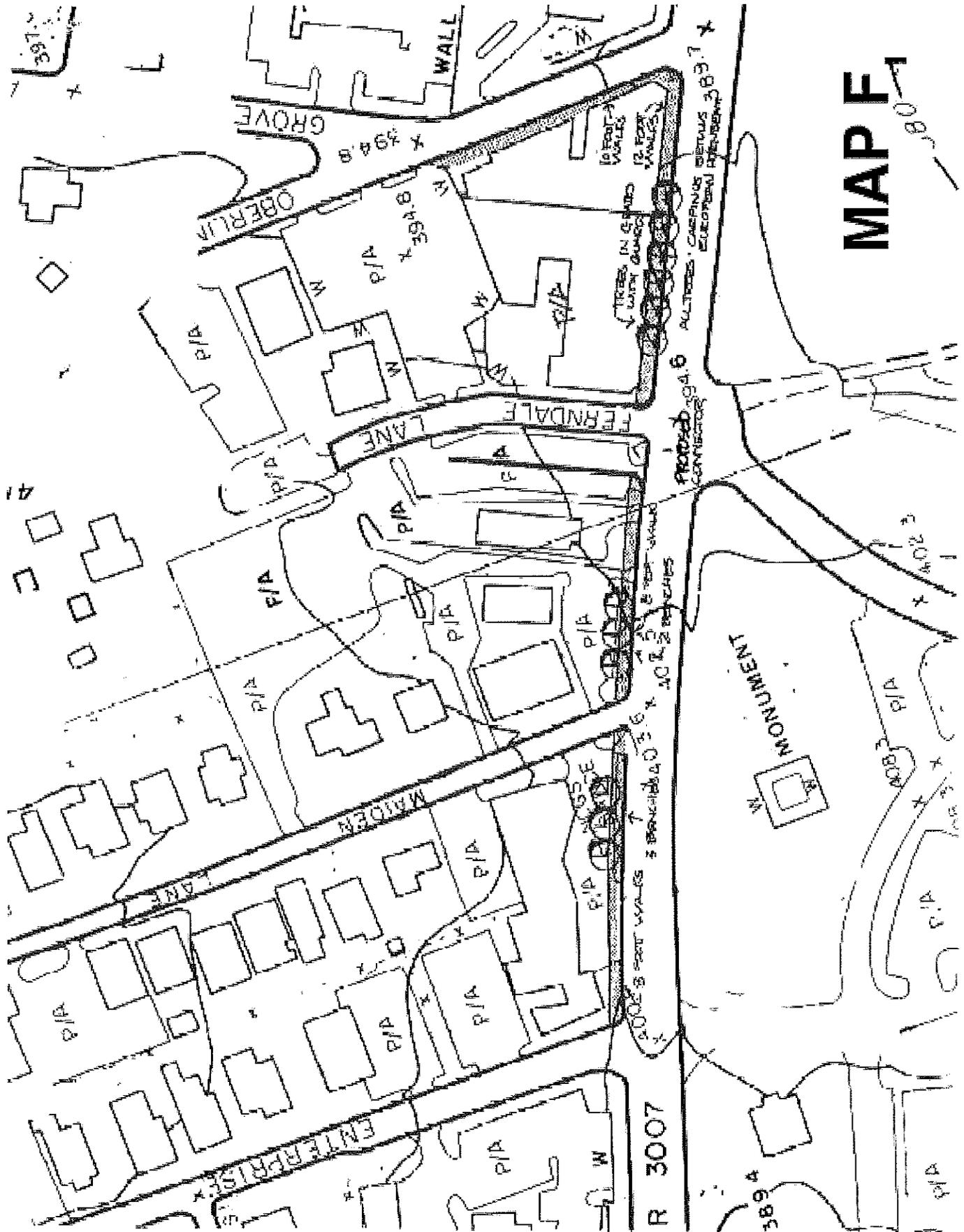
MAP B





MAP E

W/D



MAP F

SSP.1.03
Z-48-03

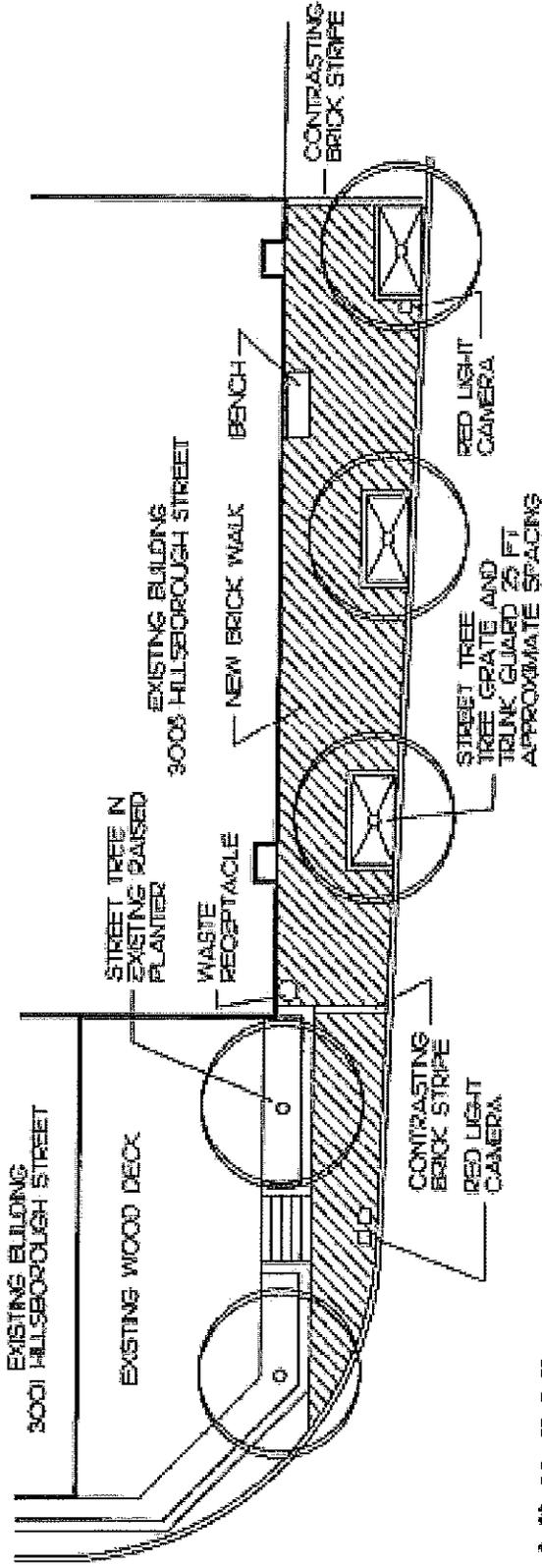
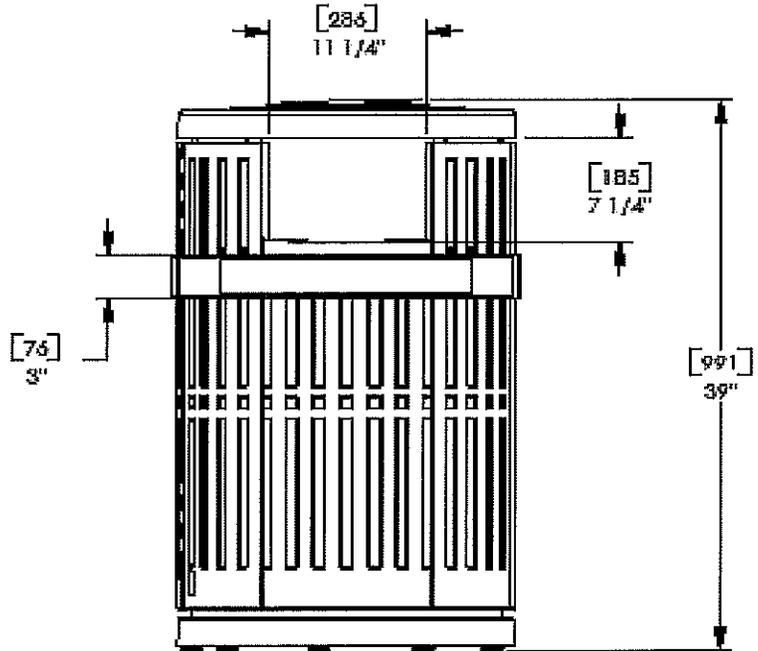
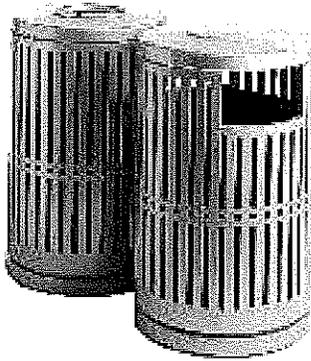


Exhibit "A"
Streetscape Plan for University Village on Hillsborough Street
March 1, 2004

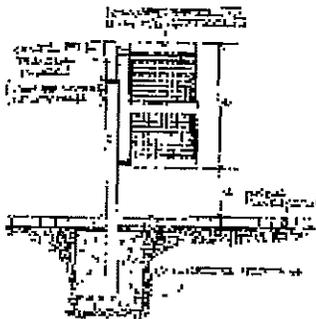
City of Durham, NC. Map 03.2004.1000A.DT

Proposed Trash Receptacle



Manufacturer: Landscape Forms
 Color: Anodized Finish
 Style: Chase Park
 Description: 24in Diameter / 36 Gallon Side
 Opening Litter
 Notes: 61% Recycled Material
 100% Recyclable

Trash Receptacle - University Village Streetscape Plan



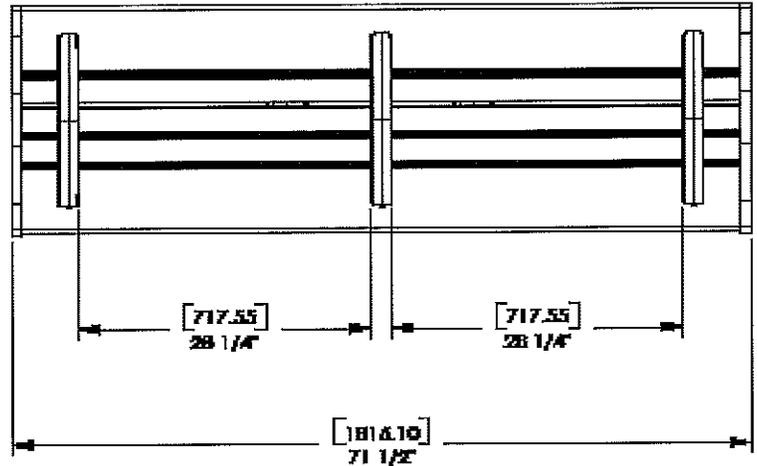
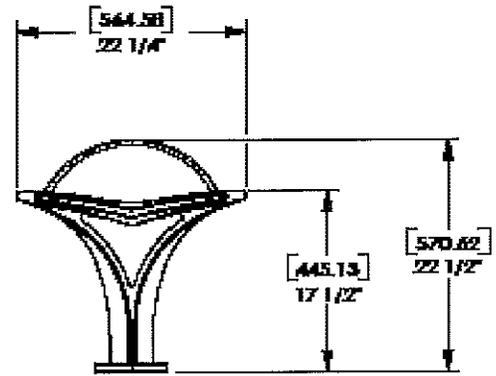
Existing Streetscape Plan Trash Receptacle
Howard Products

This design is simple, easy to empty, and similar to the already-common receptacles on the University Campus.

This contemporary style was selected because it is designed and built to meet the rigors of urban spaces. This design features a hinged door that swings open for easy trash removal. The closed top was requested by both the parks and recreation department and the maintenance staff to keep rain water out of the trash receptacle. This product is made from 61% recycled material and is 100% recyclable. Landscape Forms powder coat finish contains no heavy metals and has extremely low VOCs.

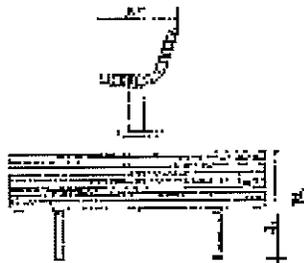


Proposed Bench



Manufacturer: Landscape Forms
 Color: Anodized Finish
 Style: Austin
 Description: Flat Bench - Cantilever -
 2 End Arms / 1 Center Arm
 Notes: 48% Recycled Material
 100% Recyclable

Bench - University Village Streetscape Plan



Existing Streetscape Plan Bench
 Woodplay, Inc.

This type of bench was selected because it is simple, yet attractive, durable, and commonly available from a variety of manufacturers in this style.

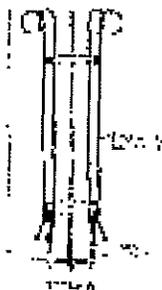
This design was selected because of its balance of contemporary style and simplicity. This product is made from 48% recycled material and is 100% recyclable. Landscape Forms powder coat finish contains no heavy metals and is a hard yet flexible finish that resists rusting, chipping, peeling and fading.



Proposed Tree Pits



Tree Grates and Guards - University Village Streetscape Plan



Existing Tree Grates and Guards
Neenah Foundry

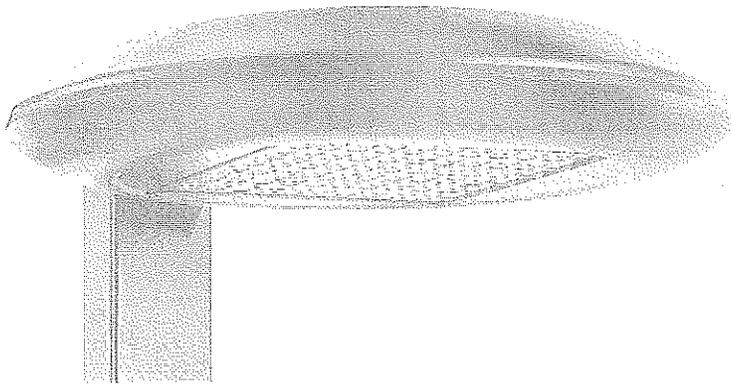
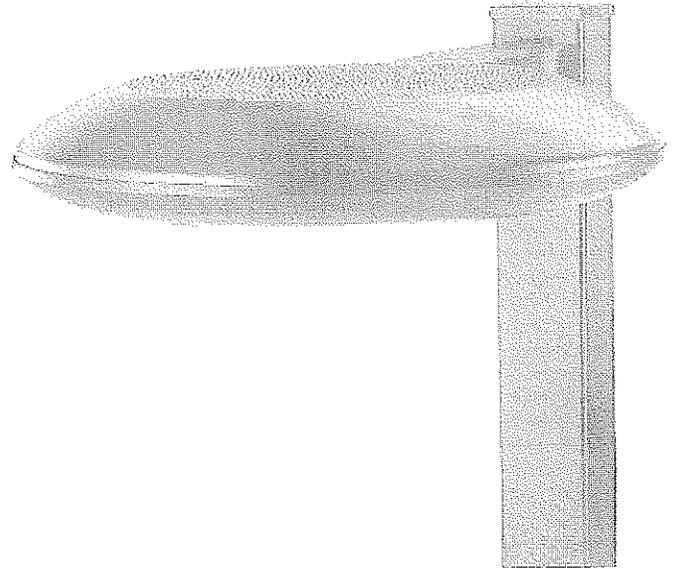
Tree grates are necessary to give the young tree access to oxygen while still allowing the space to be available for pedestrian traffic on a congested sidewalk. Three guards are necessary to protect the tree trunk from vandalism and from damage caused by bicycle locks.

An open tree pit allows for seasonal color and groundcover plantings. They also provide a better environment for the tree in the harsh urban environment. The brick edging prevents chemicals, salts and other toxins from flowing into the tree pit. The inspiration comes from the Centennial Campus.

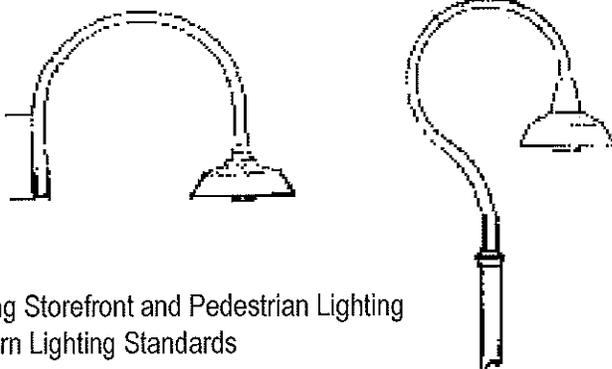


Proposed Pedestrian and Street Lighting

Manufacturer: Beta Lighting
 Color: Silver Bronze
 Style: The Edge Round Luminaire
 Description: LED Light



Pedestrian and Storefront Lighting - University Village Streetscape Plan



Existing Storefront and Pedestrian Lighting
 Western Lighting Standards

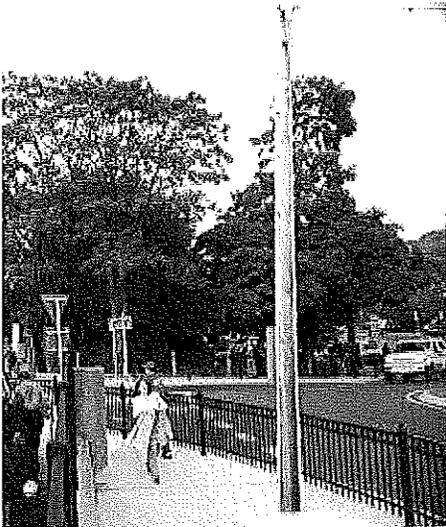
If used consistently throughout the district, this light standard can become a unifying design element along the street. The lights were chosen because they did not recall any particular historic style, and were adapted both to contemporary architecture and traditional building styles.

This contemporary pole and fixture was selected by the City because the linear arm is manufactured of wall extruded aluminum and offers a more contemporary transition between luminaire and pole. The simple elegance allows it to transform appearance while blending seamlessly into the architectural environment.

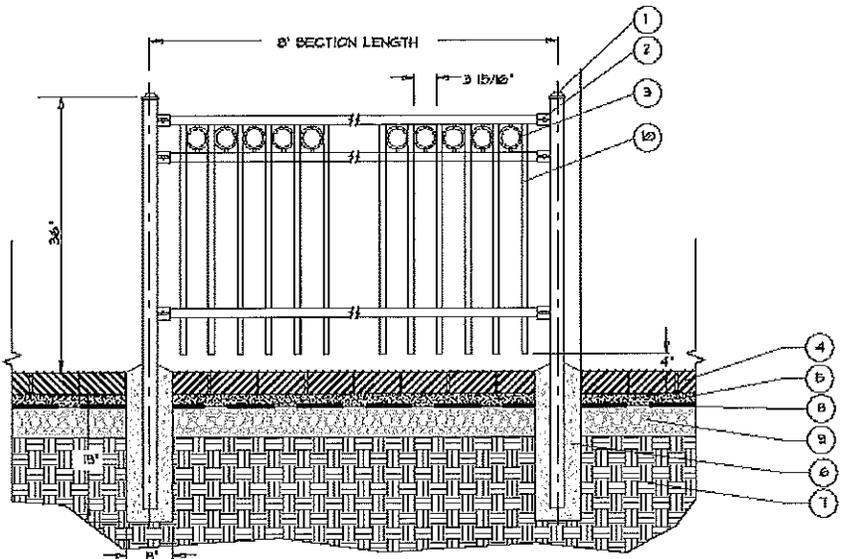
*Note: There is still no resolution by City Staff on the use of LED lights as a substitute for these fixtures. This decision is pending further analysis by City Staff.



Ornamental Pedestrian Fence



Pictured: 4' High Pedestrian Fence
 *3' High Proposed for Streetscape Plan



- ① FLAT POST CAP
- ② RAIL BRACKET
- ③ RINGS
- ④ BRICK PAVER WALK
- ⑤ SAND SETTING BED
- ⑥ CONCRETE FOOTER - CONCRETE TO BE 3000 PSI
- ⑦ COMPACTED SUBGRADE
- ⑧ GEOTEXTILE FABRIC
- ⑨ 4" STONE BASE
- ⑩ PICKETS: 1" SQUARE RAIL @ 1 5/8" SQUARE POST @ 2 1/2" SQUARE

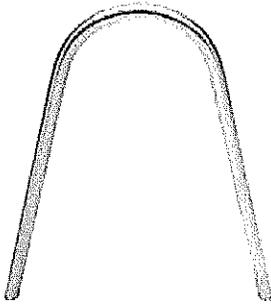
Urban Screen - University Village Streetscape Plan

The current University Village Streetscape does not address urban screens.

This design was selected because the urban fence provides a contemporary screen of unwanted views and creates a physical barrier between vehicular and pedestrian spaces where there is no room for landscape screening.



Proposed Bike Rack



Manufacturer: Landscape Forms
Color: Stainless Steel
Style: Flo Bike Rack
Notes: 91% Recycled Material
100% Recyclable

This design was selected because of its contemporary style and simplicity. This product is made of 91% recycled material and is 100% recyclable. This design is similar in character to the existing bike racks on Hillsborough Street.

Bike Rack - University Village Streetscape Plan

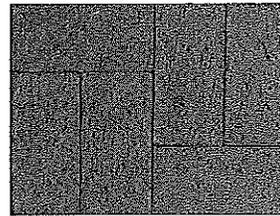
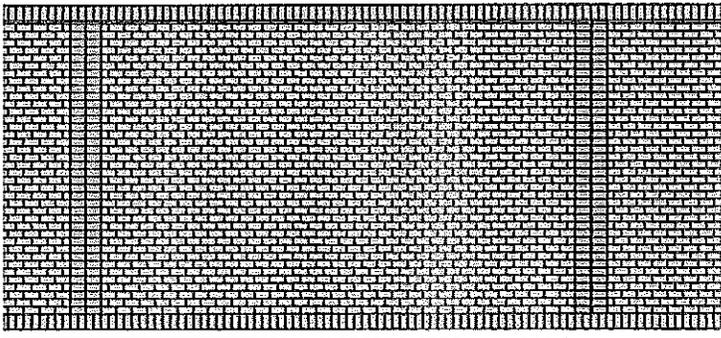


The current University Village Streetscape does not address bike racks.

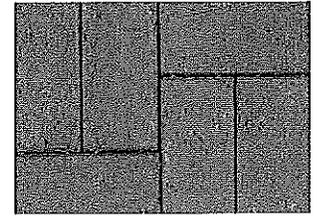
This contemporary design was selected because of its simple arching form that doubles as a bike rack and a tree guard. The X-Type can accommodate all bicycle types and offers bike parking on three sides of a tree pit. Using this design will add more open space on the sidewalk.



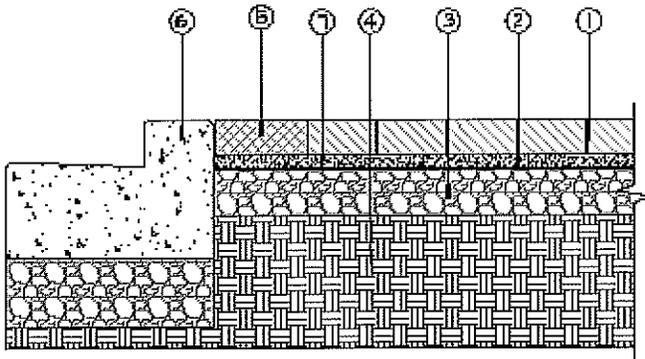
Proposed Sidewalk Construction



FIELD PAVER
Pine Hall Brick
Brick specification:
PATHWAY RED PAVER

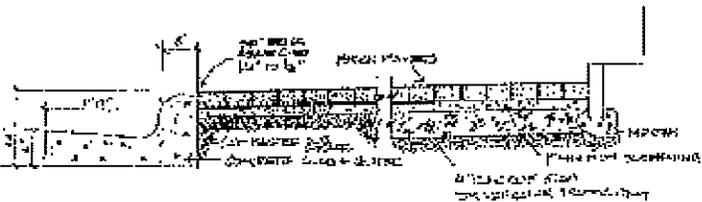


ACCENT PAVER
Pine Hall Brick
Brick specification:
**ENGLISH EDGE
DARK ACCENT PAVER**



- ① 3 1/2" X 1 1/2" X 2 1/2" PEDESTRIAN RATED BRICK PAVER FIELD - RUNNING BOND, PATHWAY RED
- ② 2" SAND SETTING BED
- ③ 4" COMPACTED STONE BASE
- ④ COMPACTED SUBGRADE
- ⑤ CONTINUOUS BRICK PAVER ACCENT BAND 3 1/2" X 1 1/2" X 2 1/2" BRICK PAVER BAND - DOUBLE SOLDIER COURSE, ENGLISH EDGE DARK ACCENT (REF SPEC6)
- ⑥ CONCRETE CURB AND GUTTER (REF ROADWAY PLANS)
- ⑦ GEOTEXTILE FABRIC - (REF SPEC6)

Sidewalk Construction - University Village Streetscape Plan



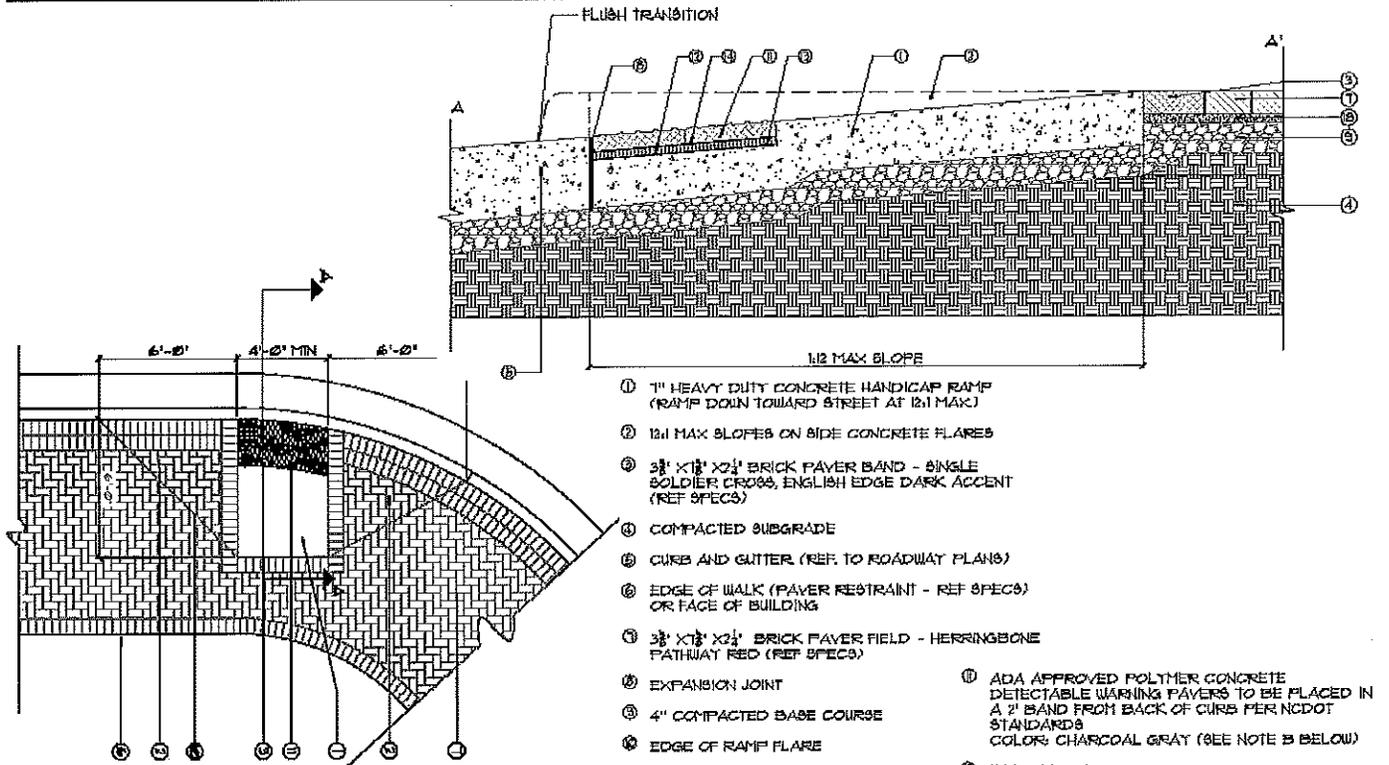
Existing Sidewalk Construction
Pine Hall Brick

Brick specification:
WIRE-CUT CHEROKEE FLASH PAVERS, or equivalent

This design was selected because it closely resembles the current University Village Streetscape Plan. This design provides a connection between the north and south sides of the street. The selected Pathway Red paver is a match for the paver chosen for the University Village Streetscape Plan which is no longer available.

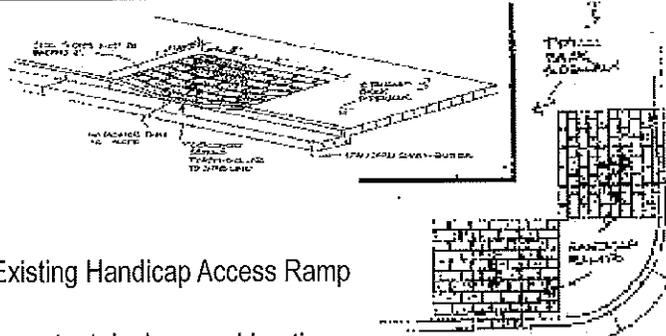


Proposed Handicap Access Ramps



- ① 1" HEAVY DUTY CONCRETE HANDICAP RAMP (RAMP DOWN TOWARD STREET AT 1:12 MAX)
- ② 1:12 MAX SLOPES ON SIDE CONCRETE FLARES
- ③ 3 3/8" X 1 1/2" X 2 1/4" BRICK PAVEMENT BAND - SINGLE SOLDIER CROSS, ENGLISH EDGE DARK ACCENT (REF SPECS)
- ④ COMPACTED SUBGRADE
- ⑤ CURB AND GUTTER (REF TO ROADWAY PLANS)
- ⑥ EDGE OF WALK (PAVER RESTRAINT - REF SPECS) OR FACE OF BUILDING
- ⑦ 3 3/8" X 1 1/2" X 2 1/4" BRICK PAVEMENT FIELD - HERRINGBONE PATHWAY RED (REF SPECS)
- ⑧ EXPANSION JOINT
- ⑨ 4" COMPACTED BASE COURSE
- ⑩ EDGE OF RAMP FLARE
- ⑪ ADA APPROVED POLYMER CONCRETE DETECTABLE WARNING PAVERS TO BE PLACED IN A 2' BAND FROM BACK OF CURB PER NCDOT STANDARDS COLOR: CHARCOAL GRAY (SEE NOTE B BELOW)
- ⑫ 1" MORTAR BED
- ⑬ MORTAR BED BOND COAT
- ⑭ BOND COAT
- ⑮ 3 3/8" X 1 1/2" X 2 1/4" BRICK PAVEMENT BAND - DOUBLE SOLDIER COURSE, ENGLISH EDGE DARK ACCENT (REF SPECS)
- ⑯ EXISTING CONCRETE SIDEWALK
- ⑰ STANDARD PAVEMENT WALKWAY, REF. DET. B, THIS SHEET
- ⑱ 2" SAND SETTING BED
- ⑲ GEOTEXTILE FABRIC - (REF SPECS)

Handicap Access Ramps - University Village Streetscape Plan



Existing Handicap Access Ramp

- Important design considerations for handicap access ramps include the following:
- Scoring at curb edge to aid visually handicapped identify edge of street
 - Adequate drainage to avoid ponding at bottom of ramp, and subsequent ice hazard
 - Avoid steep slopes, sudden "lumps" or other possible hazards to the non-handicapped as well as the handicapped

This design was selected to meet the current NCDOT standards for ADA accessibility. The ADA approved polymer concrete detectable warning tiles aid visually handicapped identify edge of street.

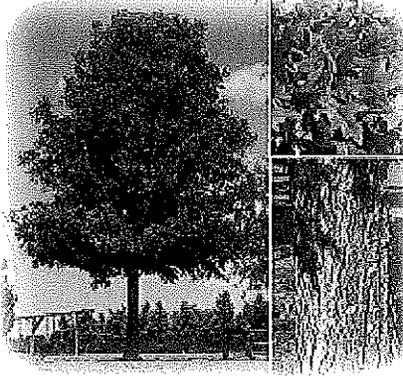


Proposed Recommended Species



PANACHE SHUMARD OAK
(*Quercus shumardii* 'QSFTC')

Location: Hillsborough Street
Shape: Rounded
Foliage: Lustrous Dark Green
Fall Color: Red



HIGHBEAM OVERCUP OAK
(*Quercus lyrata* "QSFTC")

Location: Hillsborough Street
Shape: Uniform, Dense,
Dominant Leader
Foliage: Dark Green
Fall Color: Yellow-Orange



'VALYNOR' TRIDENT MAPLE
(*Acer buergerianum* 'Valynor')

Location: Hillsborough Street
Shape: Uniform, tight upright
Foliage: Dark Green
Fall Color: Red

Recommended Species - University Village Streetscape Plan

In order to create a consistent visual image from Hillsborough Street, a particular species of street tree is to be used along HILLSBOROUGH STREET:

PINUS BETULUS - European Hornbeam

This tree is well adapted to urban conditions, and is commonly used in Europe. It has a very regular shape, which makes it particularly attractive along thoroughfares.

Other species are available to be used in special circumstances. These two trees are recommended for SIDE STREETS AND COURTYARDS:

LAGERSTROEMIA INDICA - Crape Myrtle (deciduous)

ILEX OPACA "Savannah" - Savannah Holly (evergreen)

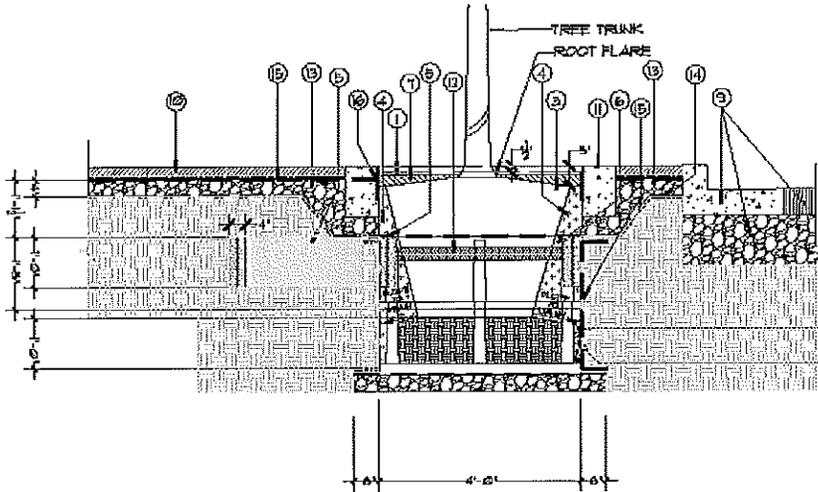
BETULA NIGRA - River Birch (deciduous)

All of these are available in local nurseries

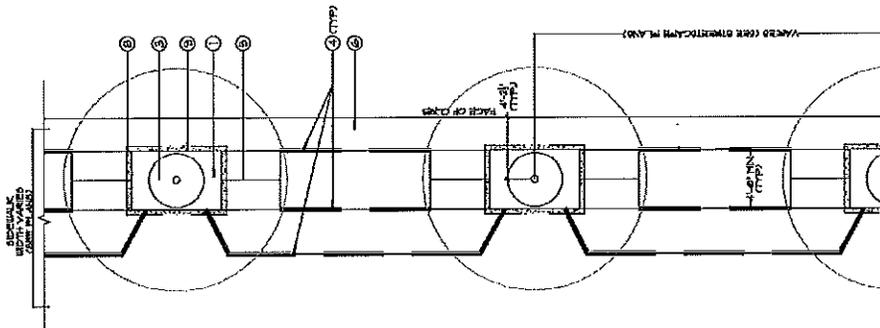
These species were selected in collaboration with the City Urban Forester and the Parks and Recreation Staff for their shape, size, and adaptation to urban settings. These trees will provide much needed shade along Hillsborough Street and will provide variety and help to unify the two sides of the street.



Proposed Tree Planting Details

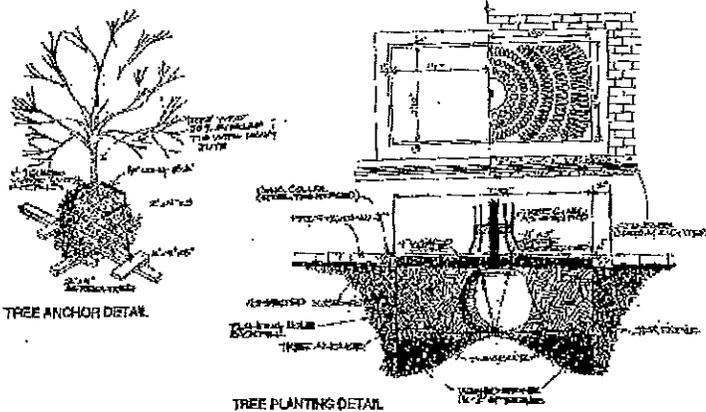


- ① TREE PIT OPENING
- ② CRUSHED STONE DRAIN BUMP WITH FILTER FABRIC WRAP
- ③ TREE ROOTBALL, REMOVE TOP 1/3 OF WIRE BASKET OR ROOTBALL STRAPS, REMOVE TOP 1/2 OF BURLAP ROOTBALL COVERINGS, REMOVE ALL SYNTHETIC STRAP MATERIAL AND COVERINGS FROM ENTIRE ROOTBALL. SET TOP SURFACE ELEVATION OF ROOTBALL FLUSH WITH FINAL EXISTING GRADE.
- ④ BACKFILL WITH PLANTING SOIL (PER SPECIFICATIONS)
- ⑤ TREE PIT ROOT PATH AERATION SHEET IN TRENCH, BACKFILL TRENCH W/SPECIFIC SOILS EXTEND IN-LINE TO NEXT ADJACENT TREE. REF. SHEET LD-02 DET. A FOR PATH LAYOUT.
- ⑥ DEEP ROOT SILVA CELL, WITH 3" OF COMPOST BETWEEN SILVA CELL DECK AND PLANTING SOIL. BACKFILL WITH SPECIFIED SOILS. REF. SHEET LD-02 DET. A FOR LAYOUT.
- ⑦ 2" MULCH AS SPECIFIED (DO NOT MULCH WITHIN 6" OF TREE TRUNK)
- ⑧ 4" DRAIN PIPE (REF ROADWAY PLANS FOR CONNECTION)
- ⑨ CONCRETE CURB, GUTTER, ROADWAY AND BASE (SEE ROADWAY PLANS)
- ⑩ BRICK PAVEMENT WALKWAY (SEE STREETSCAPE PLANS)
- ⑪ 4" x 8" FLUSH CONCRETE EDGE
- ⑫ 3" COMPOST BETWEEN SILVA CELL AND PLANTING SOIL
- ⑬ AGGREGATE BASE COURSE
- ⑭ 24" WIDE GEOTEXTILE, 18" MINIMUM OVERLAP PAST EXCAVATION
- ⑮ GEOTEXTILE 18" MINIMUM BELOW BACKFILL AT BASE. OVERLAP 12" MINIMUM AT 1
- METAL TREE GRATE FRAME



- ① TREE PIT OPENING 6'-8" x 4'-0" (TYP)
- ② 4" PVC DRAIN PIPE IN STONE DRAIN BUMP WRAPPED IN FILTER FABRIC. REFERENCE DRAINAGE PLAN FOR CONNECTING TO STORM DRAIN SYSTEM
- ③ TREE ROOTBALL
- ④ TREE PIT ROOT PATH AERATION SHEET IN 4" WIDE TRENCH, BACKFILL TRENCH W/SPECIFIED SOILS EXTEND IN-LINE TO NEXT ADJACENT TREE
- ⑤ DEEP ROOT SILVA CELL (DOUBLE STACKED), BACKFILL WITH SPECIFIED SOILS
- ⑥ CONCRETE CURB AND GUTTER (SEE ROADWAY PLANS)
- ⑦ PAVEMENT WALKWAY (SEE STREETSCAPE PLANS AND DETAILS)
- ⑧ 4" x 12" CONCRETE CURB AROUND TREE PIT, FLUSH WITH BRICK PAVING
- ⑨ METAL TREE GRATE FRAME

Tree Planting Details - University Village Streetscape Plan



All street trees are to be planted according to the detail below. PLEASE NOTE: WHEN STORM DRAINAGE IS AVAILABLE ON OR ADJACENT TO THE SITE, ALL TREE PITS MUST BE DRAINED INTO THE STORM SEWER WITH PERFORATED PVC PIPE.

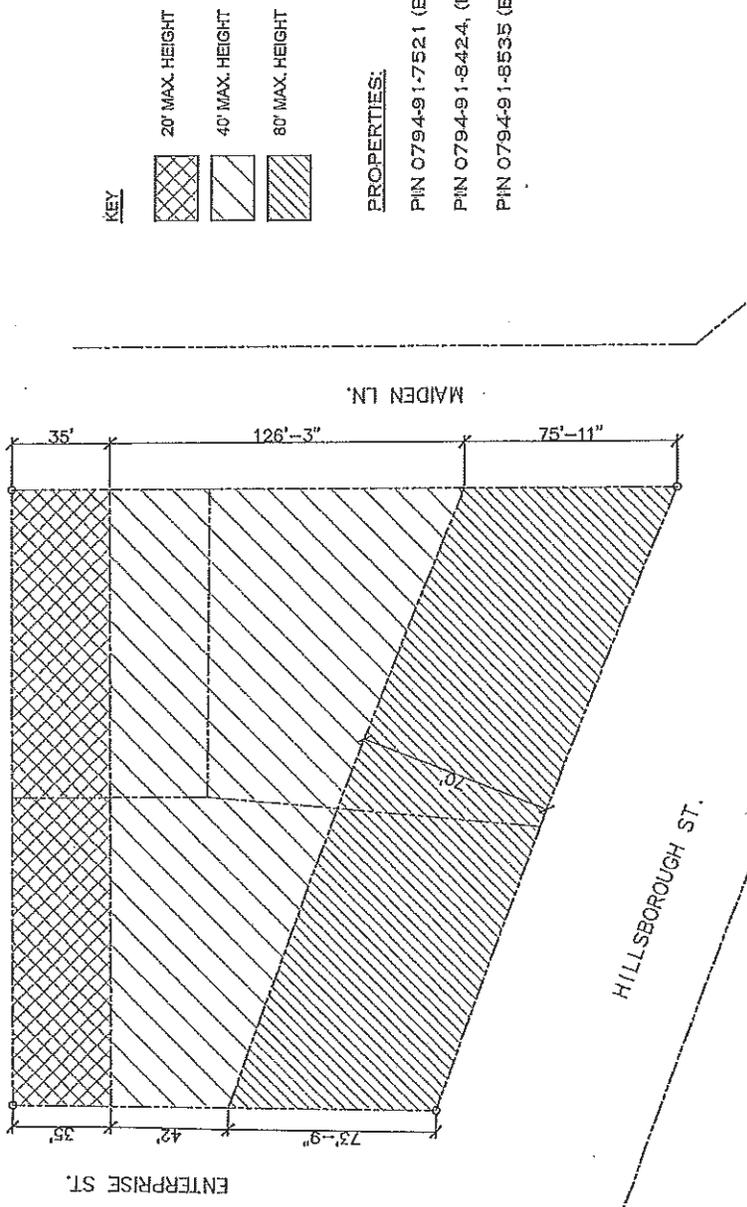
The decision to use root paths and Silva Cells prevents the soil from compacting and allowing the roots more room to grow and providing much needed nutrients for larger healthier tree. The current planting detail provides no additional systems for the tree to thrive in harsh urban environments.

*Note: The use of Silva Cells and Root Paths has not been finalized at this time. A Consultant is continuing to work with urban forester to finalize this design.



Exhibits B-1 and B-2 for
Amended University Village Streetscape Plan

EXHIBIT B-1 - HEIGHT MAP



KEY

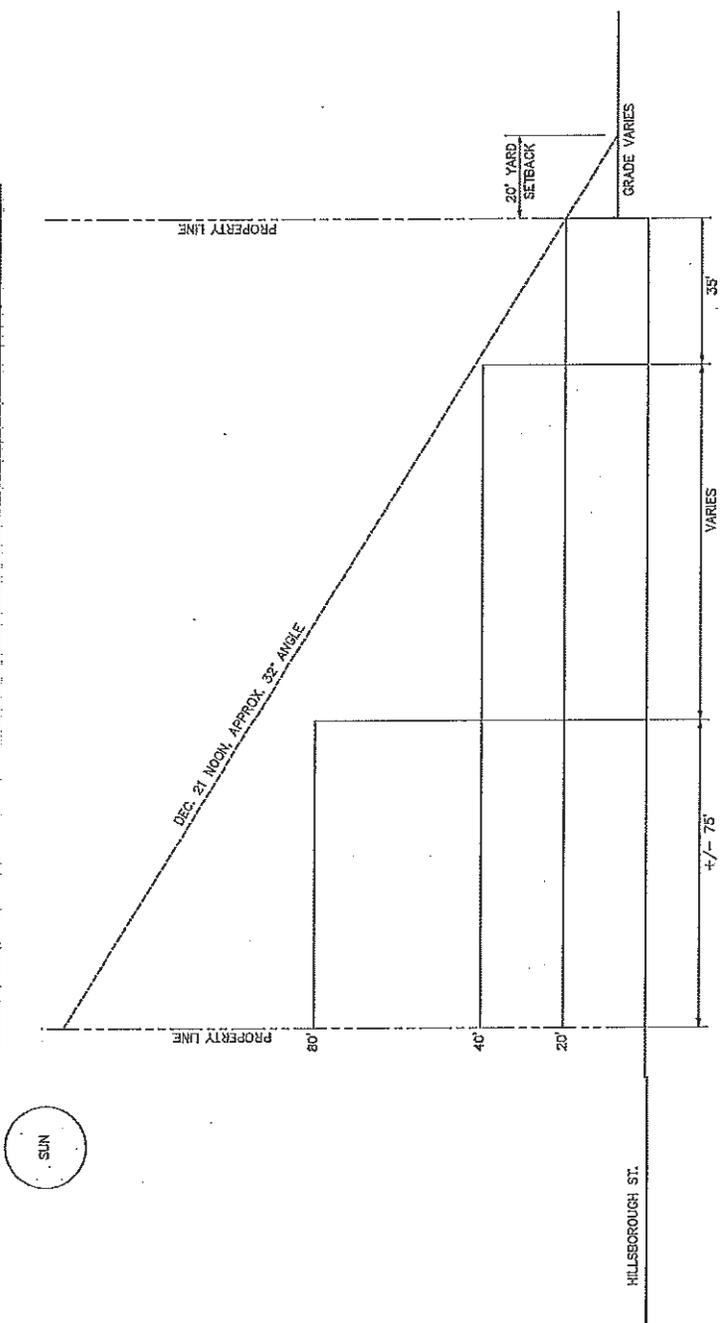
	20' MAX. HEIGHT
	40' MAX. HEIGHT
	80' MAX. HEIGHT

PROPERTIES:

- PIN 0794-91-7521 (BK 13826, PG 1699)
- PIN 0794-91-8424, (BK 13920, PG 2087)
- PIN 0794-91-8535 (BK 12900, PG 195)

NOTE: THIS HEIGHT MAP SHOWS THE MAXIMUM HEIGHT FOR THOSE PROPERTIES SUBJECT TO REZONING CASE Z-36-12. THE HEIGHT PERMITTED BY THIS HEIGHT MAP SUPERSEDES AND CONTROLS OVER THE UNDERLYING ZONING DISTRICTS. HOWEVER, THE PROPERTIES SUBJECT TO REZONING CASE Z-36-12 ARE STILL SUBJECT TO THE SOLAR ACCESS ANGLE ON PAGE 26 OF THIS STREETSCAPE PLAN, AND THE MAXIMUM HEIGHTS ALLOWED BY THIS HEIGHT MAP COMPLY WITH THE SOLAR ACCESS ANGLE STANDARD.

EXHIBIT B-2 - SOLAR ACCESS ANGLE



NOTE 1: THIS SITE SECTION SHOWS THE MAXIMUM HEIGHT FOR THOSE PROPERTIES SUBJECT TO REZONING CASE Z-36-12. NO PART OF ANY BUILDING SHOULD BE BUILT HIGHER THAN A LINE DRAWN AT A 32 DEGREE ANGLE (THE APPROXIMATE SUN ANGLE AT NOON ON THE WINTER SOLSTICE), STARTING AT THE REAR YARD SETBACK LINE FOR ADJACENT RESIDENTIAL PROPERTIES AND CROSSING THE ENTIRE COMMERCIAL PROPERTY, AS ILLUSTRATED ABOVE.

NOTE 2: THE BUILDING FAÇADE ALONG HILLSBOROUGH STREET WILL HAVE AT LEAST ONE OF THE FOLLOWING CHARACTERISTICS:
 (I) A THIRD FLOOR STEP BACK OF 5' DENOTING A HORIZONTAL FAÇADE CHANGE;
 (II) A CHANGE IN BUILDING MATERIALS TO MIMIC A CHANGE IN WALL PLANE;
 (III) VERTICAL CHANGES USING MATERIALS OR STEP BACKS BREAKING UP THE FAÇADE;
 (IV) A STEP BACK OF THE BUILDING TO CREATE OPEN SPACE CREATING AMENITIES SUCH AS OUTDOOR DINING;
 (V) ARCHITECTURAL TREATMENTS DELINEATING THE BASE, MIDDLE AND TOP OF THE BUILDING;
 OR (VI) A SECOND FLOOR OVERHANG WHICH REDUCES THE SCALE OF THE SIDEWALK AREA TO "PEOPLE-SIZE" SPACE.

NOTE 3: THOSE EXTERIOR PORTIONS OF A PARKING STRUCTURE FRONTING ALONG THE PUBLIC RIGHT-OF-WAY SHALL BE DESIGNED TO BE COMPATIBLE WITH THE PRIMARY BUILDING ON THE PROPERTY BY USING COMPATIBLE BUILDING MATERIALS AND ARCHITECTURAL DESIGNS.

EXHIBIT D

SUMMARY OF DISCUSSION ITEMS

On Thursday, June 14, 2012, at 6:00 p.m., the applicant held a neighborhood meeting for the property owners adjacent to the parcel subject to the proposed rezoning. The following items were discussed:

1. Properties included in rezoning
2. Proposed use
3. Timeline of development
4. Public realm improvements
5. Reason for amending Streetscape Plan
6. Public and private investments along Hillsborough Street

Z-36.12
SSP-8.12

EXHIBIT E

NEIGHBORHOOD MEETING ATTENDEES

Matt Zaytoun
6 Maiden Lane
Raleigh, NC 27607

Simon Everett
11 Maiden Lane
Raleigh, NC 27607

Joseph Boisvert
2824 Van Dyke Avenue
Raleigh, NC 27607

Elizabeth Wilson
2216 Garden Place
Raleigh, NC 27607

Jeff Murison
1 Maiden Lane
Raleigh, NC 27607

Jay Dawkins
5 Maiden Lane
Raleigh, NC 27607